



GLEN EIRA CITY COUNCIL

ORDINARY COUNCIL MEETING

TUESDAY 27 APRIL 2021

MINUTES

**Meeting was held in the Council Chambers,
Corner Hawthorn & Glen Eira Roads, Caulfield
at 7:32pm**

Present

The Mayor, Councillor Margaret Esakoff
Councillor Tony Athanasopoulos
Councillor Anne-Marie Cade
Councillor Jim Magee
Councillor Sam Parasol
Councillor Neil Pilling
Councillor Li Zhang
Councillor Simone Zmood
Councillor David Zyngier

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**1. ACKNOWLEDGEMENT**

The Mayor read the acknowledgement.

Glen Eira City Council respectfully acknowledges that the Boon Wurrung people of the Kulin nation are the traditional owners of the land now known as Glen Eira. We pay our respects to their Elders past, present and emerging and acknowledge and uphold their continuing relationship to an responsibility for this land.

2. APOLOGIES - NIL**3. REMINDER TO DECLARE ANY CONFLICTS OF INTEREST IN ANY ITEMS ON THE AGENDA**

Councillors were reminded of the requirement for disclosure of conflicts of interest in relation to items listed for consideration on the Agenda, or any item that was considered at this meeting, in accordance with Section 130(2) of the *Local Government Act 2020* and Rule 60(3) of the Glen Eira City Council Governance Rules. Councillors were then invited to indicate any such conflict of interest.

- Cr David Zyngier - Item Number 8.6 – Post-War and Hidden Gems Heritage Review and Thematic Environmental History (Amendment C214) – Consider Submissions

Procedural Motion

Moved: Cr Magee

Seconded: Cr Zmood

That Council:

1. considers the meeting to be adjourned for 10 minutes from the commencement of any technical problem which prevents Council from livestreaming the meeting; and
2. adjourns the meeting to be reconvened on Wednesday 28 April at 7.30pm via livestreaming on Council's website, in the event livestreaming cannot be resumed within 30 minutes from the commencement of the technical problem which prevents livestreaming on Council's website.

CARRIED UNANIMOUSLY

4. CONFIRMATION OF MINUTES OF PREVIOUS COUNCIL MEETINGS**Moved: Cr Cade****Seconded: Cr Parasol**

That the minutes of the Ordinary Council Meeting held on 6 April 2021 be confirmed.

CARRIED UNANIMOUSLY

5. RECEPTION AND READING OF PETITIONS AND JOINT LETTERS

Nil

6. REPORTS BY DELEGATES APPOINTED BY COUNCIL TO VARIOUS ORGANISATIONS

It is recorded that Cr Athanasopoulos reported on a recent Metropolitan Transport Forum (MTF) meeting.

7. REPORTS FROM COMMITTEES AND RECORDS OF ASSEMBLY

7.1 Advisory Committees

7.1.1 ADVISORY COMMITTEE MINUTES**Moved: Cr Athanasopoulos****Seconded: Cr Cade**

That the minutes of the Advisory Committee meeting as shown below be received and noted and that the recommendations of this Committee be adopted.

1. Elsternwick Cultural Precinct Advisory Committee Minutes - 8 February 2021
2. Community Grants Advisory Committee Meeting Minutes – 6 April 2021

CARRIED UNANIMOUSLY

7.2 Records of Assembly

7.2.1 RECORDS OF ASSEMBLIES OF COUNCILLORS**Moved: Cr Zmood****Seconded: Cr Parasol**

That the Records of the Assemblies as shown below be received and noted.

1. 23 March 2021
2. 30 March 2021
3. 6 April 2021 pre-meeting

CARRIED UNANIMOUSLY

Procedural Motion**Moved: Cr Magee****Seconded: Cr Pilling**

That Council changes the order of business to deal with Item 10.5 Written public questions to Council at this stage of the meeting.

CARRIED UNANIMOUSLY

10.5 Written public questions to Council

1. Helen Fischer – Bentleigh

At a Special Meeting on Tuesday May 5th, 2020, Council passed a motion to declare a climate emergency. Council also committed to ambitious emission reduction targets of: Zero net corporate emissions by 2025 Zero net community emissions by 2030. Council has set these targets after listening to community who urged our Council to commit to stronger climate action. The Council has stated that they will start working on a more ambitious environmental sustainability strategy to guide its work. With regard to the Draft Caulfield Racecourse Reserve Trust Land Management Plan, will the Council advocate for sustainability, biodiversity and climate action in Glen Eira and oppose the filling in of the southern lake for a carpark and/or sporting field? If financial support for a proposal to remove the lake was requested, would the Council commit ratepayers' funds to this? Or would the Council pledge to save the wetland?

Response:

Council generally supports the overall vision outlined within the final Caulfield Racecourse Reserve Trust Land Management Plan, however Council did make a submission to the Draft Plan that acknowledged that there is more work required to understand and finalise detailed designs and implementation.

Council resolved to make a financial contribution to the development of the business case for the entire Land Management Plan, which will allow Council and the Trust to completely understand how the plan is implemented and how environmental protections, sustainability, green spaces and links, and Aboriginal Cultural Heritage Values can be better embedded across the various precincts.

Council acknowledges there are concerns from some about the loss of wetland areas and the protection of biodiversity in the reserve. A key part of the Land Management Plan is protecting and enhancing these opportunities at the North lake and wetlands.

The Reserve's redevelopment also needs to provide for a range of functional open spaces to support community sport and participation. This is why Council has been broadly supportive of the Trust's proposed changes at the southern lake.

Any future investment of Council funds to specific projects would be subject to a business case and would be formally agreed at a Council meeting.

2. Beverly Dillon – Elsternwick

Could council please list all the projects that have currently been either abandoned or put on hold over the past 2 financial years, plus the envisaged savings as a result of this?

These projects include things like constructions like pavilions, toilets, bike paths, street scapes, and very importantly car parks, not approving developments with Zero parking.

Response:

Thank you for this question which is more complex than it appears. Your question will be taken on notice, and our officers will be in contact with you following the Council meeting to seek further clarification ahead of providing a detailed reply.

3. Max Deacon – Elsternwick

My question relates to item 8.11 Elsternwick Precinct Design. In the attachment 2 under the heading "What we heard" it states that "Proposed bus drop offs to Sinclair Street may require negotiation with residents, consider relocating", also in attachment 2 under the heading "Changes we have made" it states that "2 No. bus drop off zones in Sinclair Street, located east of Selwyn Street away from residential properties;" (Is it east or west)

1. Why were the bus stops moved?
2. Which residents were consulted on the move? The bus stops are now directly in front of the balconies of the apartments at 14 Sinclair St.

Response:

1. The bus stops were relocated following feedback received by stakeholders of the Elsternwick Cultural Precinct Advisory Committee over multiple meetings. The proposed location was identified as being preferable for the optimal function of the entire precinct.

This change allowed the relocation of the pedestrian crossing point to a safer and preferable location in the proposed precinct design, linking the precinct with the Sholem Aleichem College.
 2. The Elsternwick Cultural Precinct Advisory Committee was formed so that all stakeholders, including residents had a form of representation as Council continued to develop its vision and design. There are resident members that sit on the Elsternwick Cultural Precinct Advisory Committee in this capacity.
-

4. Stuart Edge – Ormond

Could the council please provide the following data:

- (a) What is the date of the latest council performed traffic counts for any part of Hawthorn Rd, Glen Huntly Rd, Centre Rd, East Boundary Rd, Inkerman Rd, Murrumbeena Rd, Grange Rd? and
- (b) Will the council publish this data in full please?

Response

- a) Council only conducts traffic survey counts on roads managed by Council. Of your list, only Inkerman Road and Glenhuntly Road are managed by Council, the rest are under the management of the Department of Transport.

The latest traffic survey data was collected in March 2021. The traffic count was undertaken outside Caulfield Park, just west of Normanby Road.

The tram tracks on Glenhuntly Road mean that it is not possible to undertake traffic tube counts.

However, we did recently undertake a number of traffic movement counts at intersections as part of the Glen Huntly Major Activity Centre transport impact assessment.

- b) An officer from our traffic engineering department will get in touch with you in the coming days to clarify your request and provide a timeline for providing the information that you are seeking. They will also help to put you in touch with the relevant area of the Department of Transport so that you can source the other data that you are seeking directly from them.
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5. Andrew Pringle – Melbourne

It is very disappointed that the Inkerman project is proposed to be put on hold due to 'cost'. Has there been any consideration of a lower-cost 'pop-up' approach that has been implemented around the world?

Response

In preparing the Inkerman Road Safe Cycling Corridor recommendation, officers noted the merits of safe cycling infrastructure, particularly separated bike lanes.

The officer recommendation for Agenda Item 8.7 states in Point 5:

That Council receives a further report at a future Ordinary Council Meeting providing insights into Glen Eira's Safe Cycling Corridor network and recommendations of cycling projects that will become the focus of further work

This recommendation suggest that a further report will be prepared detailing Glen Eira's safe cycling network and other projects that could be progressed at a lower cost, including trial or "pop up" installations, that will still support safe cycling.

6. Martin Lohan – Elsternwick

Woolworths supports the Elsternwick Cultural Precinct in Selwyn Street as well as anchoring one side of the proposed precinct.

Will Council delay adopting the proposed design based on it not being fully endorsed by the key stakeholders in the Jewish Holocaust Centre, Kadimah, Sholem Alecheim College and the Jewish Arts Quarter? Is it wise to adopt a design until a decision is made on 7 Selwyn Street and 10 Selwyn Street?

Response

Council has advanced the development of a design for a cultural precinct in Selwyn Street since the adoption of the Elsternwick Structure Plan in 2016 and has worked with the Elsternwick Cultural Precinct Advisory Committee through a collaborative design process.

Throughout its development, the precinct design has balanced the requirements of the stakeholders against Council's vision for the creation of a cultural precinct which include it being a pedestrianised precinct.

It is acknowledged that due to some competing needs of the stakeholders that compromises had to be made. It is considered that a reasonable balance has been achieved that aligns with Council's vision for the precinct and is able to be delivered regardless of the outcomes of the two referenced applications at 7 Selwyn Street and 10-16 Selwyn Street.

7. Jacinta Smith – Elsternwick

Recently, there has been significant concern in the media regarding the shedding of plastic micro and macro particles into waterways from synthetic grass surfaces. I note that the Caulfield Racecourse Reserve Trust Land Management Plan proposes to cover significant areas with synthetic grass or playing surface.

Will Council advocate for minimal use of synthetic grass?

AND also advocate to ensure the incorporation of a suitable filter system, *into the plans*, to trap synthetic particles before they enter either the lake or drainage system?

Response

The resolution by Council to make a financial contribution to the development of the business case for the Caulfield Racecourse Reserve Land Management Plan will ensure Council's active participation in the process and therefore provide a better understanding of the environmental sustainability needs for the green spaces and playing surfaces including synthetic surfaces, noting that there is a need and a desire within the community for a variety of open space uses within the Reserve.

8. Warren Green – Bentleigh

- a) Can Council please provide an update on the Carnegie Swim Centre development planning, including current project status, the provision of the current design concept, projected completion timeframes, and projected project cost?
- b) It is noted that Council has received an extension of permit time application for a seven-storey development at 342-344 Centre Road, Bentleigh. As background, this development is inconsistent with the current interim controls for the site with the permit being granted prior to the implementation of these controls. It is expected for this and many other reasons, including car parking not meeting the statutory requirements, that this extension of permit time will be rejected. As a result of a Council error, the original development application for this site was not advertised to residents and while this was the case, similar development applications in Bentleigh received in excess of 200 objections. Can Council outline details on the permit for the site including the date that the original permit was granted, the date that the current permit expires, details on any previous permit extensions and Council's position on the extension application?

Response

- a) The Carnegie Swim Centre project is nearing the completion of the detailed design phase. Following a phase of community communication and engagement in May, the procurement process for the Head Contractor will commence with an Expression of Interest in June. The Construction has now been scheduled for early 2022, with the centre to open by mid-2023. Project cost is estimated to be approximately \$51m. An article updating the community will appear in the May edition of Glen Eira News.
- b) For the avoidance of any doubt, there has been no error made by Council with the processing of the original application which followed correct process, including having been advertised in accordance with the legislative requirements.

The advertised application attracted 16 objections from the community, including an objection from yourself. Ultimately, the application was approved on review to the Victorian Civil and Administrative Tribunal (VCAT) on 31 January 2017.

A request to extend the permit was refused by Council on 20 September 2018 due largely to the introduction of the interim Design and Development Overlay that prescribes mandatory maximum height limits to parts of the Bentleigh Major Activity Centre. This decision was also reviewed by VCAT which approved an extension. The VCAT decision considered whether it was appropriate to extend the permit in light of the introduction of the interim planning controls. VCAT noted the following:

"As stated by the Tribunal in Hamilton Chase 'the relevant mandatory height controls are temporary and at present do not represent a permanent shift in planning controls applying to the land and as a result, in the context of this application, cannot be afforded the same weight as may be given to a permanent shift in planning policy or even planning controls'. This statement equally applies to this case".

Council received the latest request to extend the planning permit on 13 April 2021. The request seeks a one-year extension so that the development can start by 1 May 2022 and be completed by 1 May 2024.

Council is yet to undertake an assessment of the latest request to extend the planning permit and therefore has not formed a position on the request. However, should this extension of time request not be supported by Council, the applicant similarly has the ability to challenge this decision at VCAT.

9. Mary Neighbour – Caulfield South

In responses to public questions over the past four months, Council has repeatedly referred to the development of Urban Design Frameworks for Caulfield North and South, as well as for Bentleigh East. Council is now proposing Built Form Frameworks. How does Council interpret the differences between the two, what were the reasons for the change, and how will it affect planning outcomes for these neighbourhood centres?

Response

There are many different assessments and strategies that can be used to support the development of planning controls. Both Structure Plans and the development of Urban Design Frameworks are guided by the Minister for Planning in Planning Practice Notes and other aligned guidelines.

Urban Design Frameworks define direction for change for a centre but have a primary focus on public land and public areas. They still however, require an analysis of land use, economic activity, environment, traffic and transport and pedestrian activity. They are a multi-faceted, multi-disciplinary, deep technical dive into each activity centre and this takes an extensive amount of time to complete.

Built Form Frameworks, which directly focus on planning controls as a more immediate outcome, can be viewed as the next tier down in the management of change in activity centres. Their formulation is less rigorous than Urban Design Frameworks and can be tailored in their scope to more accurately reflect the particular, nuanced challenges of smaller neighbourhood centres and local centres. By their nature, Built Form Frameworks are relatively quicker to produce than Urban Design Frameworks but as you can see from Agenda Item 8.5, they target one specific component of the centres. That is, the commercially zoned parts of each shopping centre, for which there are currently no planning height controls.

In this case, the urgency demanded by the community and the desire for three centres to be reviewed in parallel required Council to focus primarily on the main concerns of residents, that being the absence of height related planning controls. This was the determining factor in Council's approach.

10. Markus Oswald - Carnegie

- a) Council has now basically rejected two proposed major planning documents – Amendment C184 and the Glen Huntly Structure Plan. What progress has been made on these documents and when does council envisage they will be presented to the community?
- b) On January 30th 2021 council advertised a tender for “Consultancy Services for the Glen Eira Housing Strategy”. Has this tender been awarded, what is the contract amount awarded and when does council envisage the strategy will be presented to the community?

Response

- a) Following Council's decision to not proceed further with Amendment C184 and to undertake a reset of the process, officers are now preparing a housing strategy. The housing strategy will provide a clear vision for housing over the next 15 years and provide strategic support for current and future structure plans. This will provide guidance on housing types and where growth should be accommodated to ensure we can meet future housing needs. While this strategy is being prepared, officers are working concurrently to revise the Bentleigh and Carnegie Structure Plans as well as the Glen Huntly Structure Plan. With support from an adopted housing strategy, this will help to address the strategic justification to support new amendments and build the case for successful introduction of planning controls in the future.

Officers are still refining the community and stakeholder engagement program associated with the housing strategy. This will be the subject of a report to Council in June. Opportunities to comment on the strategy will be widely communicated soon. The final strategy is scheduled to be completed in April 2022.

- b) Upon review of the tenders received for 'Consultancy Services for the Glen Eira Housing Strategy' officers made the decision to not proceed further with this tender process. Instead, it was decided to pursue independent consultants to undertake specific individual components of the housing strategy such as the demographic analysis, neighbourhood character assessments and other aspects. Officers will now lead the drafting of the Strategy document. This was considered a more timely and cost effective approach.

It is recorded that Cr Athanasopoulos vacated the Chamber at 8:00pm.

11. Catherine Mc Naughton – Glen Huntly

How will Council deliver its Integrated Transport Plan and Cycling Action Plan in 2021&2 to increase active travel to 50% of trips to achieve Glen Eira; - Health and Well-being Plan 2021-25 More locals riding supports 3 of the 8 priority areas.

- o Tackling climate change and its impact on health
- o Increasing active living
- o Improving mental wellbeing

Response

Council is very supportive of active travel to provide the community with choices other than car trips, particularly for short distances. Officers are implementing actions associated with Council's *Walking and Accessibility Action Plan* and *Cycling Action Plan* along with the advocacy work informed by the *Public Transport Advocacy Plan*.

The officer recommendation on the Inkerman Road Safe Cycling Corridor item, recommends a further report at a future Ordinary Council Meeting providing insights into Glen Eira's Safe Cycling network and recommendations of cycling projects that will become the focus of further work'. This will contribute to the goal of achieving a 50:50 mode split. Additionally, Council continues to advocate for walking and cycling improvements associated with the Glenhuntly Level Crossing Removal Project, and the Caulfield Structure Plan which Council has now taken over from the VPA.

8. OFFICER REPORTS (AS LISTED)

8.1 1032-1032A NORTH ROAD, BENTLEIGH EAST

Moved: Cr Cade**Seconded: Cr Pilling**

It is recorded that Cr Athanasopoulos entered the Chamber at 8:03pm.

That Council issues a Notice of Decision to Grant a Planning Permit for Application No. GE/DP-33848/2020 for construction of a four storey building containing two shops and 12 dwellings, a reduction in the car parking requirements and use and construction of a building in the Public Acquisition Overlay at 1032-1032A North Road, Bentleigh East, in accordance with the following conditions:

Amended plans

1. Before the development starts, amended plans and documents to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plans will be endorsed and will then form part of the permit. The plans must be drawn to scale with dimensions and must be generally in accordance with the advertised plans identified as drawing numbers TP-020-G (dated 13/11/2020), TP-021-J (dated 13/11/2020), TP-100-H (dated 04/11/2020), TP-101-J (dated 13/11/2020), TP-102-J (dated 13/11/2020), TP-103J (dated 13/11/2020), TP-401-J (dated 13/11/2020), TP-400-J (dated 13/11/2020), Area Schedule and Bette Apartment Design Standard Analysis (Rev E, dated 13/11/2020) and prepared by Ewert Leaf Pty Ltd, but further modified to show:
 - (a) The building canopy modified in accordance with the Department of Transport requirements at Condition 18.
 - (b) Car stacker systems shown on the plans to be consistent with the advertised Traffic Report prepared by Ratio dated 19 November 2020.
 - (c) Convex mirrors and amber warning lights shown on the ground floor plan at the gate to the parking area, with a notation to ensure the amber warning lights operate during the door opening and closing sequence.
 - (d) The two horizontal bicycle spaces provided with e-bike parking facilities.
 - (e) The privacy screen for the south facing bedroom window of Apartment 2.03 labelled as "EF06" on the elevation plans and shown on the floor plan.
 - (f) An Acoustic Report prepared by a suitably qualified acoustic engineer and to the satisfaction of the Responsible. The report must prescribe the form of acoustic treatment to the car stacker system to limit its impact on nearby dwellings. Any acoustic attenuation requirements are to be shown on the plans.
 - (g) Enlarged sections at scale of 1:20 that are fully dimensioned and detail the screening design and materials and demonstrate the compliance with Standard B22 of Clause 55.04-6 of Glen Eira Planning Scheme as follows:
 - i. Privacy screen used along the south boundary of the balcony for Apartment 1.03.
 - ii. Privacy screen used for the south facing bedroom windows of Apartments 1.03 and 2.03.

- iii. Privacy screen used to divide the balconies.

Layout not to be altered

- 2. The development and layout and description of the uses as shown on the endorsed plans must not be altered or modified (unless the Glen Eira Planning Scheme specifies a permit is not required) except with the prior written consent of the Responsible Authority.

Landscaping

- 3. Any vegetation in the planter box on the upper level must be maintained in good order and any dead, diseased or damaged plant replaced to the satisfaction of the Responsible Authority.

Staff numbers

- 4. No more than two staff may be working in each shop on the land at any one time, except with the prior written consent of the Responsible Authority.

Construction Management Plan

- 5. Before the development starts, including any demolition and excavation, a Construction Management Plan (CMP) to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plan will be endorsed and will then form part of the permit. The CMP must show:
 - (a) Delivery and unloading points and expected frequency.
 - (b) Truck haulage routes, circulation spaces and queuing lanes.
 - (c) Details how traffic and safe pedestrian access will be managed. These must be in the form of a Traffic Management Plan designed by a suitably qualified traffic practitioner.
 - (d) A liaison officer for contact by owners / residents and the Responsible Authority in the event of relevant queries or problems experienced.
 - (e) An outline of requests to occupy public footpaths or roads, or anticipated disruptions to local services.
 - (f) Any requirements outlined within this permit as required by the relevant referral authorities.
 - (g) Construction activity must only occur between the hours of 7.00 am and 6.00 pm, Monday to Friday, 7.00 am to 1.00 pm on Saturday and no construction on Sunday and Public Holidays.
 - (h) Measures to control noise, dust, water and sediment laden runoff.
 - (i) Measures to ensure that subcontractors or tradespersons operating on the site are aware of the requirements of the CMP.
 - (j) Any construction lighting designed, baffled and located to prevent any adverse effect from light spill to the amenity of abutting and adjoining land.
 - (k) During the construction of the buildings and works allowed by this permit, the laneway adjacent to the subject land must be kept free of parked or standing vehicles or any other obstruction, including building materials, equipment etc. so as to maintain free vehicular passage to abutting benefiting properties at all times, unless with the written consent of the Responsible Authority.

6. All construction (including demolition and excavation) must be carried out and complied with in accordance with the approved Construction Management Plan to the satisfaction of the Responsible Authority and must not be varied except with the prior written consent of the Responsible Authority.

Car Stacker System Management Plan

7. Before the building is occupied, a Car Stacker System Management Plan (CSSMP) to the satisfaction of the Responsible Authority must be submitted to and approved by the Responsible Authority. When approved, the plan will be endorsed and will then form part of the permit. The CSSMP must show:
 - (a) Allocation of car spaces according to vehicle size and type.
 - (b) Ongoing maintenance of the car stacker system.
 - (c) Instructions to owners and occupiers about the operation of the car stacker system.
 - (d) Communicating to prospective residents about the availability of car stacker spaces and sizes.
8. The provisions, recommendations and requirements of the endorsed Car Stacker System Management Plan must be implemented and complied with to the satisfaction of the Responsible Authority and must not be varied except with the prior written consent of the Responsible Authority.
9. Before the building is occupied, the mechanical car stackers as shown on the endorsed plans must be installed and always maintained in a good working order. The mechanical car parking stackers must be permanently available for the parking of vehicles to the satisfaction of the Responsible Authority, except with the prior written consent of the Responsible Authority.

Implementation of the Waste Management Plan

10. The provisions, recommendations and requirements of the endorsed Waste Management Plan (prepared by Ratio Consultants Pty Ltd, dated 3 September 2020) must be implemented and complied with to the satisfaction of the Responsible Authority and must not be varied except with the prior written consent of the Responsible Authority.

Implementation of acoustic measures

11. The provisions, recommendations and requirements of the endorsed Acoustic Report must be implemented and complied with to the satisfaction of the Responsible Authority and must not be varied except with the prior written consent of the Responsible Authority.

Car parking layout and access

12. Before the building is occupied, the areas set aside for the parking of vehicles and access lanes as shown on the endorsed plans must be:
 - (a) fully constructed.
 - (b) properly formed to such levels that may be used in accordance with the plans.

- (c) surfaced with an all weather surface or seal coat (as appropriate).
- (d) drained and maintained in a continuously usable condition.
- (e) line marked to indicate each car space, loading bay and/or access lane.
- (f) clearly marked to show the direction of traffic along access lanes and driveways.

all to the satisfaction of the Responsible Authority.

Allocation of car parking spaces

- 13. No fewer than one car parking space for each one or two bedroom dwelling; two car parking spaces for each dwelling with three or more bedrooms; and three car parking spaces for the shops must be provided on the land to the satisfaction of the Responsible Authority.

Screening of windows

- 14. Before the building is occupied, all screening shown on the endorsed plans must be installed and permanently maintained in accordance with the endorsed plans. The screening measures as shown on the endorsed plans are not to be altered or removed except with the prior written consent of the Responsible Authority.

Plant and equipment

- 15. No plant, equipment, services and substations other than those shown on the endorsed plans are permitted except with the prior written consent of the Responsible Authority

Concealment of services

- 16. All pipes, fixtures, fittings and vents servicing any building on the site must be concealed in service ducts or otherwise hidden from view to the satisfaction of the Responsible Authority.

External lighting

- 17. External lighting must be designed, baffled and located to prevent any adverse effect from light spill to the amenity of abutting and adjoining land to the satisfaction of the Responsible Authority.

Department of Transport requirements (Conditions 18 and 19)

- 18. Prior to the commencement of the buildings and works, amended plans must be submitted to and approved by the Responsible Authority. The plans must be generally in accordance with the plans date stamped November 2020 and annotated Dwg no. TP-101, Revision J but modified to show:
 - (a) The building canopy along the North Road frontage modified such that it maintains a 3 metre vertical clearance from the footpath level and a 4.25 metre horizontal clearance from the existing kerbline.
- 19. No compensation is payable under part 5 of the *Planning and Environment Act 1987* in respect of anything done under this permit.

Permit expiry

20. This permit will expire if one of the following circumstances applies:

- (a) The development is not started within three years of the date of this permit.
- (b) The development is not completed within five years of the date of this permit.
- (c) The use is not started within six years of the date of this permit.
- (d) The use is discontinued for a period of two years.

The Responsible Authority may extend the permit if a request is made in writing in accordance with Section 69 of *Planning and Environment Act 1987*.

Permit notes

- Condition 1 of the planning permit sets out the changes that need to happen before the use or development can start. If any other changes are made to the plans that are not generally in accordance with the decision plans, then you will be required to apply for an amendment to the planning permit under Section 72 of the *Planning and Environment Act 1987*.
- This planning permit represents the planning approval for the use and/or development of the land. This planning permit does not represent the approval of other departments of Glen Eira City Council or other statutory authorities. Other approvals may be required and may be assessed on different criteria from those that are considered as part of the planning permit process. You are required to ensure you comply with any obligation.
- Residents of the dwellings allowed under this permit will not be issued Residential Parking Permits (including visitor parking permits).

Asset Engineering notes

- No net increase in peak stormwater runoff in Council drainage network. Post development peak storm water discharge to Council drainage network must be maintained to the predevelopment level for 10 year ARI. Detailed plans and computations must be submitted to Council for approval before any works start.
- All on-site stormwater is to be collected from the hard surface areas and must not be allowed to flow uncontrolled into adjoining properties. The on-site drainage system must prevent discharge from the proposed driveway/laneway onto the existing laneway. Such a system may include either:
 - A trench grate (150mm minimum internal width) located within the proposed laneway/driveway and/or
 - Shaping the laneway/driveway so that water is collected in a grated pit on the property and connect to internal drainage system.
- Any firefighting equipment for the building must be accommodated within title boundary. Council will not allow private fire equipment in the Road Reserve.

- The existing footpath levels must not be altered or modified to accommodate any DDA compliance.
- An Asset Protection Permit must be obtained from Council Engineering Services Department before the development starts.
- All relevant Engineering Permits must be obtained before any development starts within the Road Reserve and or stormwater connection to Council drainage network.

FOR: Crs Athanasopoulos, Esakoff, Cade, Parasol, Pilling, Zhang and Zyngier (7)

AGAINST: Crs Magee and Zmood (2)

CARRIED

8.2 VCAT WATCH

Moved: Cr Cade**Seconded: Cr Pilling**

That Council notes the update on VCAT matters.

CARRIED UNANIMOUSLY

8.3 FINANCIAL MANAGEMENT REPORT (MARCH 2021)

Moved: Cr Pilling**Seconded: Cr Magee**

That Council notes the Financial Management Report for the period ending 31 March 2021.

CARRIED UNANIMOUSLY

8.4 WRITTEN PUBLIC QUESTIONS TO COUNCIL

Moved: Cr Cade**Seconded: Cr Parasol**

That Council confirms that:

1. the *Written Public Questions to Council* guidelines (shown as Attachment 1 to this report); and
 2. the *Public Participation at Ordinary Council Meetings* guidelines (shown as Attachment 2 to this report);
- will recommence from the 19 May 2021 Ordinary Council Meeting.

FOR: Crs Athanasopoulos, Esakoff, Magee, Cade, Parasol, Pilling, Zhang and Zmood (8)

AGAINST: Cr Zyngier (1)

CARRIED

**8.5 DRAFT BUILT FORM FRAMEWORKS FOR PUBLIC CONSULTATION -
CAULFIELD PARK, CAULFIELD SOUTH AND BENTLEIGH EAST
NEIGHBOURHOOD ACTIVITY CENTRES**

Moved: Cr Athanasopoulos**Seconded: Cr Zmood**

That Council:

1. endorses the draft Built Form Frameworks prepared for Caulfield Park, Caulfield South and Bentleigh East Neighbourhood Activity Centres, as attached to the minutes for public consultation for a period of six weeks; and
2. receives a further report on the consultation outcomes, officer response, and final recommended Built Form Frameworks.

CARRIED UNANIMOUSLY

CAULFIELD PARK NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK

Tract



Prepared for Glen Eira City Council

DRAFT BUILT FORM FRAMEWORK

APRIL 2021

QUALITY ASSURANCE

Glen Eira NAC`s Built Form Frameworks
Caulfield Park NAC Draft Built Form Framework

Prepared for
Glen Eira City Council

Project Number
320.0714.U.03

Revisions

Issue	Date	Description	Prepared By	Reviewed By	Project Principal
00	21 January 2021	Response to Council Officer Comments	KXS	TS	MN
01	4 February 2021	Response to Council Officer Comments	KXS	TS	MN
02	21 April 2021	Response to Council Officer comments	KXS	TS	MN
03	14 April 2021	Response to Council Officer comments	KXS	TS	MN
04	21 April 2021	Revisions from Council discussions (Municipal context plan revised)	KXS	TS	MN

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INTRODUCTION

PROJECT OVERVIEW

STUDY AREA

DR



פיתה מן
رجل البينا
THE PITA MAN

STICK ON
SIGNS

Growing Together

ulfield Family
chiropractic

Phone: 9532 8715

O'Milk
Convenience Store

40



TOBACCO
NO SMOKING

PROJECT OVERVIEW

This built form framework for the Caulfield Park Neighbourhood Activity Centre (NAC) is one of three built form frameworks being prepared to address an existing policy gap. The other centres forming part of the project are the Caulfield South Neighbourhood Activity Centre and the Bentleigh East Neighbourhood Activity Centre.

The Built Form Frameworks set out preferred development outcomes in each centre. This will ensure new development provides for the continual revitalisation of the shopping strips with high quality design whilst maintaining the valued character of each centre and protecting the amenity of adjoining residential areas.

The Frameworks provide guidance for building heights, setbacks and other design considerations. These frameworks are supported by rigorous analysis and guided by sound urban design principles appropriate to the context of each centre.

This Caulfield Park NAC Built Form Framework consists of the following parts:

Part 1 - Context and Analysis - Provides analysis and background research to understand the NAC, its context and drivers for change.

Part 2 - The Built Form Framework - Provides the guiding principles and the built form framework including building heights and setbacks.

Proposed planning controls have been developed to reflect the requirements of the Caulfield Park NAC Built Form Framework. The controls are provided in the form of a Design and Development Overlay.

STUDY AREA

The Caulfield Park NAC is located approximately 1.2km west of the Caulfield Junction Major Activity Centre. It provides a limited amount of retail, hospitality, personal and business services, and office uses. It is surrounded by housing, two schools and opens up onto Caulfield Park, which is a 26 Hectare major open space destination. The study area for the Caulfield Park NAC Built Form Framework generally aligns with the existing Commercial 1 Zoning. Refer to Figure 1 - Study Area Plan.

There are a number of people living within the centre with recent, mixed use developments located along Hawthorn Road. These residents benefit from good public transport access with tram routes along both Hawthorn and Balaclava Roads.

DRAFT

- Study Area
- ◆ Tram Stops



Figure 1. Study Area Plan

PART 1 - CONTEXT & ANALYSIS

1. CONTEXT

- 1.1 STRATEGIC CONTEXT
- 1.2 STRATEGIC DOCUMENTS
- 1.3 PLANNING POLICY FRAMEWORK
- 1.4 LOCAL PLANNING POLICY FRAMEWORK
- 1.5 PLANNING ZONES
- 1.6 OVERLAYS
- 1.7 RECENT DEVELOPMENT CONTEXT

2. ANALYSIS

- 2.1 TOPOGRAPHY
- 2.2 LAND USE & INTERFACES
- 2.3 BUILT FORM & CHARACTER
- 2.4 DEVELOPMENT ACTIVITY & CAPACITY





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8-12

1 CONTEXT

1.1 STRATEGIC CONTEXT

Consistent with sustainable land use and development principles, Neighbourhood Activity Centres are identified in state and local planning policy and strategy as suitable locations for accommodating increased residential density. The policy framework including Plan Melbourne, City Plan and the relevant clauses of the Glen Eira Planning Scheme (State and local) establishes this clear direction.

1.1.1 PLANNING POLICY CONTEXT

Planning policy relating to Caulfield Park Neighbourhood Activity Centre (NAC) identifies the centre as one of 11 NACs in Glen Eira. The higher order centres in Glen Eira are Major Activity Centres, of which there are six. Consistent with sustainable land use principles, NACs in the metropolitan areas are high amenity areas that should accommodate growth to meet the needs of existing and future communities. They differ from the Major Activity Centres identified in Figure 3, which offer a greater amount of transport options, retail and services. As such the level of development and change in the NACs is expected to be less than that experience in the Major Activity Centres.

The following sections provide a brief outline of this policy context. As noted previously, the centre is largely in the Commercial 1 Zone.

In and around activity centres, a degree of change will occur, consistent with Plan Melbourne, City Plan and the Glen Eira Planning Scheme and commensurate with the level of amenity and accessibility provided by each centre and also taking into account locally-specific amenity and character considerations. Planning controls informed by the built form framework will provide Council with the necessary guidance to assess planning applications and to provide proponents and the community with clear parameters for design in centres.

Council aims to sustainably manage anticipated growth and amenity and to identify and preserve the valued features of the built environment as its activity centres evolve. For Council to implement built form guidance that is robust and defensible,

the guidance must balance the strong policy objectives of directing more intense development to well-located areas such as neighbourhood activity centres with the need to protect the key elements that make these centres desirable places in their own right. In the Victorian planning system, planning controls that do not achieve a reasonable balance cannot be introduced to the planning scheme.

1.1.2 BUILT FORM FRAMEWORKS IN CONTEXT

Built form frameworks, like structure plans, consider locally-specific elements of a centre such as land use patterns, built form and character, function and amenity and form a vision and built form controls for addition to the planning scheme. Figure 2 provides an overview of where built form frameworks sit in the planning framework relative to built form decisions and residential growth.

1.1.3 PURPOSE OF THE CAULFIELD PARK NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK

Recent development activity and planning applications at Caulfield Park NAC and other NACs indicate the need for more detailed and specific built form policy and controls to guide development. While there are existing policies and controls applying to the Caulfield Park NAC they generally consist of:

- High-level strategy and planning policy that supports increased densities in NACs.
- Zoning that predominantly guides land use rather than built form outcomes. The Commercial 1 Zone allows for more intense mixed use development and nearby residential zoning provides for a transition up from the residential hinterland towards activity centres.

This built form framework will provide the locally-relevant analysis of physical characteristics and a vision to inform the drafting of built form controls for the planning scheme and provide that more specific and detailed guidance.

The following issues are addressed by the built form framework:

- Preferred building heights based on built form and shadow analyses.
- Street wall and interfaces.
- Building setbacks and modulation – for amenity and visual impact.
- Transitions to less intense development including residential uses.



Figure 2. Built Form Framework Planning Context

1.1.1 CAULFIELD PARK NEIGHBOURHOOD ACTIVITY CENTRE LOCATION AND PHYSICAL CONTEXT

The Caulfield Park Neighbourhood Activity Centre is located on Balaclava Road and Hawthorn Road in Caulfield North (Refer to Figure 3 - Context Plan). The centre is located towards the north of the municipality in a predominantly residential context and enjoys access to fixed rail (tram) services along both roads and the Night Bus along Balaclava Road. The centre is located approximately 7.6 km south-east of the Melbourne CBD.

The centre also benefits from the amenity and character of Caulfield Park at the north-eastern corner of the main intersection. This intersection provides an important vantage point for viewing and experiencing the centre. See the Built Form & Character analysis section of this report for an overview of the existing built form context. The areas immediately surrounding the centre are low-rise neighbourhoods in the General Residential Zone. The General Residential Zone allows for development of up to three storeys.

As mentioned previously, Council's planning policies identify the centre as one of 11 NACs in Glen Eira, being second in the activity centre hierarchy in terms of suitability for increased density and height due to the level of amenity offered.



Figure 3. Context Plan

1.2 STRATEGIC DOCUMENTS

1.2.1 PLAN MELBOURNE 2017-2050

The Plan Melbourne provides strategic direction and guidance for some of Melbourne’ state significant employment and activity centre clusters, along with housing and transport directions for greater Melbourne. While the document does not provide specific guidance to the Caulfield Park Neighbourhood Activity Centre some of the more relevant directions are:

- Direction 2.1: Manage the supply of new housing in the right locations to meet population growth and create a sustainable city.
- Direction 5.1: Create a city of 20-minute neighbourhoods.

These directions aim to cluster new housing in activity centres and other places that offer good access to jobs, services and public transport and includes a policy for local governments to prepare structure plans for activity centres to accommodate growth.

There are significant opportunities for medium and higher density housing options in locations close to jobs and services including located around Major Activity Centres

The strategic direction closely aligns to the future outcomes envisaged for the NAC. Particularly providing housing opportunities close to transport enabling people to work and live in close proximity.

1.2.2 GLEN EIRA CITY PLAN

The Glen Eira City Plan provides a broad framework to manage growth and change in both housing and employment. Consistent with Plan Melbourne and several existing local planning policies, City Plan aims to direct the majority of new growth into activity centres towards sustainable land use and development outcomes.

To implement the strategic framework (introducing planning controls to the planning scheme), structure planning or built form frameworks need to be prepared for key areas suitable for growth such as major and neighbourhood activity centres and urban renewal areas. These activities produce the built

form, amenity and character material to assist to refine height controls and identify specific ways of managing amenity and character through the planning scheme.

Glen Eira City Plan provides an activity centre hierarchy which identifies Major Activity Centres at the top, followed by Neighbourhood Activity Centres and local centres. The highest levels of growth are to be directed to Major Activity Centres and urban renewal areas. Neighbourhood Activity Centres are identified as suitable for ‘moderate focus for housing growth’.

City Plan provides vision statements for growth locations. The vision for the Caulfield Park NAC is:

- **Caulfield Park** will be a local hub, which embraces its strong culture and heritage. A transport junction anchored by green open space, the centre will maintain a strong sense of community and connected village feel.

The Plan also seeks to nominate appropriate heights for the identified housing change areas with the neighbourhood activity centres identified as being suitable for up to between four storeys (for commercial areas with heritage overlays) and five storeys (for commercial areas without heritage overlay).

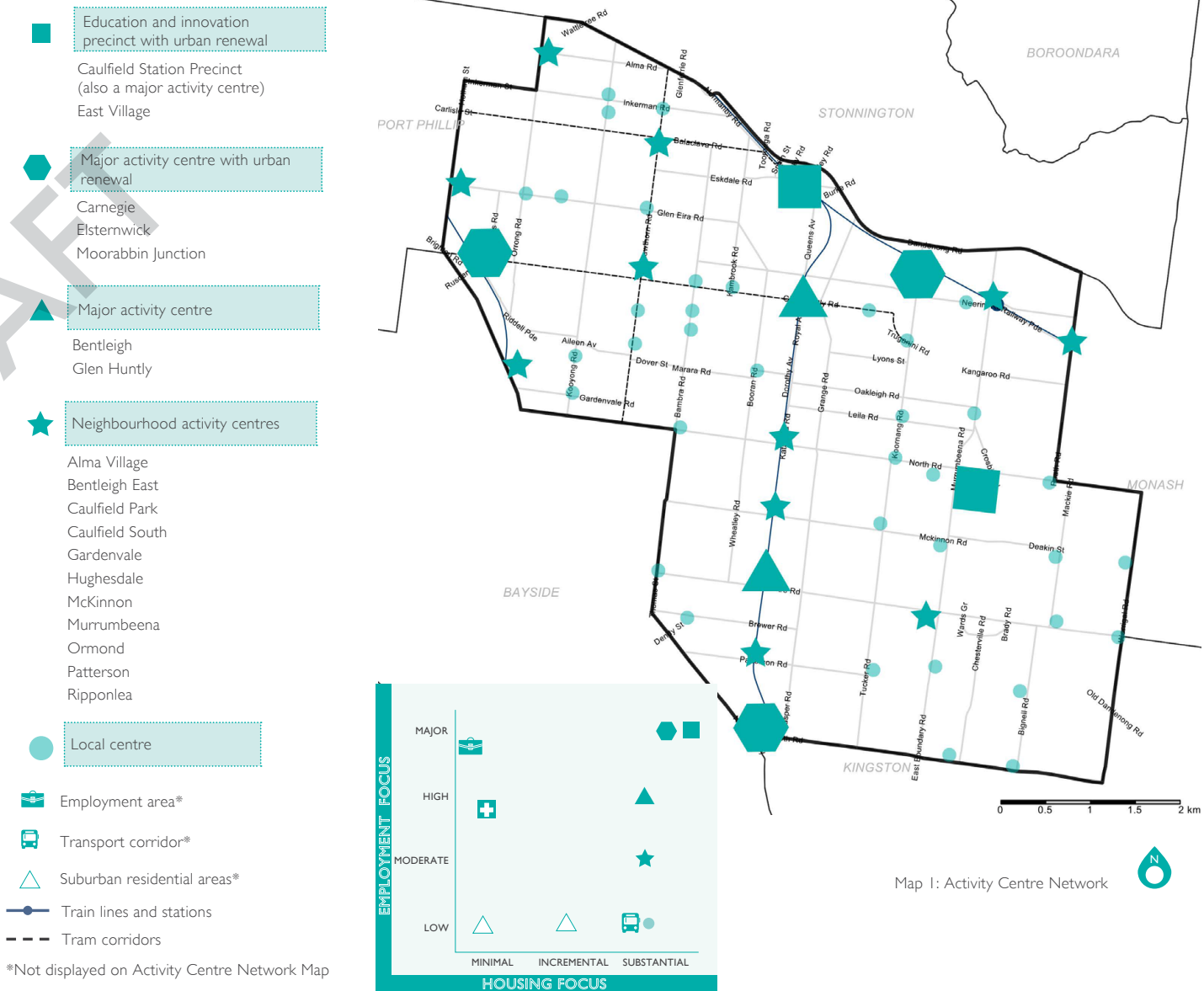


Figure 4. Glen Eira Activity Centre Network Map

1.3 PLANNING POLICY FRAMEWORK

1.3.1 PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following state policies of the Glen Eira Planning Scheme, summarised as follows:

1.3.2 CLAUSE 11 – SETTLEMENT

Seeks to anticipate and respond to the needs of existing and future communities through the provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure.

Clause 11.03-1S (Activity Centres)

Seeks to encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

Clause 11.03-1R (Activity Centres – Metropolitan Melbourne)

Aims to support the development and growth of Metropolitan Activity Centres by encouraging the location of new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.

1.3.3 CLAUSE 13 (ENVIRONMENTAL RISK AND AMENITY)

Seeks to strengthen the resilience and safety of communities by adopting a best practice environmental management and risk management approach.

Clause 13.03-1S (Floodplain management)

- Seeks to assist the protection of:
- Life, property and community infrastructure from flood hazard.
 - The natural flood carrying capacity of rivers, streams and floodways.

Clause 13.04-1S (Contaminated and potentially contaminated land)

Seeks to ensure that potentially contaminated land is suitable for its intended future use and development, and that contaminated land is used safely.

1.3.4 CLAUSE 15 – BUILT ENVIRONMENT

Seeks to ensure that all new land uses and development appropriately responds to its landscape, valued built form and cultural context, and protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value.

Clause 15.01-1S (Urban design)

Aims to create urban environment that are safe, functional and provide good quality environments with a sense of place and cultural identity.

Clause 15.01-1R (Urban design – Metropolitan Melbourne)

Aims to create a distinctive and liveable city with quality design and amenity by supporting the creation of well-designed places that are memorable, distinctive and liveable.

Clause 15.02-1S (Energy and resource efficiency)

Aims to encourage land use and development that is energy and resource efficient, supports a cooler environment and minimises greenhouse gas emissions through strategies such as greening urban areas, buildings, transport corridors and open spaces with vegetation to reduce urban heat island effects.

Clause 15.03-1S (Heritage conservation)

Seeks to ensure the conservation of places of heritage significance by retaining elements that contribute to the importance of the heritage place.

1.3.5 CLAUSE 16 – HOUSING

Seeks to provide for housing diversity and ensure the efficient provision of supporting infrastructure. However, the quantum of development intensification remains relatively open, with appropriate controls to be defined through local planning based on local context.

Clause 16.01-1S (Housing supply)

Seeks to facilitate well-located, integrated and diverse housing that meets community needs through measures such as increasing the proportion of housing in designated locations in established urban areas (including under-utilised urban land) and reduce the share of new dwellings in greenfield, fringe and dispersed development areas and encouraging higher density housing development on sites that are well located in relation to jobs, services and public transport.

Clause 16.01-1R (Housing Supply – Metropolitan Melbourne)

Aims to manage the supply of new housing to meet population growth and create a sustainable city by developing housing and mixed use development opportunities in established areas. Those locations include neighbourhood activity centres - especially those with good public transport connections.

1.4 LOCAL PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following local policies of the Glen Eira Planning Scheme, summarised as follows:

Please note that while this content is correct at the time of writing, Council is in the process of updating its local planning policy. While there will be changes to specific policies it is not expected that the general intent of policy relating to NACs will change.

1.4.1 CLAUSE 21.04 – HOUSING AND RESIDENTIAL DEVELOPMENT

This policy provides objectives and strategies to guide housing and residential development in Glen Eira. The following policies are considered relevant to this built form framework:

Objective 4

- To stimulate and improve the vitality of Glen Eira’s commercial centres.

Strategies

- Encourage a mix of housing types, increased residential densities and mixed use developments within urban villages and neighbourhood centres.
- Encourage residential development, where considered appropriate, subject to consultation with traders and the wider community, above or below existing car parks in urban villages and neighbourhood centres, whilst maintaining or enhancing the existing level of parking.
- Ensure that developments in commercial areas that adjoin residential areas are sensitively designed to protect residential amenity.
- Ensure residential development in commercial areas does not contribute to traffic and car parking problems.

1.4.2 CLAUSE 21.06 – BUSINESS

This policy provides objectives and strategies to guide the development of identified urban villages and neighbourhood centres to cater for a wider variety of commercial use while retaining a hub of convenience shops within Glen Eira. The following policies are considered relevant to this built form framework:

Objectives (amongst others)

- To maintain a mix of commercial centres that cater for the needs of the Glen Eira community.
- To enhance and further develop urban villages and neighbourhood centres as the focus for community life.
- To maintain pleasant and safe public environments in commercial centres in partnership with business.

Strategies (amongst others)

- Maintain the hierarchy of commercial centres so that centre functions are easily definable and to ensure that retail and other requirements are suitably provided at each level in the hierarchy.
- Ensure that new or expanded land uses are able to be accommodated in existing commercial centres, including the encouragement of non-retail businesses, where appropriate.
- Ensure commercial activities in mixed use zone complement other commercial activities nearby and not undermine the commercial hierarchy.
- Ensure that, wherever possible, additional population growth can be accommodated (for example through multi-unit development) so existing facilities in commercial centres can continue to be supported.
- Ensure that commercial centres have a high standard of urban design and are attractive and appealing to potential customers, traders and investors.

1.4.3 CLAUSE 22.07 – HOUSING DIVERSITY AREA POLICY

This policy identifies the areas where housing diversity will be encouraged. It also seeks to ensure that the density, mass and scale of development is consistent with the role, capacity and constraints of each centre.

Objectives (amongst others)

- To encourage housing diversity in preferred strategic locations that have good access to public transport, commercial, community, educational and recreational facilities.
- To ensure that the density, mass and scale of residential development is appropriate to the location, role and neighbourhood character of the specific housing diversity area.
- To ensure that the siting and design of new residential development takes account of its interface with existing residential development on adjoining sites.
- To ensure that the design of new residential development is sensitive to and respectful of the scale of existing residential development on adjoining sites.

Strategies for Neighbourhood Activity Centres

- Recognise neighbourhood centres as locations which provide significant opportunities for housing diversity, but at a lesser scale and density than developments in urban villages and the Phoenix Precinct.
- Recognise that different development outcomes are sought in the commercial and residential areas of neighbourhood centres.

Strategies for commercial areas of centres (amongst others)

- Ensure that the density, mass and scale of development is appropriate to the scale, character and physical size of the neighbourhood centre.
- Ensure that any new residential development is designed and sited to minimise its adverse impacts on adjoining existing residential properties by way of building bulk, overlooking and overshadowing.

- Ensure that residential buildings are encouraged to step down at the rear to achieve a transition to residential areas.
- Ensure that building heights are compatible with their site context and the wider neighbourhood centre.
- Ensure that where the new building is greater in height than the prevailing building height or where significant changes in building height are proposed for residential buildings:
 - There is a graduated transition in building height between the proposed building and adjoining buildings.
 - The resulting height, mass and scale of the building does not dominate or visually intrude on the streetscape and takes account of views from the wider neighbourhood and at a distance.
 - The upper storeys are recessive so that the visibility of upper storeys is reduced when viewed from the footpath opposite or residential properties to the rear.
 - Provision be made for the establishment of new landmark buildings on strategic redevelopment sites that are made available through the removal of a level crossing.
- Ensure that the retail function and active frontage of centres is retained on the ground floor.

Strategies for the residential areas of centres (amongst others)

- Recognise that these areas offer opportunities for multi-unit development, but at a lower scale and density than development in the commercial and mixed use areas of neighbourhood centres.
- Ensure that the density, mass and scale of residential development is appropriate to that of the neighbourhood centre.
- Encourage a decrease in the density of residential development as the proximity to the commercial area of the neighbourhood centre decreases.
- Ensure that the siting and design of residential development responds positively to its interface with existing residential development in minimal change areas.

1.5 PLANNING ZONES

1.5.1 CLAUSE 34.01 COMMERCIAL 1 ZONE (C1Z)

The NAC is predominantly included within the Commercial 1 Zone the purpose of which is (amongst others) to support vibrant mixed use commercial centres for retail, office, business, entertainment and community uses, and additionally provides for residential uses at densities complementary to the role and scale of the commercial centre.

The zone contains decision guidelines that ask decision makers to consider certain aspects of development when deciding planning applications. The most relevant decision guidelines for this zone are:

Amenity

- Consideration of the interface with adjoining zones, especially the relationship with residential areas.
- Consideration of the overlooking and overshadowing as a result of building or works affecting adjoining land in a General Residential Zone, Neighbourhood Residential Zone, Residential Growth Zone or Township Zone.
- The objectives, standards and decision guidelines of Clause 54 and Clause 55. This does not apply to an apartment development. For an apartment development, the objectives, standards and decision guidelines of Clause 58.

Built form

- Consideration of the streetscape, including the conservation of buildings, the design of verandahs, access from the street front, protecting active frontages to pedestrian areas, solar access, the treatment of the fronts and backs of buildings and their appurtenances, illumination of buildings or their immediate spaces and the landscaping of land adjoining a road.

Transport

- Considerations relating to the movement of pedestrians and cyclists, and vehicles providing for supplies, waste removal, emergency services and public transport.

1.5.2 CLAUSE 32.08 GENERAL RESIDENTIAL ZONE (GRZ1)

Two allotments within the south-east portion of the Caulfield Park Neighbourhood Activity Centre are included within Schedule 1 to the General Residential Zone (see Figure 5). The General Residential Zone encourages a diversity of housing types and housing growth particularly in locations offering good access to services and transport, and additionally to allow educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

Schedule 1 to the General Residential Zone is designated 'Neighbourhood Centre Areas'.

In respect to dwellings and residential buildings on land subject to Schedule 1 to the GRZ, residential developments must not exceed a height of 11 metres.

The relevant decision guidelines of the General Residential Zone and Schedule1 ask decision makers to consider:

Residential development

- For the construction and extension of two or more dwellings on a lot, dwellings on common property and residential buildings, the objectives, standards and decision guidelines of Clause 55. Clause 55 addresses issues such as neighbourhood character, amenity impacts and on-site amenity.
- Whether subdivision will result in development which is not in keeping with the character and appearance of adjacent buildings, the streetscape or the area.

Non-residential use and development

- Whether the use or development is compatible with residential use.
- Whether the use generally serves local community needs.
- The scale and intensity of the use and development.
- The design, height, setback and appearance of the proposed buildings and works.
- The safety, efficiency and amenity effects of traffic to be generated by the proposal.

All development

- Whether opportunities exist to avoid a building being visually obtrusive through the use of alternative building designs, particularly for developments with overall building heights in excess of 10.5 metres.
- The layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed off-street car parking.

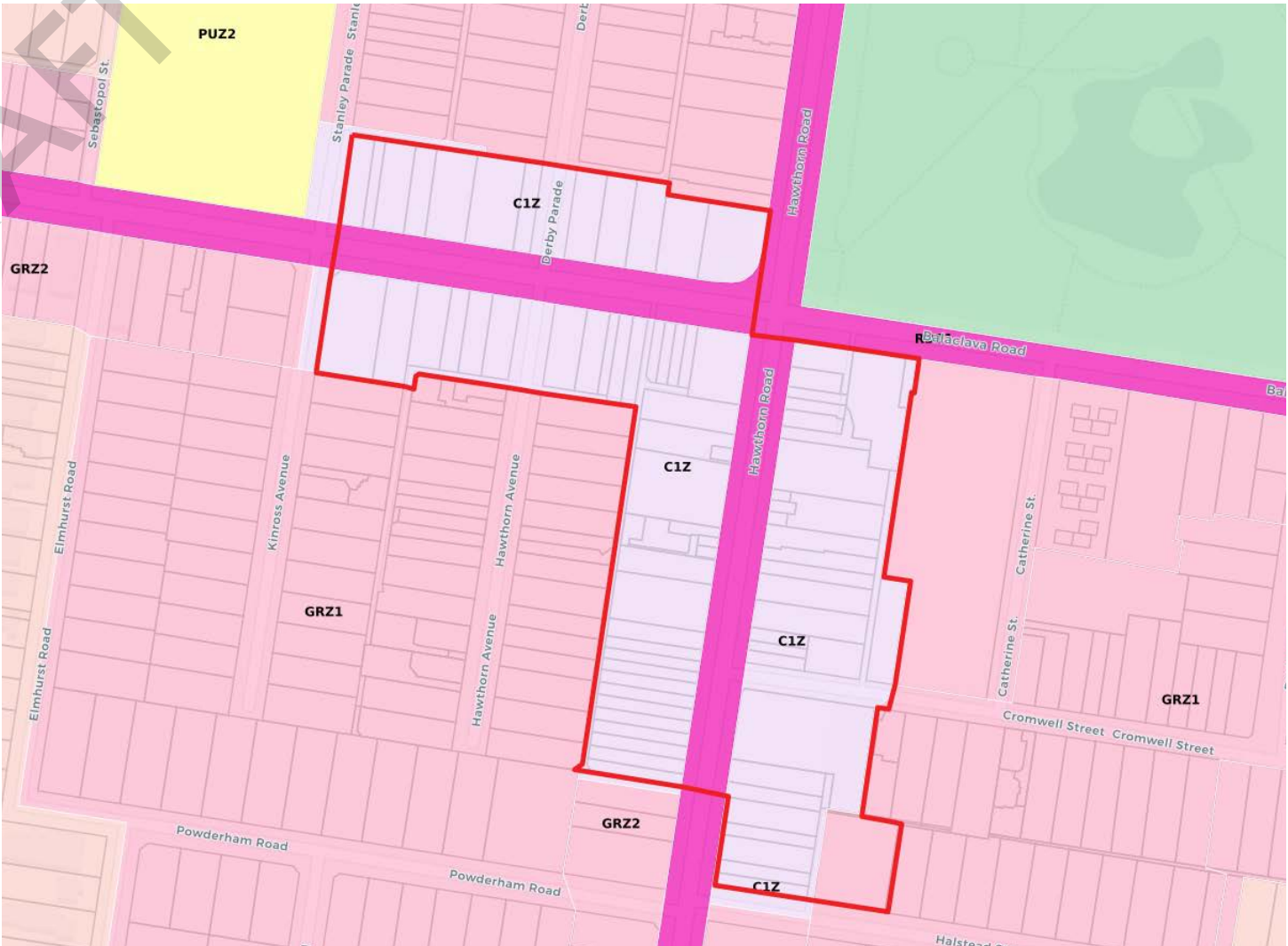


Figure 5. Planning Zones

1.6 OVERLAYS

1.5.3 CLAUSE 44.05 SPECIAL BUILDING OVERLAY

The Special Building Overlay applies to land predominantly north of Balaclava Road, including Balaclava Road. The Overlay identifies land in urban areas liable to inundation by overland flows from the urban drainage system and seeks to ensure development maintains the free passage and temporary storage of floodwaters, minimises flood damage, is compatible with the flood hazard and local drainage conditions and will not cause any significant rise in flood level or flow velocity.

Applications are assessed against a set of decision guidelines and are referred to the relevant floodplain management authority for comment.

1.5.4 CLAUSE 45.09 PARKING OVERLAY (PO2)

Schedule 2 to the Parking Overlay applies to the entire NAC. Schedule 2 to the Parking Overlay is titled 'Student Housing in Specific Areas'. Where student housing is proposed in this area, the schedule aims to provide car parking at a rate commensurate with the reduced ownership pattern of students, allowing a reduced rate of on-site car parking of 0.4 spaces per student housing bed provided.



Figure 6. Special Building Overlay Plan



Figure 7. Parking Overlay Plan

1.7 RECENT DEVELOPMENT CONTEXT

The Caulfield Park NAC is a desirable location for more intense development, particularly residential. It is well-located, well serviced by public transport and offers a range of services and facilities. Planning policy at both the state and local level support the more intense development of this type of centre.

As a result, several development applications have been applied for, approved or constructed. Without detailed built form guidance in place, resultant development can appear ad-hoc and potentially not in keeping with the vision for a particular centre.

Currently more intense development has been centred on the Caulfield South NAC with some activity also occurring in the Caulfield Park NAC. Given the policy direction to locate more intense development within neighbourhood activity centres, it is prudent to develop detailed guidance for all the neighbourhood activity centres.

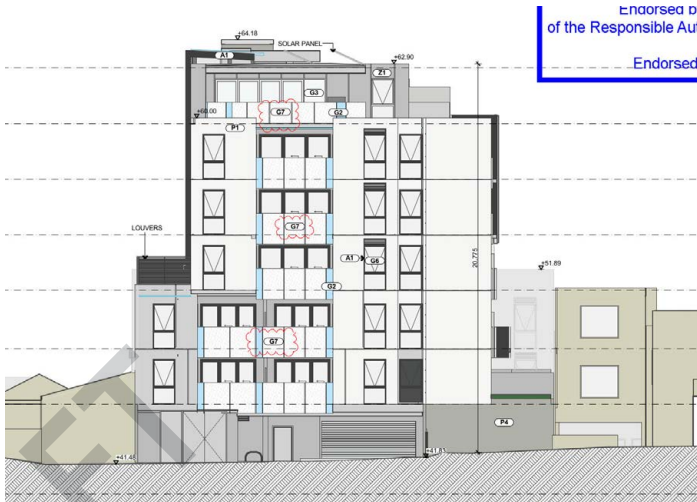
Notable development in the Caulfield Park NAC includes:

67-77 Hawthorn Road, Caulfield North

An application for an eight storey building was refused by Council. The permit applicant appealed to the Victorian Civil and Administrative Tribunal (the Tribunal) which approved the building at seven levels based on amended plans substituted at the hearing. In its decision, the Tribunal found that there was strong policy support for this kind of development in this location and that the height of the building was appropriate given the overall form and strategic context of the site.

219-229 Balaclava Road, Caulfield North

An application for a seven storey building was submitted to Council. At its meeting on 24th November 2020 Council resolved to support the development on the condition that the height of the building is reduced to five storeys.



67-77 Hawthorn Road, Caulfield North



219-229 Balaclava Road, Caulfield North

2 ANALYSIS, ISSUES AND OPPORTUNITIES

The following chapter outlines analysis, issues and opportunities to be addressed through the development of the Built Form Framework for the Caulfield Park NAC.

2.1 TOPOGRAPHY

Figure 8 reveals the elevation of landform within the NAC and surrounds, and identifies key topographic features.

The landform of Caulfield Park is reasonably flat with a small fall of approximately 4m from the eastern edge of the centre to the west. Both Hawthorn Road and Balaclava Road are flat making the centre highly walkable.

There is notable rise to the east along Balaclava Road towards Caulfield Park. This provides for views into the park and also to the St Aloysius Church. Views from these locations back to the NAC will be important to consider.

Buildings on the south side of Balaclava Road opposite the park will be slightly more prominent in the NAC because of the rise in elevation.

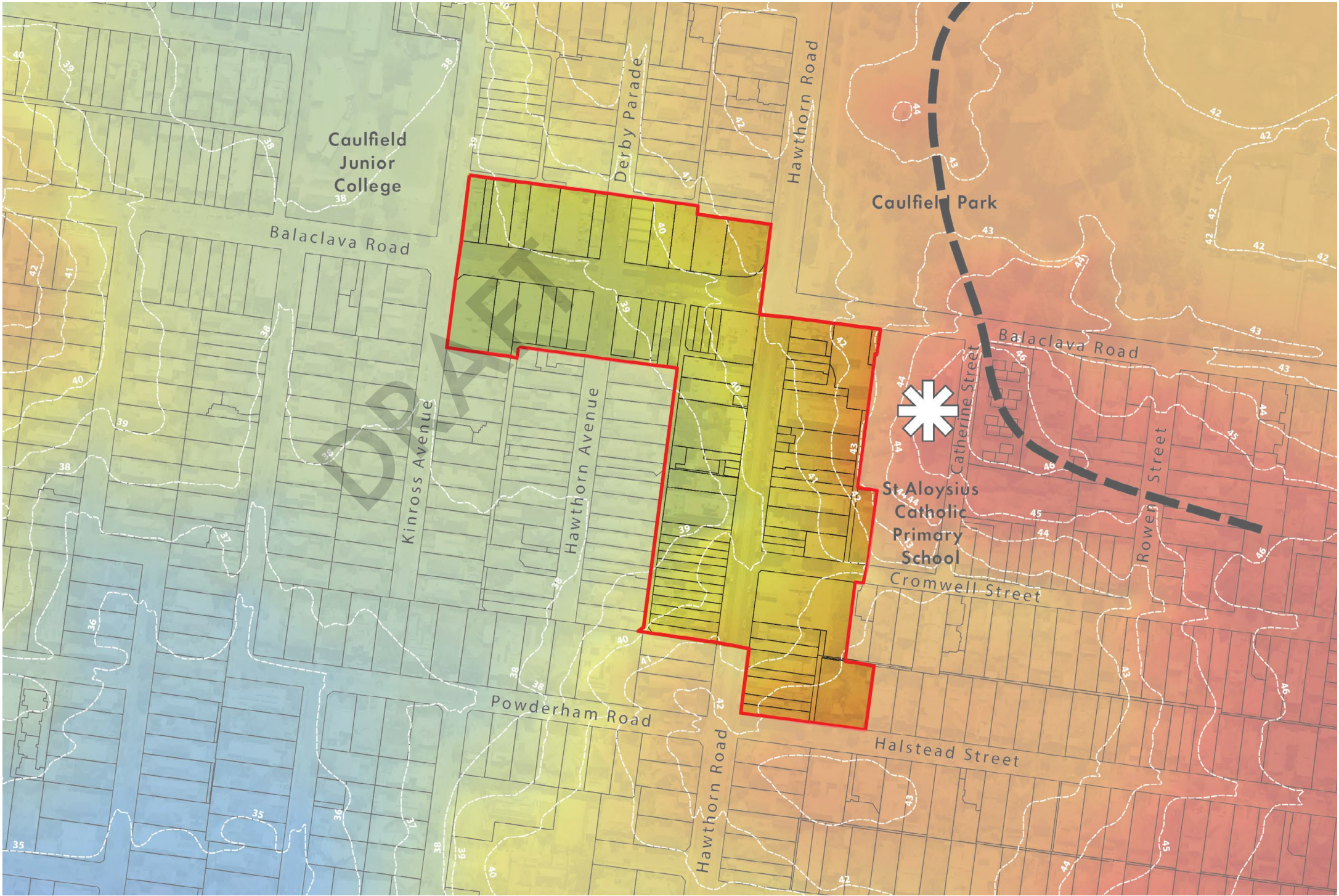


Figure 8. Elevation Plan

Figure 9 provides an assessment of land slope within the NAC.

The land slope across properties on Hawthorn Road ranges between 2.5% and 7.5% whereas properties along Balaclava Road are flatter with slope around 0-2.5%.

There is more significant slope at the eastern edge of the study area adjoining the St Aloysius Catholic Primary School. This evidenced by the retaining walls along the laneway in this location.

Overall, the existing land slope will present minimal constraints to development in the NAC.

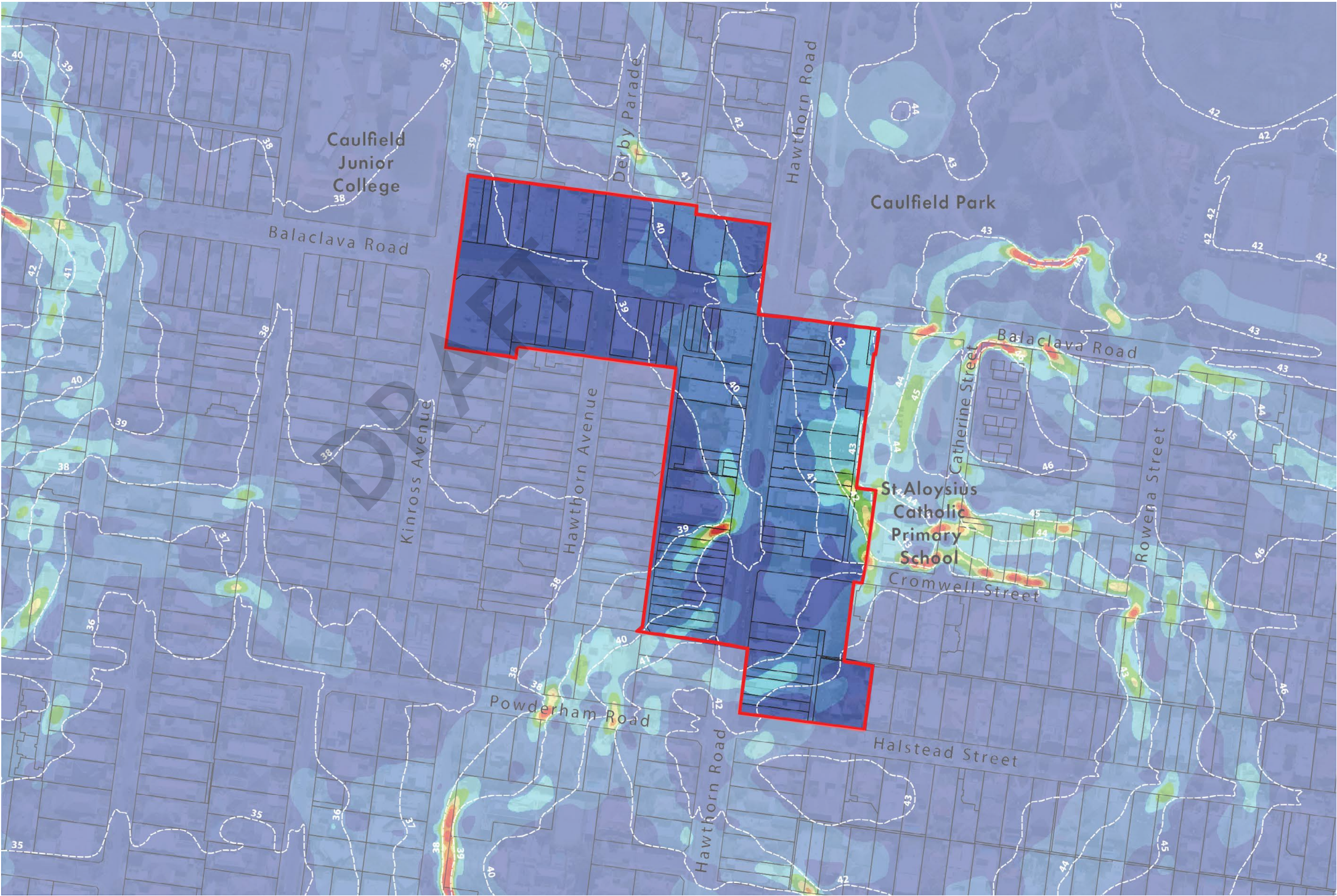
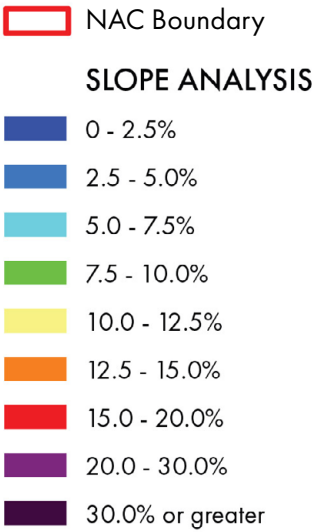


Figure 9. Slope Analysis

2.2 LAND USE & INTERFACES

Figure 10 identifies land use precincts and range of interface conditions across the NAC.

The Caulfield Park is NAC a compact Centre anchored by major open space and two schools providing key destinations for the community. It is also very well served by public transport with two tram routes running through the centre.

The centre is broadly divided into three land use areas. The primary retail area is located along Hawthorn Road and extends onto the southern side of Balaclava Road for a short distance. This area comprises a range of retail, hospitality, personal and business service uses provided in small tenancies (see Image 1).

Larger footprint office and warehouse uses are located on Balaclava Road (see Image 2) and a smaller office area is located at the southern end of Hawthorn Road. These areas are less active than the primary retail area and provide some undesirable frontages to the footpaths (see Image 3).

There are several recently constructed mixed use developments in the NAC on Hawthorn Road which include retail, office and residential uses. These developments help to activate the streets by providing more people living in the centre and retail uses at ground level.

The NAC has a range of interface conditions with adjoining land uses. The predominant interface is to residential uses which are located along the northern, western, eastern and southern boundaries. There is a mix of laneways separating residential and commercial uses and direct abuttal between the uses.

Other interfaces include two schools, the St Aloysius Church, which is protected by a heritage overlay and Caulfield Park.

2.1.1 KEY ISSUES AND OPPORTUNITIES

- There are opportunities to enhance Balaclava Road and the southern end of Hawthorn Road through new, mixed use developments that provide more active ground level uses.
- The blank wall at north-west corner of Hawthorn Road / Balaclava Road junction creates a poor street address at a key location (see Image 4). There is an opportunity to improve this frontage.
- The residential interface to the south, west and east provide constraints for future building heights and setbacks. Development will require a sensitive design response to transition to adjoining residential areas.
- The activity centre's eastern boundary interfaces with St Aloysius Catholic Primary School and the St Aloysius Church (see Image 5), which is protected by a Heritage Overlay. This will require a sensitive design response regarding height, setbacks and solar access.



Image 1. Retail activity is focused along Hawthorn Road in small tenancies



Image 2. Office and commercial uses are focused along Balaclava Road in larger footprint buildings



Image 3. There is an opportunity to improve the poor street address on Derby Parade if the site is redeveloped



Image 4. There is an opportunity to improve the street address and blank walls at the junction of Balaclava Rd & Hawthorn Rd if the site is redeveloped

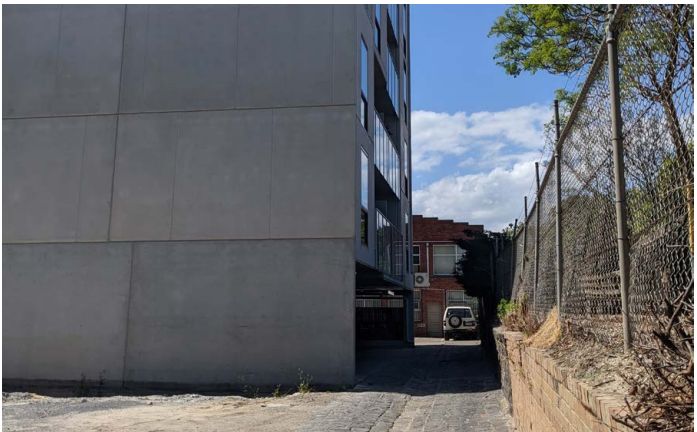


Image 5. Mixed use Laneway interface with St Aloysius Catholic Primary School playground. The retaining wall along the edge of the laneway provides visual separation.

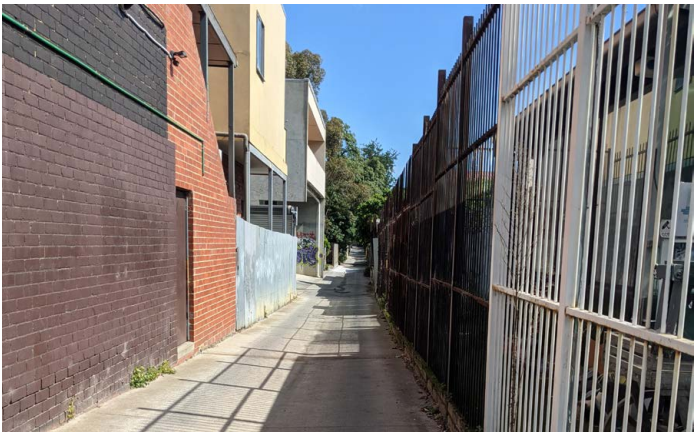


Image 6. Existing laneway from Balaclava Road with commercial uses located either side.

 NAC Boundary

LAND USES

-  Primary Retail
-  Office / Commercial / Secondary Retail
-  Mixed Use - Redevelopment Sites
-  Open Space

INTERFACES

-  Residential Interface - Laneway Separation
-  Residential Interface - Direct Abuttal
-  Open Space Interface
-  School/Community Interface
-  Poor Street Address / Interface

DESTINATIONS





-  Key Public Space
-  School
-  Open Space
-  Tram Stop



Figure 10. Land Uses and Interfaces Analysis

2.3 BUILT FORM CHARACTER & PUBLIC REALM

Figure 11 maps a range of built form character and public realm elements across the Caulfield Park NAC.

Understanding the existing and emerging built form and character of the Caulfield Park NAC is important in order to identify character elements that should be protected, how built form could be improved and assess the impact of new, taller development on the streetscapes.

The Caulfield Park NAC has a local feel reinforced by the low scale and narrow shopfronts, which are predominantly 1-2 storeys. This fine-grain character is focused along Hawthorn Road and a small part of Balaclava Road (see Image 7).

The built form character changes along Balaclava Road. Larger floorplate office buildings and warehouses provide less engagement with the footpath (see Image 8). These buildings tend to have bulkier forms and less parapet articulation than the original shopfronts on Hawthorn Road (see Image 9).

There is a row of Tudor Revival style buildings at 158-166 Hawthorn Road (see Image 10), which bookend the southern edge of the centre. These buildings make an important contribution to the character of the centre and have been recognised for their heritage significance by being included in a proposed Heritage Overlay.

New development has occurred in the centre and is focused along Hawthorn Road. These buildings extend up to seven storeys however the building height at the street is typically three storeys. The impact of these buildings on the NAC is mixed. Because they are much taller than the existing built form they are very prominent when looking along Hawthorn Road (see Images 25 and 26). The choice of materials and lack of upper level articulation on some of these buildings detracts from these views. However the three storey street wall makes a positive contribution to the streetscape particularly where basement car park entries and services are provided from laneways rather than Hawthorn Road.

Existing awnings provide weather protection to footpaths along the majority of Hawthorn Road within the core retail area. Weather protection is however limited along Balaclava Road. This is a reflection of the existing land uses which tend to be retail and hospitality focused along Hawthorn Road and office focused on Balaclava Road (see Image 11).

Although the centre benefits from major open space close by, the footpaths along Hawthorn Road and Balaclava Road are key public spaces. They have good access to sunlight and provide good opportunities for on street dining and gathering (see Image 12).

Another key gathering place is Caulfield Park and in particular the square that is located at the south western corner of the park. This is located at a prominent junction and provides a key vantage point for looking back to the NAC.

2.3.1 KEY ISSUES AND OPPORTUNITIES

- The existing one to three storey shop-fronts provide a good sense of 'human scale' to the streets. There is an opportunity to retain a similar scale and set back taller building elements in future developments to reduce their visual impact on the streetscape.
- The corner of Balaclava Road and Hawthorn Road is a significant location which is visually prominent from Caulfield Park. There are opportunities to enhance this corner with high quality buildings.
- Recent development along Hawthorn Road provides a street wall height of three storeys, which maintains a scale that does not overwhelm the footpath. There is an opportunity to continue this height in new development where appropriate (see Image 13-15).
- The proposed heritage buildings along Hawthorn Road should be retained and new development should ensure their prominence in the streetscape is maintained.
- There are opportunities to continue the fine-grain character in new developments to provide visual interest and diversity of activity to the street.
- It is important to maintain solar access to key pedestrian street in the activity centre particularly the east and west footpaths on Hawthorn Road. This should also be extended to southern footpath of Balaclava Road, which will play a more important role for pedestrians as land use across the activity centre intensifies.
- There are a number of laneways running parallel to Hawthorn and Balaclava Road. These could provide service and car park access to future development in order to minimise driveway disruptions to footpaths and maintain continuous active frontages.
- As the existing laneways are typically only 3 metres wide and do not support two way traffic, this may present constraints in the long-term for car parking and service access.



Image 7. The fine grain retail on Hawthorn Road is an important part of the NAC's character providing for a diversity of land uses. This character can be reinforced in new developments



Image 9. Balaclava Road has a different character due the larger format office uses. These buildings are bulkier in their forms and have limited vertical modulation.

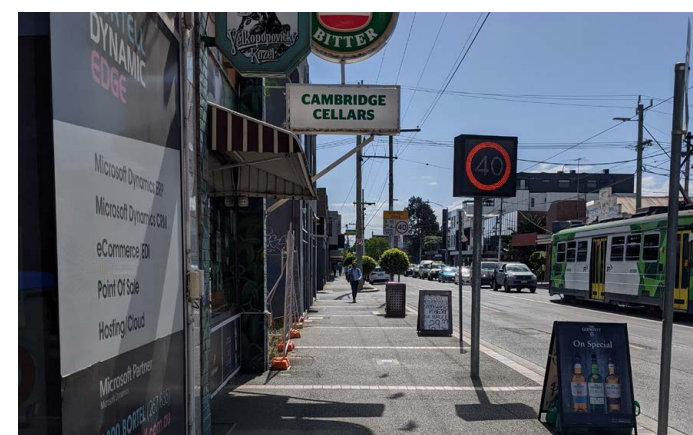


Image 11. The lack of awnings along Balaclava Road is a reflection of the predominant office uses in the area. There is an opportunity to provide weather protection when buildings are redeveloped.



Image 8. Office uses along Balaclava Road provide limited engagement with the footpath. There is an opportunity to enhance this interface if redeveloped.



Image 10. The Tudor style architecture buildings at the southern entry to the centre are important and will be protected through a future heritage overlay. Their prominence in the streetscape should be retained if new development occurs.



Image 12. Footpaths on Balaclava Road near the intersection of Hawthorn Road. Outdoor dining uses are taking advantage of the sunlight provided by the northern aspect.



Image 13. Recent five storey building on Hawthorn Road. Upper levels are recessed and well articulated to the front and side boundaries



Image 14. Recent mixed-use development of seven storeys on Hawthorn Road. The heavy use of glass and lack of horizontal articulation make the upper levels appear very prominent and out of context with the streetscape



Image 15. Recent mixed use development of five storeys at the corner of Balaclava Road and Stanley Parade. The upper level setbacks reduce the visibility of levels four and five from the street

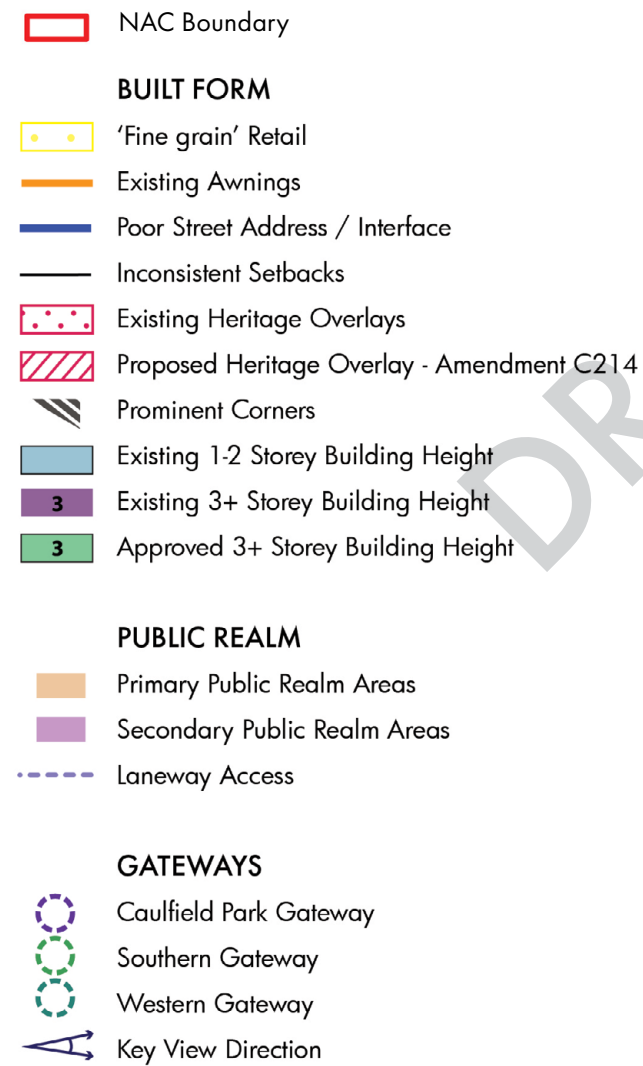


Figure 11. Built Form Character & Public Realm Analysis



Image 22. Panorama of Hawthorn Road and Balaclava Road Intersection from Caulfield Park. This view highlights the prominence of the corner buildings.

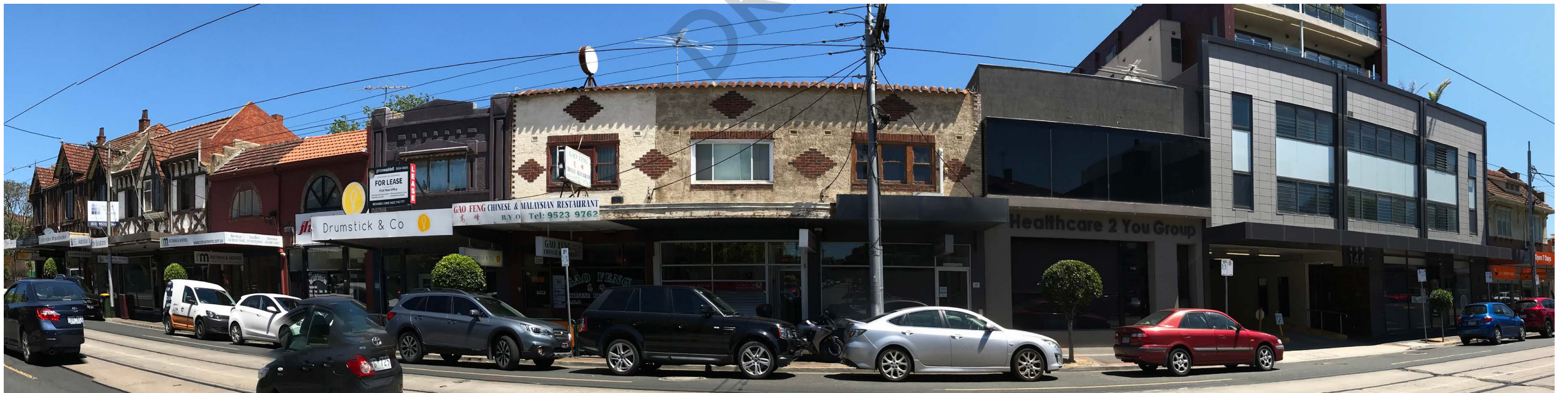


Image 23. Panorama of the west side of Hawthorn Road. This view shows the proposed heritage overlay buildings bookending the southern entry to the NAC. There is a consistent two storey street wall with the exception of the newer development at the right side of the image.



Image 24. Panorama from Caulfield Park. Buildings on the south side of Balaclava Road are very prominent from this view.



Image 25. View looking north along Hawthorn Road towards Caulfield Park. The recessed upper levels on the east side of Hawthorn Road are effective and retain a lower scale street wall.



Image 26. View along Hawthorn Road looking south. The recent development is very prominent in this view. The vertical articulation and darker materials make the upper levels appear more dominant.

2.3.2 EXISTING SHADOW ANALYSIS

Figure 13 demonstrates the shadows cast by existing buildings within the Caulfield Park NAC. Development above 3 storeys and recent approvals are shown separately in green.

The date selected for the shadow analysis is September 22 - the equinox. This represents the mid-point between the summer solstice and winter solstice. It is the date commonly used in planning assessments to assess the impact of shadows.

The analysis demonstrates that more recently developed buildings on Hawthorn Road shadow the footpath from 3pm onwards at the Equinox. The lower scale shopfronts have minimal shadow impact on footpaths.

The analysis also demonstrates there is minimal shadow impact on existing residential areas from commercial buildings.

The 3D model for the shadow analysis has been developed using PSMA Geoscape building footprint data which provides an outline of the building footprint and overall roof height. The building footprints have been extruded up to the overall roof height and integrated into a topographic model.




-  NAC Boundary
-  Approved / Constructed Development of 3 storeys or greater
-  Shadow from existing and approved buildings



Figure 13. Existing Shadow Analysis







2.4 DEVELOPMENT ACTIVITY AND CAPACITY

Figure 15 maps recent development activity within the Caulfield Park NAC, sites that are constrained for development and sites that present good development opportunities.

Development activity and capacity is an important consideration in preparing the Built Form Framework. Recent development can indicate the type of development that the centre will attract in the future. Analysing the potential capacity of the centre is also important to understand the scale and type of development that could potentially be accommodated.

Development activity within the Caulfield Park NAC has been focused along Hawthorn Road. There has been one recent development of seven storeys (see image 28), two developments of five storeys. Within the context of a small strip, these developments have introduced some noticeable changes to its character.

Another notable recent development is located at the western end of the centre on Balaclava Road. This development comprises five storeys and provides a high quality address to two street frontages.

2.4.1 KEY ISSUES AND OPPORTUNITIES

- The mapping opposite identifies areas along Hawthorn Road and a small section of Balaclava Road where future development may be constrained, due to lot size, width and depth. This also encompasses the proposed Heritage Overlay which will apply to 158-166 Hawthorn Road (see Image 29-30).
- There are a number of sites that present good opportunities for development based on lot size. These include larger office and warehouse sites on Balaclava Road and some sites along Hawthorn Road. These sites generally benefit from laneway access or a secondary street frontage which can assist with future vehicle and service access.
- The Commercial area is surrounded by properties in the General Residential Zone which allows for development of 3 storeys (11 metres). It will be important for future building heights to transition sensitively to the surrounding residential areas.

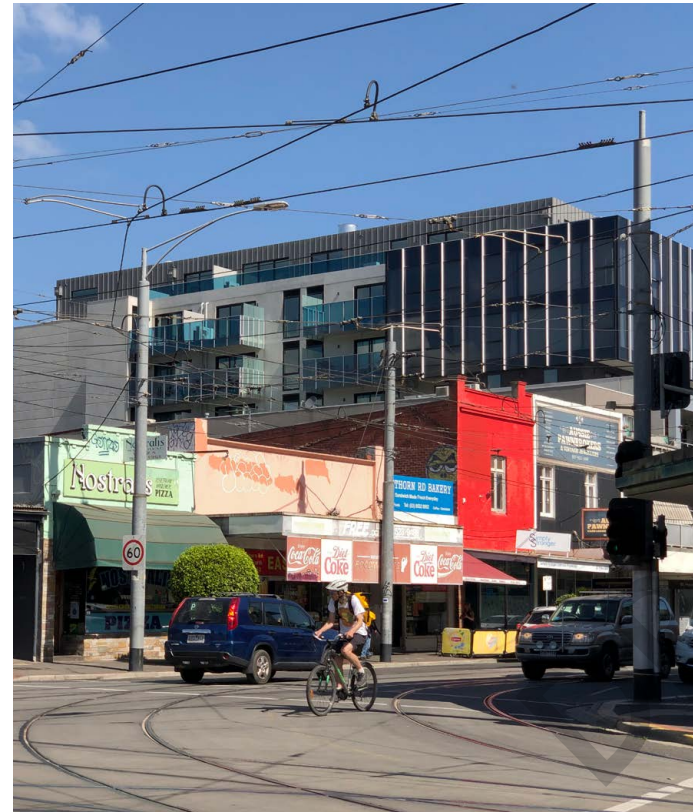


Image 27. A recent seven storey development on Hawthorn Road. This development sits next to sites that are considered to be constrained for development because of their depth.



Image 28. The heritage buildings at the southern edge of the centre should be retained and will present a constraint for future development. These lots are also narrow in width.



Image 30. Larger office buildings along Balaclava Road that could potentially be redeveloped with higher quality built form that provides activation to the street.



Image 29. Smaller sites on Balaclava Road that may be constrained for development unless they are consolidated.

2.4.2 LOT WIDTH ANALYSIS & TYPOLOGIES

Figure 16 provides analysis of lot widths across the Caulfield Park NAC and Figure 17 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot width.

The typology analysis has been undertaken on the basis of a residential development outcome. Commercial development would be less constrained with less of a need to provide daylight into habitable rooms.

Front and rear facing apartments provide the simplest development outcome for narrow lots, as they can facilitate 1-2 apartments facing the street, and one facing the rear of the property, on each floor (subject to width). This type of development occupies the entire width of the lot, and does not provide any articulation or activation to either side of the development (which is assumed will develop in a similar manner).

Wider properties can accommodate a different model of development, podium and tower, which can have an outlook to other developments either side. This type of development can provide for a greater range of apartment types with varying aspects. It does however create issues of apartment separation for privacy and daylight that need to be considered.

The analysis indicates that there are a small number of lots on Hawthorn Road and Balaclava Road that would not support apartment development in their current configuration. However the majority of the centre includes lots wide enough to provide apartment development and a small number supporting side facing apartments.

While this analysis identifies potential development outcomes for individual properties, it does not take into consideration the potential for multiple properties being consolidated, which often occurs in Activity Centres.

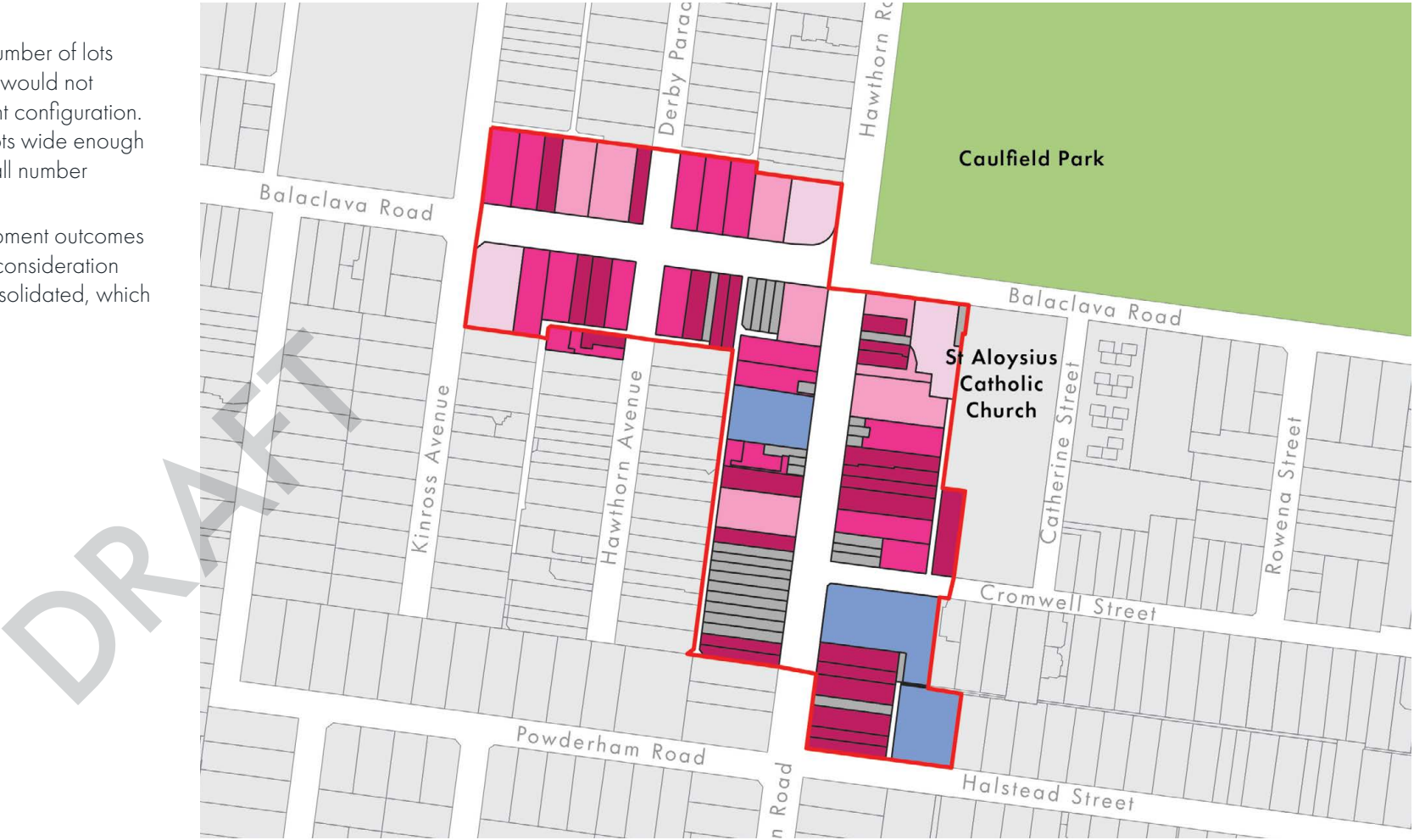


Figure 16. Lot Width Analysis

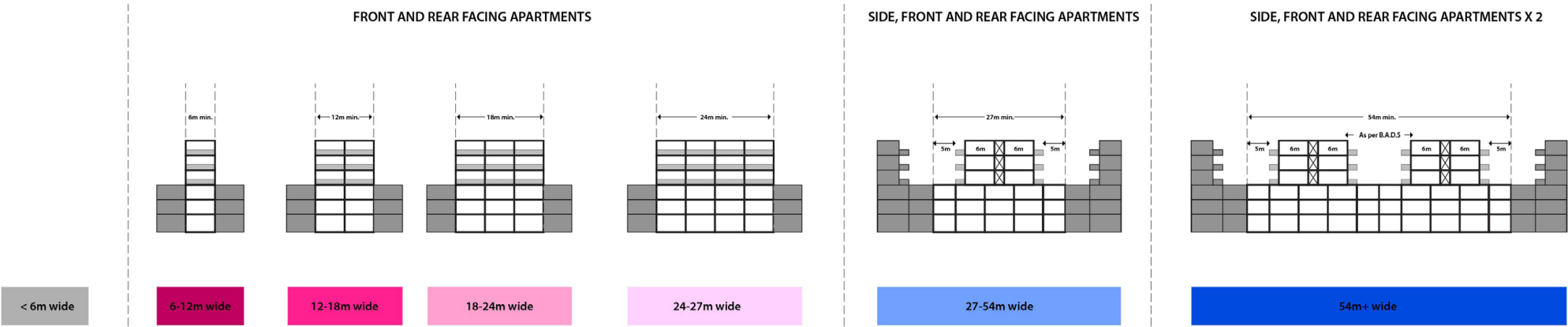


Figure 17. Lot Width Typologies

2.4.3 LOT DEPTH ANALYSIS & TYPOLOGIES

Figure 18 provides an analysis of lot depths across the NAC and Figure 19 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot depth.

Similar to lot width, the depth of a property has an impact on the type of development that can be provided as shown on the typologies opposite.

The typologies indicate that shallow lots provide a limited opportunity for development, and allow for only single aspect apartments. This takes into account a typical setback to a residential interface, and a suitable upper level setback to the primary street frontage.

Lots which are deeper provide opportunities for dual aspect apartments, addressing the primary street, and the rear of the lot.

Lots greater than 55m (approx.) could allow for two buildings with both front and rear facing apartments. This outcome does create apartment separation issues that need to be considered.

Lot depths in Caulfield Park are fairly consistent and would generally support front and rear facing apartments. There are small areas of shallow lots that would support single aspect apartment buildings, however some of these properties interface with commercial uses so rear setbacks may not be as onerous.



Figure 18. Lot Depth Analysis

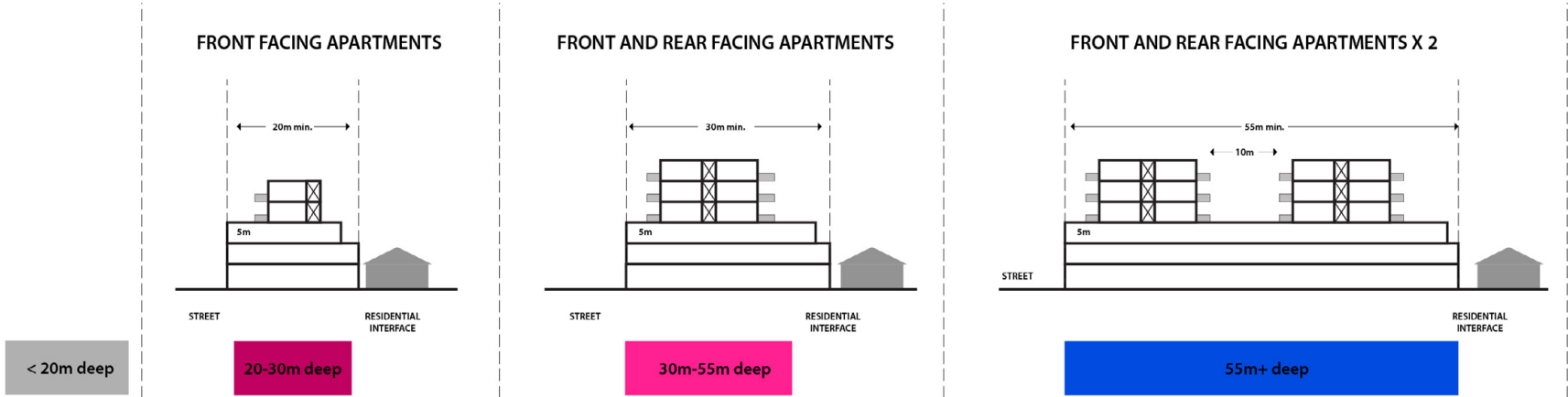


Figure 19. Lot Depth Typologies

PART 2 - THE BUILT FORM FRAMEWORK

3 DESIGN PRINCIPLES

3.1 THE PRINCIPLES

4 BUILT FORM FRAMEWORK

4.1 BUILT FORM FRAMEWORK

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

4.3 PRECINCT 1 - HAWTHORN ROAD

4.4 PRECINCT 2 - BALACLAVA ROAD





Home Delivery
& Catering
PH:9532 9566

CROMWELL ST.
ST. ALFRED'S
PRIMARY SCHOOL

macco bi

WOLFSTEP COURT

BCA 176

3 DESIGN PRINCIPLES

The following over-arching principles have been prepared to guide the drafting of design objectives and requirements for the Caulfield Park NAC. The principles aim to respond to the issues and opportunities identified in Part 1 relating to amenity and character and an understanding of the centre’s strategic role in meeting future housing and employment needs.

3.1 THE PRINCIPLES

PRINCIPLE 1 - A VIBRANT NEIGHBOURHOOD CENTRE WITH A MID-RISE CHARACTER THAT VALUES HERITAGE

MID-RISE CHARACTER

The approach for Caulfield Park is to establish a mid-rise character providing building heights in the 4-6 storey range. These heights will support further growth of the centre whilst being compatible with the surrounding lower scale residential context and heritage within the centre.

Glen Eira includes a mix of both Major Activity Centres (MACs) and Neighbourhood Activity Centres. The MACs play a significant role for the community providing a number of public transport options, and access to retail, services and employment opportunities. The NACs play an important, though less significant role in meeting future needs for housing and employment as they are smaller in size with less retail and employment opportunities. They tend to serve the surrounding local neighbourhoods.

The Glen Eira City Plan outlines the preferred focus for the NACs and notes that they will have a ‘medium’ focus for employment growth and a ‘moderate’ focus for housing growth. Caulfield Park is identified as a Neighbourhood Activity Centre and as such is intended to experience moderate growth.

The Caulfield Park NAC is somewhat constrained for development. It includes a small heritage precinct, some areas of smaller lots, a residential interface along most of its edges and key footpaths which need to be protected from overshadowing. Combined together, these constraints will limit development opportunities.

PROTECT AND ENHANCE HERITAGE

Caulfield Park NAC includes a proposed heritage precinct at the southern end of the centre and the existing heritage protected St Aloysius Church. The Built Form Framework will ensure the prominence of these heritage buildings is maintained in the centre.

This will be achieved by matching the street wall height of existing contributory and individually significant heritage buildings within the proposed heritage precinct, providing lower overall building heights and adequately recessing upper levels to ensure the street wall is the dominant element in the streetscape. Additional design details will ensure the new buildings and additions do not compete with the heritage facades.

For the St Aloysius Church, a transition in scale and upper level setbacks to the church will ensure it remains a dominant feature.



PRINCIPLE 2 - AN ENJOYABLE AND SAFE PLACE TO SHOP, WORK, GATHER, DINE AND EXERCISE

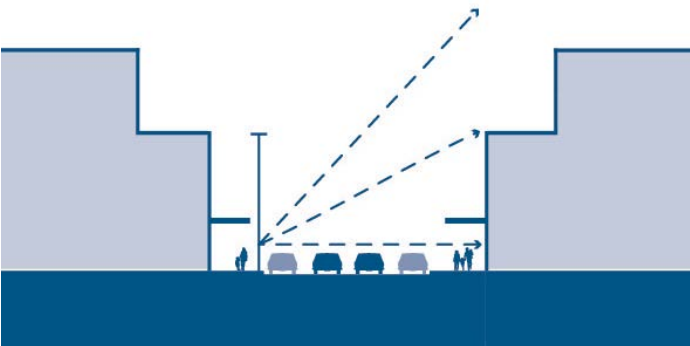
REINFORCE THE HUMAN SCALE TO KEY STREETS

Existing buildings within the Caulfield Park NAC are generally low scale, between one and two storeys in height. The low scale character of the centre will change over time and Hawthorn Road has experienced a number of recent, higher scale developments.

It is important that new, taller buildings are designed in a way that integrates with the existing low scale of the centre and do not dominate the streetscape. This is possible by providing a lower scale building at the street edge and setting the taller elements further behind.

A two to three storey building height at the street is recommended for the Caulfield Park NAC, which will maintain a scale that is both compatible with existing heritage and non-heritage shopfronts, and does not overwhelm the streetscape. This has been evidenced with recent developments in the NAC that provide a three storey street wall.

Another aspect to the human scale in the Caulfield Park NAC is the ‘fine-grain’ narrow shopfronts, which provide visual interest and provide for a greater diversity of uses and experiences. It is recommended this character is continued through new developments across the centre.

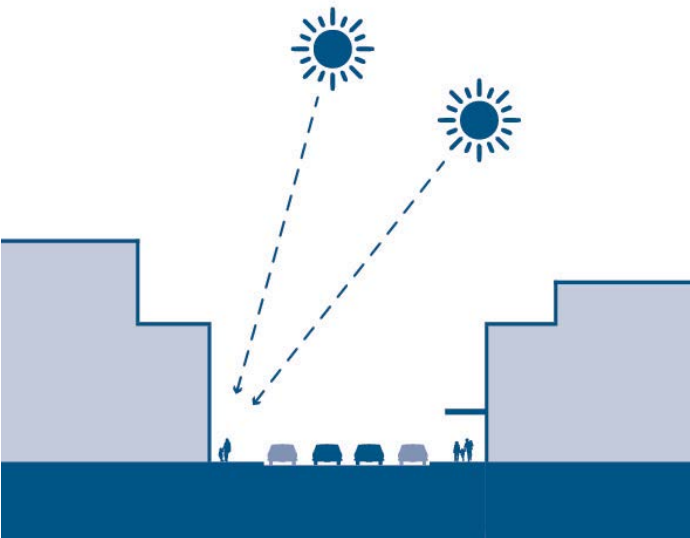


MAINTAIN SOLAR ACCESS TO KEY FOOTPATHS

The Caulfield South Park NAC’s footpaths play an important role as spaces for people to gather and interact. Maintaining sunlight to key footpaths is important to encourage outdoor dining and street based retail, and support the vitality of the centre.

The Built Form Framework Plan for the Caulfield Park NAC identifies the key footpaths where access to sunlight is considered to be important. This includes the eastern and western footpaths of Hawthorn Road, where retail and hospitality activity is currently focused, and the southern footpath of Balaclava Road where there is a greater amount of office based activity.

Sunlight access is often measured at the equinox (22 September) in Planning Schemes across Victoria. This date sits mid-point between the winter solstice (June 22) where shadows are at their longest, and the summer solstice (December 22) where shadows are at their shortest. For the Caulfield Park NAC Built Form Framework, the Equinox was selected as the date to measure solar access.



The following measures for solar access have been adopted for the Built Form Framework. These time periods will ensure sunlight is provided to the footpaths at the most active times of the day, which will help to support hospitality and retail uses.

These measures were tested and considered to provide a balance between providing good solar access whilst not unreasonably limiting development opportunities in the centre.

Maintain sunlight to key footpaths as follows:

- Southern footpaths of Balaclava Road - Solar access from 10am on September 22
- Western footpaths of Hawthorn Road - Solar access from 10am - 12pm on September 22
- Eastern footpaths of Hawthorn Road - Solar access from 12pm - 2pm on September 22

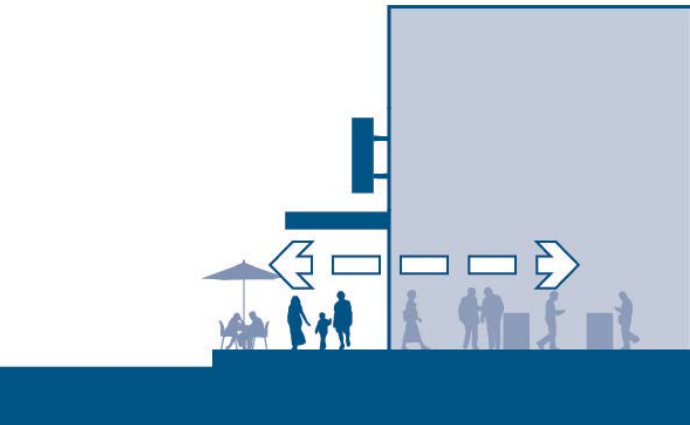
PROVIDE FOR A CONTINUOUS NETWORK OF ACTIVE FRONTAGES WITH WEATHER PROTECTION

Continuous retail and business activity across the Caulfield Park NAC is key to providing a positive pedestrian experience. Locations where there are blank walls, car park or loading areas to the street, disrupt the flow of retail activity and provide limited passive surveillance of the footpaths.

It is recommended that all of Hawthorn Road and Balaclava Road treated as active frontages, with windows at ground level, and uses at the front of the building that provide for customer engagement. Buildings on corners would provide active frontages to both streets. In addition, uses above the ground level are encouraged to address the street with windows and balconies.

All buildings should be constructed with floor to ceiling heights that would support retail, commercial and hospitality uses at ground level. This will allow for buildings to be easily adapted for such uses into the future and further strengthen street based activity.

Existing awnings provide weather protection along the majority of properties on Hawthorn Road within the core retail area. New developments should continue this element along both Hawthorn Road and Balaclava Road, with awnings placed at lowered heights to reinforce an intimate pedestrian environment.



PRINCIPLE 3 - A CENTRE THAT EMERGES SENSITIVELY FROM THE SURROUNDING NEIGHBOURHOOD AND CONSIDERS RESIDENTIAL INTERFACES BOTH WITHIN AND OUTSIDE THE CENTRE

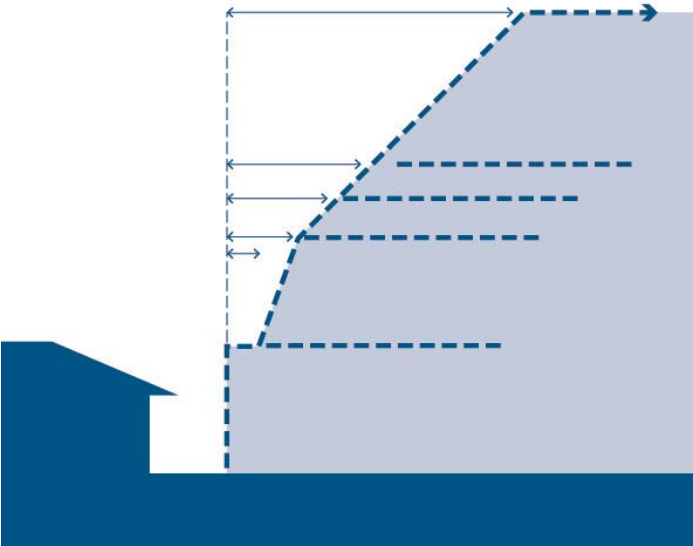
ENHANCE THE RESIDENTIAL INTERFACE

The Caulfield Park NAC abuts residential uses along most of its edges. The interface varies between direct abuttal to a residential property or a laneway separating the uses.

It is important that development in the NAC transitions appropriately to the residential edge to protect the amenity of these areas.

For locations where there is a direct abuttal to residential properties, a single storey built form edge is proposed, which is then recessed to minimise visual impact and reduce the impacts of overlooking and overshadowing. Where a laneway exists, a two storey built form edge is proposed to the laneway which then recesses considerably.

In addition to the upper level setbacks, development will need satisfy relevant requirements from Clause 54, 55 and 58 of the Glen Eira Planning Scheme to further protect residential amenity.

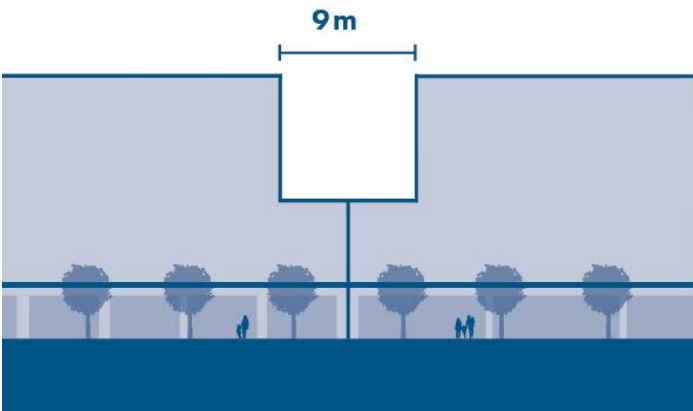


PROVIDE FOR EQUITABLE ACCESS TO AMENITY

Development across the Caulfield Park NAC is likely to be sporadic with potential for new, taller buildings to be located adjacent to existing, low scale buildings for a substantial period of time. It is important to have measures in place to ensure the future development potential of adjoining sites is not significantly compromised by the first development.

A key consideration in equitable access is ensuring adjoining buildings have sufficient separation, to limit overshadowing and ensure adequate privacy for apartments and access to daylight. The centre-wide framework provide for a 9 metre separation distance between apartment developments where they have balconies or windows of habitable rooms facing each other.

However, for 1 to 3 storeys, zero side and rear setbacks are recommended in most cases. Land uses on these levels will typically be retail or office uses where access to sunlight and privacy issues are less critical.



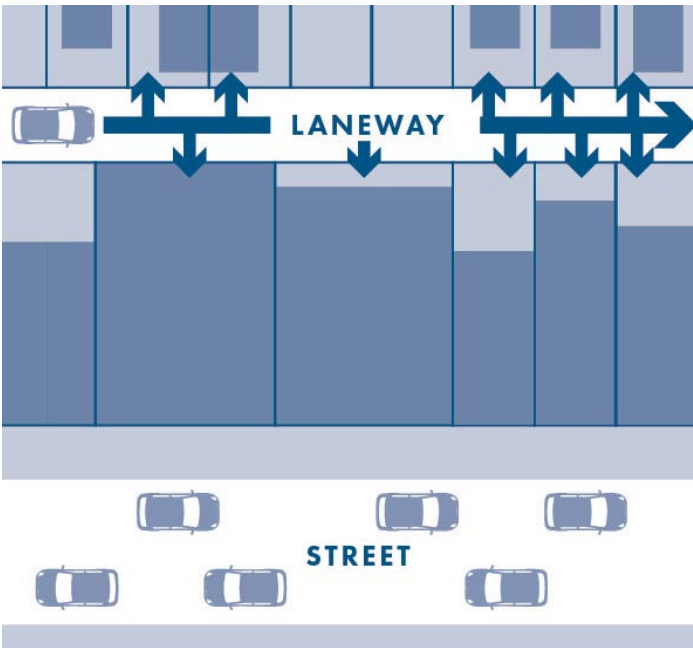
PRINCIPLE 4 - A WELL SERVICED AND ACCESSIBLE CENTRE

ENSURE ADEQUATE SERVICING OF EXISTING AND NEW DEVELOPMENTS

As the Caulfield Park NAC grows and intensifies with additional development, so do its servicing and access requirements for buildings. This includes access to car parking for residents, workers and customers, access for service vehicles, and pedestrian and cycle access.

It is important that new development takes advantage of existing service access arrangements through laneways in order to avoid vehicle disruption to footpaths, and to prioritise ground floor active frontages and land uses rather than access and parking infrastructure. Where no laneway exists, driveway crossovers should be located on secondary frontages and minimised in width.

Another important consideration is ensuring new development can be accessed adequately by pedestrians and cyclists.



4 BUILT FORM FRAMEWORK

This section outlines Centre-Wide Design Objectives and Requirements that will apply to all development within the NAC, and more specific building height, setback and design requirements that apply to two Built Form form precincts identified in Figure 20.

4.1 BUILT FORM FRAMEWORK

Figure 20 - Built Form Framework Plan, identifies the preferred and mandatory heights, setbacks and other built form requirements across the Caulfield Park NAC.

The proposed building heights have been developed through application of the design principles outlined above, responding to key issues and opportunities identified in Part 1 of this report, and testing of the built form outcomes through 3D modelling. The recommended heights have taken into account a number of considerations including responding to sensitive interfaces, ensuring solar access to key footpaths and public spaces, and understanding the development potential of properties based on size and heritage requirements.

This plan identifies the key public realm areas, where good access to sunlight is required. These areas include existing footpaths where there is a high amount of pedestrian activity. Upper level setbacks are recommended at these locations to maintain good access to sunlight.

The strategic justification and rationale for the application of building heights and setbacks in each precinct is discussed in detail in Sections 4.3 and 4.4.

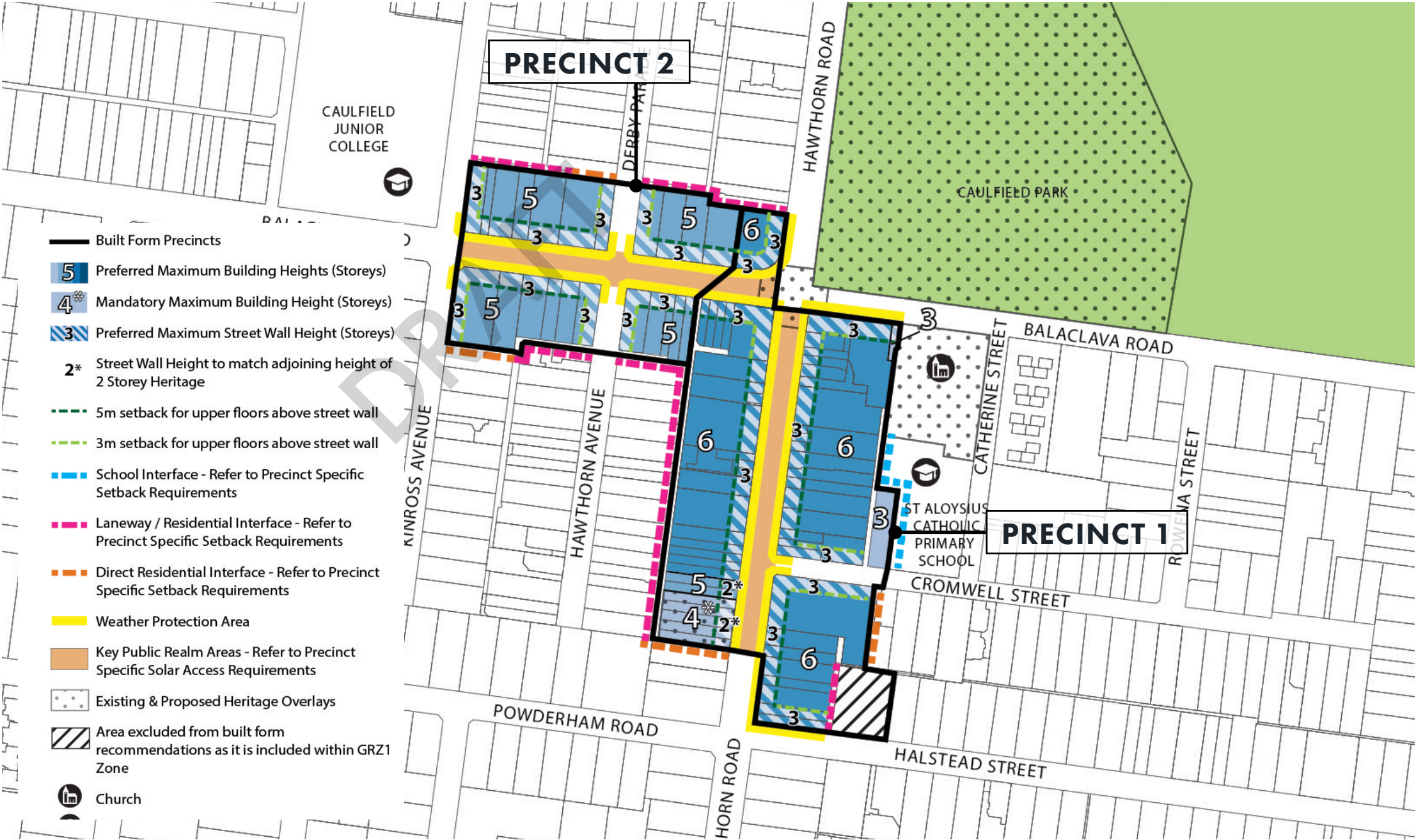


Figure 20. Built Form Framework Plan

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

4.2.1 DESIGN OBJECTIVES

- To support a new mid-rise scale built form character for the centre with lower built form at the street interface and to adjoining residential areas.
- To ensure development maintains the prominence of the heritage buildings at 158-166 Hawthorn Road and respects the heritage of the St Aloysius Church.
- To maintain direct sunlight during the most active hours to key footpaths in order to encourage outdoor dining and street based retail, and support the vitality of the centre.
- To ensure development enhances the prominent intersection of Hawthorn and Balaclava Road with high quality built form that addresses both roads.
- To ensure development enhances the pedestrian experience through improved activation at ground floor and maintaining sunlight to the Hawthorn Road and Balaclava Road footpaths.
- To ensure development protects existing residential amenity and does not overwhelm adjoining residential properties.

4.2.2 DESIGN REQUIREMENTS

Building heights

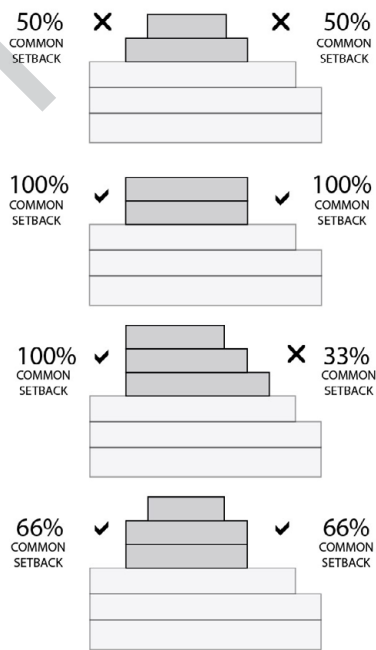
- The preferred maximum building height excludes rooftop services which should be hidden from view from any adjoining public space or designed as architectural roof top features. Roof top services includes but is not limited to plant rooms, air conditioning, lift overruns and roof mounted equipment.
- Buildings should be designed with a floor-to-floor dimension of a minimum of 4.0 metres at ground floor to enable adaptation for other uses in the future.

Building separation

- Where development shares a common boundary, upper level development should:
 - Be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
 - Be setback a minimum of 3.0m from the common boundary where a commercial or non-habitable window is proposed.

Building form and design

- Building design should minimise the visual bulk of large buildings through significant breaks and recesses in building massing.
- Development should avoid repetitive stepped building forms by providing a common street and rear setback for a minimum of 65% of the upper levels above street wall. Refer to diagram below:



- Buildings should reflect the existing fine grain pattern (narrow shopfronts) within the shopping strip by incorporating separate ground floor tenancies and vertically and horizontally modulated forms that integrate with the streetscape context.
- Roof forms should be integrated with the overall building façade design.
- Buildings should utilise materials that do not generate glare, and can withstand the effects of weathering.
- All new buildings are to incorporate best practice Environmentally Sustainable Development (ESD) principles. Refer to the Sustainable Design Assessment in the Planning Process (SDAPP) Framework.

Heritage Design

- These requirements apply to properties within an existing or proposed Heritage Overlay or abutting an existing or proposed Heritage Overlay.
- Facade design and details for infill buildings and new buildings should:
 - Be simple and not compete with the elaborate detailing of the adjoining heritage buildings.
 - Respect the vertical proportions of the heritage streetscape and adjoining heritage buildings.
 - Avoid large expanses of glazing except for ground floor shopfronts.
 - Adaptation and reuse of heritage buildings should:
 - Maintain existing openings and avoid highly reflective glazing in historic openings.
 - Encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings.
 - Maintain the inter-floor height of the existing building and avoid new floor plates and walls cutting through historic openings.
 - Development above the street wall on land within a Heritage Overlay or adjoining a Heritage Overlay should:
 - Be visually recessive and not dominate the heritage building and streetscape.
 - Utilise materials and finishes that are recessive in texture and colour.
 - Incorporate simple architectural detailing that does not detract from the heritage buildings and streetscape
 - Reflect the fine-grain pattern of subdivision of the streetscape.

Street interface

- Provide a minimum of 80% of the building façade at ground floor level with a visually permeable façade incorporating windows and door openings with clear glazing to ‘Key Public Realm Areas’.
- Buildings on corner sites should be designed to actively address both frontages at street level.
- Upper levels of buildings should be designed to provide habitable rooms with windows or balconies that overlook the public realm.
- Buildings fronting laneways should be designed for passive surveillance with a permeable façade, including windows and door openings.
- Ensure car parking is not visible from the street by incorporating design measures such as sleeving, or alternatively providing as basement parking.

Residential Interface

- Development should provide upper level setbacks in accordance with the precinct specific setbacks (refer to sections 4.3 to 4.4).
- Development must satisfy the objectives and standards of Clauses, 54,55 or 58, which aim to protect the amenity of adjoining residential areas.

Weather protection

- Provide verandahs on all buildings located in the ‘Weather Protection Areas’.
- Verandahs should be at an appropriate height above the footpath to avoid damage whilst still providing effective weather protection, generally between 3.0 and 4.0m and consistent with adjoining sites.
- Verandahs should be designed to mitigate the potential for visual clutter effects from light fittings, service cables and under awning signage.

Access and services

- Pedestrian entries to buildings should be clearly visible and easily identifiable from the street and accessible for all abilities.
- Residential entries should be distinguished from retail and commercial entries.
- Loading, service access and car park access should be provided from laneways and secondary streets. Where this not possible, vehicle crossovers should be minimised to reduce disruption to the footpaths and active frontages and located to avoid street trees if present.
- Provide appropriate setbacks at the rear of the building to laneways ensure adequate space for car park access and servicing. Further details at Clause 52.06 of the Glen Eira Planning Scheme.
- Screen air conditioning services, antennas and other utilities from public view using balcony treatments / roof structures / architectural elements. Avoid using walls to screen services.
- Avoid and minimise building services and utilities at ground floor street frontages to prioritise active frontages at these locations. Integrate services and utilities with the building design.
- Waste storage, loading and recycling facilities should be screened from public view. They should be easily accessed by residents and well ventilated.

Landscaping

- Communal garden spaces should be provided at podium and rooftop levels where appropriate to create amenity for residents, workers and visitors. The gardens should take into consideration, aspect, materials and solar orientation.
- Large development sites are encouraged to contribute to improved pedestrian amenity of the centre by providing for new private/public pedestrian/cycling links through the centre, where sites present an appropriate and useful opportunity to do so.

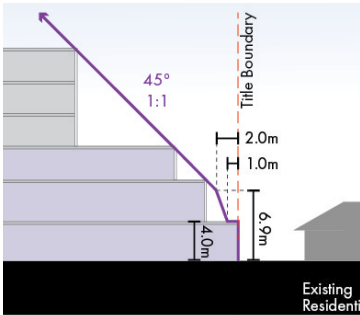
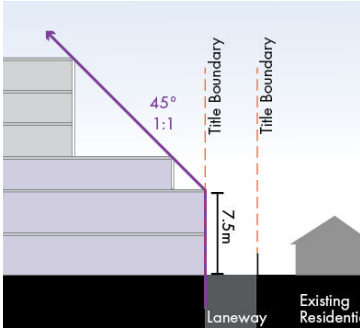
4.3 PRECINCT 1 - HAWTHORN ROAD

4.3.1 PRECINCT OVERVIEW

Hawthorn Road will strengthen its role as a vibrant shopping and employment strip supported by new mixed use developments of up to six storeys, which transitions in scale to the proposed heritage precinct and the St Aloysius Church.

New buildings of high architectural quality will refresh and lift the character of the street and mark the important intersection of Hawthorn and Balaclava Road and provide an attractive interface to the park.

4.3.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
<div>Mandatory maximum Heights</div> <div><ul style="list-style-type: none">Up to 4 storeys (15.0m) within the proposed heritage precinct at 158-166 Hawthorn Road</div> <div>Preferred Maximum Heights</div> <div><ul style="list-style-type: none">Up to 5 storeys (18.0m) adjacent to the proposed heritage precinct for land at 154-156 Hawthorn RoadUp to 6 storeys (21.0m) elsewhere</div>	<div>Preferred Street wall Height</div> <div><ul style="list-style-type: none">Up to 2 storeys (9.0m) or match the height of the adjoining heritage building for land at 154-156 Hawthorn Road3 storeys (11.0m) elsewhere</div>	<div>Preferred Setbacks</div> <div><ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall5.0m front setback for development above the street wall3.0m setback to the secondary street frontage for development above the street wall on corner sites3.0m setback to both street frontages for development above the street wall on sites located on the corner of Hawthorn Road and Balaclava Road5.0m setback to the side eastern boundary of 219 Balaclava Road for development above the street wallDevelopment directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:</div> <div></div> <div><ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below:</div> <div></div>	<div>Heritage</div> <div><ul style="list-style-type: none">Development should respect the heritage protected St Aloysius Church.Development should respect the proposed heritage precinct at 158-166 Hawthorn Road.Retain individually significant and contributory heritage buildings.Upper levels should be set behind the entire front hipped roof form of significant and contributory buildings at 158-166 Hawthorn Road.</div> <div>Solar Access</div> <div><ul style="list-style-type: none">Ensure solar access is maintained to the entire eastern footpath of Hawthorn Road from 12pm - 2pm on September 22.Ensure solar access is maintained to the entire western footpath of Hawthorn Road from 10am - 12pm on September 22.Ensure solar access is maintained to the entire southern footpath of Balaclava Road from 10am on September 22.</div>

4.3.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 1 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.2:

- The key footpaths along Hawthorn Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to the entire western footpath from 10am - 12pm at the equinox and the eastern footpath will be in sunlight from 12pm - 2pm. This will support the role of the footpaths as key public spaces at times where they are likely to be more active.
- The desired low to mid rise scale of the centre will be achieved with a maximum building height of 6 storeys. This height supports additional density in the centre while balancing character and amenity considerations. Setbacks of 5 metres for development above 3 storeys will ensure the street wall is the dominant element in the streetscape when viewed from opposite footpaths, rather than the overall building height. The long cross sections shown in Figures 24 and 25 demonstrate that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The proposed 6 storey building height is also consistent with recent developments. The larger lots within this precinct could support this height and transition appropriately to the adjoining residential areas.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the NAC.

- The proposed heritage area at 158-166 Hawthorn Road will retain its prominence in the streetscape by providing mandatory height of 4 storeys on the heritage sites and providing a gradual scale transition to 5 storeys on the two sites immediately north. The heritage buildings are approximately 11m in height (including the prominent roof form), which is equivalent to three contemporary storeys. The additional one storey in height above the heritage building will ensure it remains prominent. Because of the heritage significance of these buildings, and the potential for taller development to reduce the prominence of the heritage building, a mandatory height control was considered necessary. In addition, a specific requirement to set upper levels behind the entire hipped roof form fronting the street will ensure the buildings retain their heritage significance.
- The heritage significance of the St Aloysius Church will be maintained by providing upper level setbacks of 5.0m above 3 storeys to the eastern boundary of 219 Balaclava Road. This setback is the equivalent width of 231 Balaclava Road which has a recommended height of 3 storeys. The additional heritage requirements outlined in 4.2.2 will further enhance the heritage place.
- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks in accordance with Section 4.2.2 and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- Sunlight to the outdoor areas of the St Aloysius Primary School will be maintained by applying rear upper level setbacks to commercial properties adjoining the laneway.
- The intersection of Hawthorn Road and Balaclava Road will be enhanced by providing buildings with reduced upper level setbacks and encouraging high quality architecture at this important junction.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

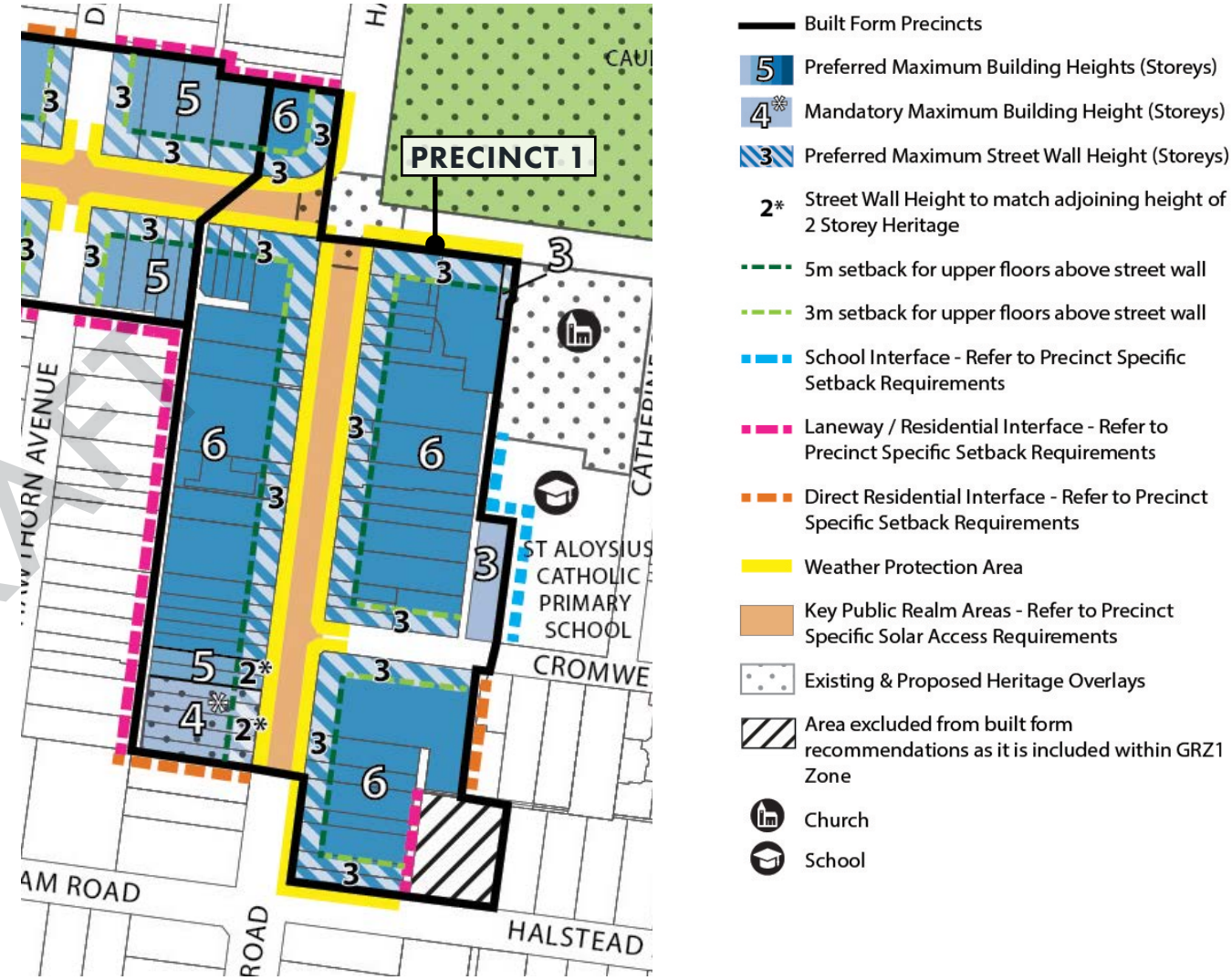


Figure 21. Built Form Framework - Precinct 1

4.3.4 PRECINCT 1 - CROSS SECTIONS

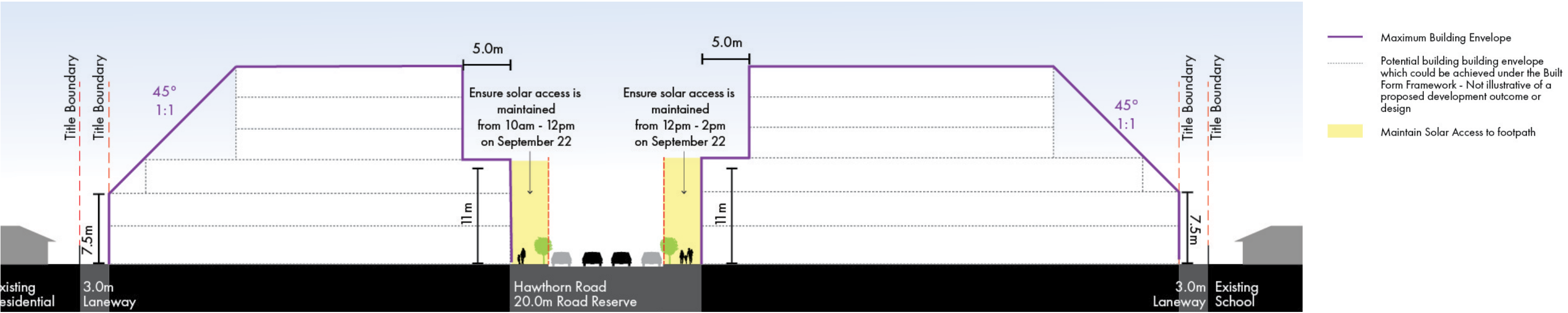


Figure 22. Precinct 1 - Cross Section A

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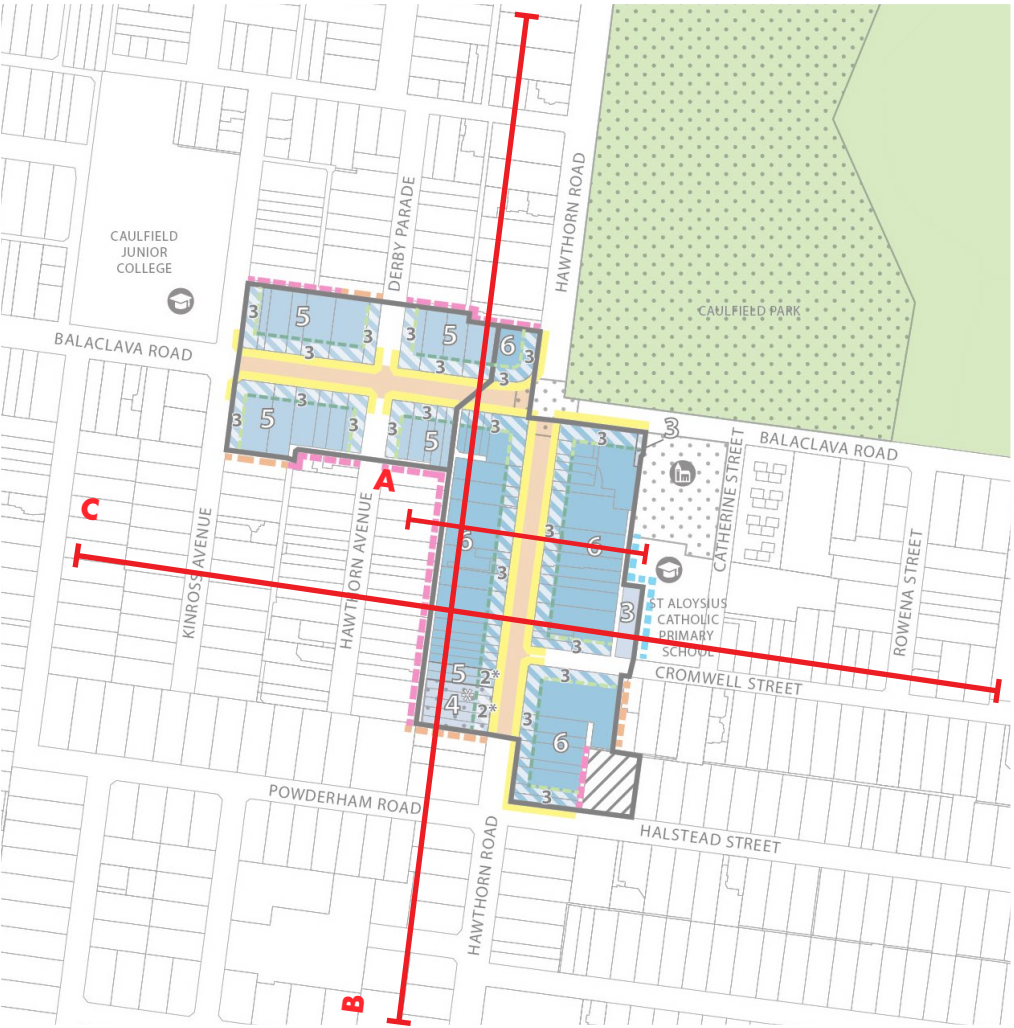


Figure 23. Precinct 1 - Cross Section Key Plan

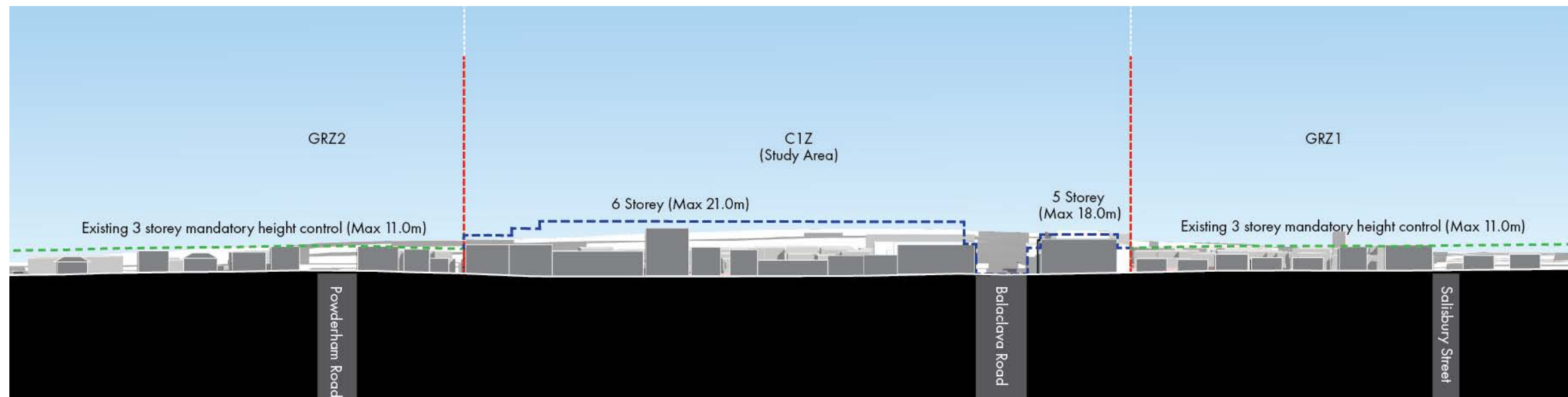


Figure 24. Precinct 1 - Cross Section B

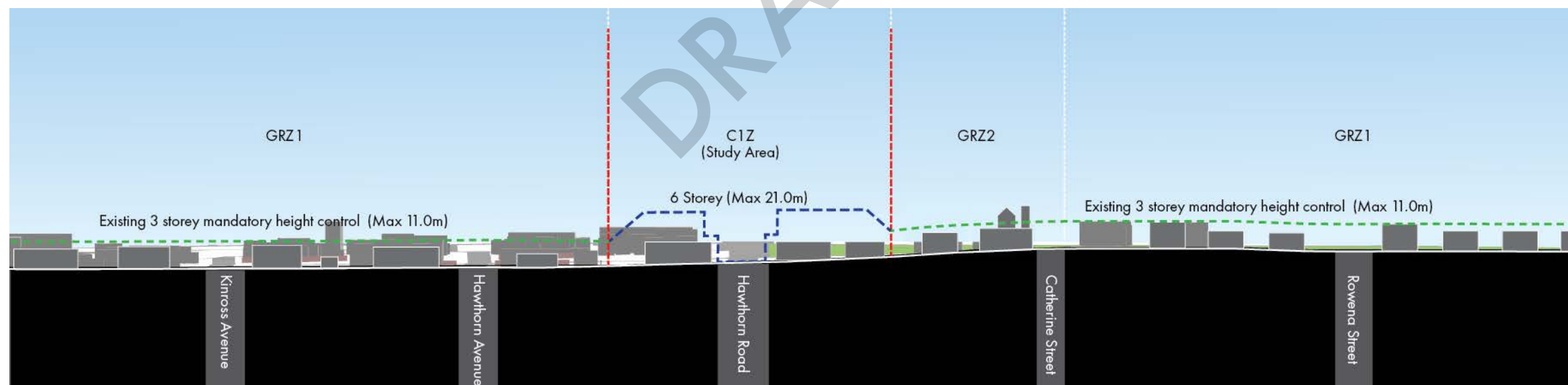


Figure 25. Precinct 2 - Cross Section C

- Study Area Boundary
- Planning Scheme Zoning Boundary
- Existing Built Form
- Existing Maximum Built Form Height
- Proposed Maximum Built Form Height

4.3.5 PRECINCT 1 - SKETCH VISUALISATION



Figure 26. Viewpoint 1 - Existing Conditions



Figure 28. Viewpoint 1 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

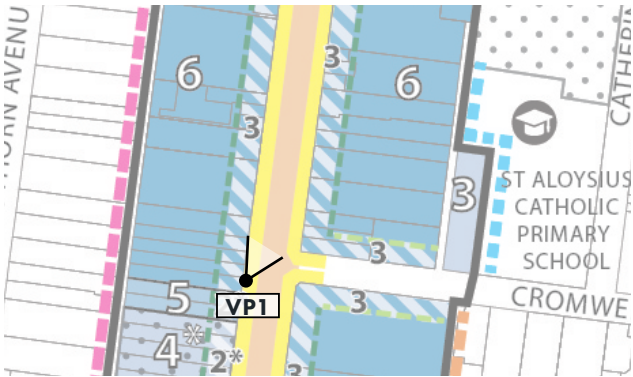


Figure 27. Viewpoint 1 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the street wall will integrate with the emerging three storey street wall provided in recent developments. The upper levels are visible and match in with the sixth storey of the existing development at 67 Hawthorn Road. The street wall, however, remains the dominate element in the streetscape.

4.3.6 PRECINCT 1 - SHADOW ANALYSIS

Figure 29 demonstrates the shadows cast by existing buildings within the Caulfield Park NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

9am - Sept 22



10am - Sept 22



11am - Sept 22



Figure 29. Precinct 1 - Shadow Analysis

12pm - Sept 22



1pm - Sept 22



2pm - Sept 22



3pm - Sept 22



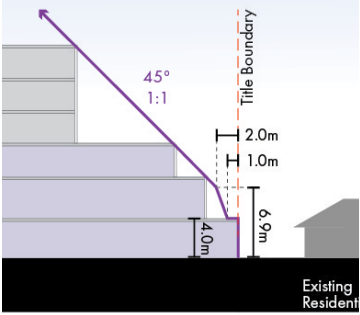
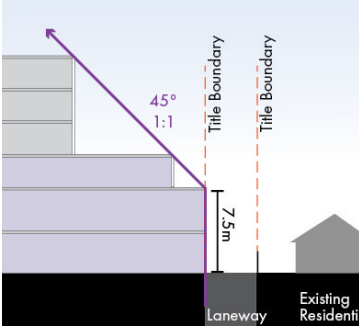
4.4 PRECINCT 2 - BALACLAVA ROAD

4.4.2 PRECINCT OVERVIEW

Balaclava Road continues to provide a commercial and office focus with mixed use buildings of up to five storeys replacing older warehouses and providing greater activation to the street.

Development sensitively scales to adjoining residential areas.

4.4.1 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 5 storeys (18.0m)	Preferred Street wall Height <ul style="list-style-type: none">3 storeys (11.0m)	Mandatory Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall Preferred Setbacks <ul style="list-style-type: none">5.0m front setback for development above the street wall3.0m setback to the secondary street frontage for development above the street wall on corner sitesDevelopment directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Balaclava Road from 10am on September 22.
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below: 	

4.4.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 2 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.2:

- The southern footpath along Balaclava Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to the entire southern footpath from 10am. This will ensure the footpath is able to fulfil its role as a key public space by providing sunlight at times when it is likely to be more active.
- The desired low to mid rise scale of the centre will be achieved with a maximum building height of 5 storeys. This height supports additional intensification and development in the centre which is consistent with the planning policy context outlined in Part 1 of this report. The proposed height is slightly lower than the proposed heights for Hawthorn Road, which reflects the lower order role of Balaclava Road. The larger lots and presence of laneways within this precinct could support this height and transition appropriately to the adjoining residential areas. Setbacks of 5m for development above 3 storeys will ensure the street wall is dominant in the streetscape when viewed from the opposite footpath. The long cross section shown in Figure 25 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be strengthened by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised and help to activate Balaclava Road.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks in accordance with Section 4.2.2 and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. This is particularly important as residential uses are located to south of commercial properties. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

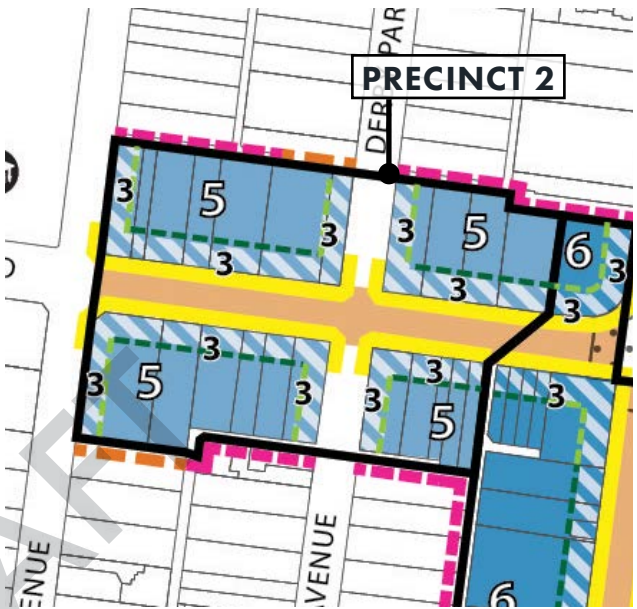


Figure 30. Built Form Framework - Precinct 2

- Built Form Precincts
- 5 Preferred Maximum Building Heights (Storeys)
- 4 Mandatory Maximum Building Height (Storeys)
- 3 Preferred Maximum Street Wall Height (Storeys)
- 2* Street Wall Height to match adjoining height of 2 Storey Heritage
- 5m setback for upper floors above street wall
- 3m setback for upper floors above street wall
- School Interface - Refer to Precinct Specific Setback Requirements
- Laneway / Residential Interface - Refer to Precinct Specific Setback Requirements
- Direct Residential Interface - Refer to Precinct Specific Setback Requirements
- Weather Protection Area
- Key Public Realm Areas - Refer to Precinct Specific Solar Access Requirements
- Existing & Proposed Heritage Overlays
- Area excluded from built form recommendations as it is included within GRZ1 Zone
- Church
- School

4.4.4 PRECINCT 2 - CROSS SECTION

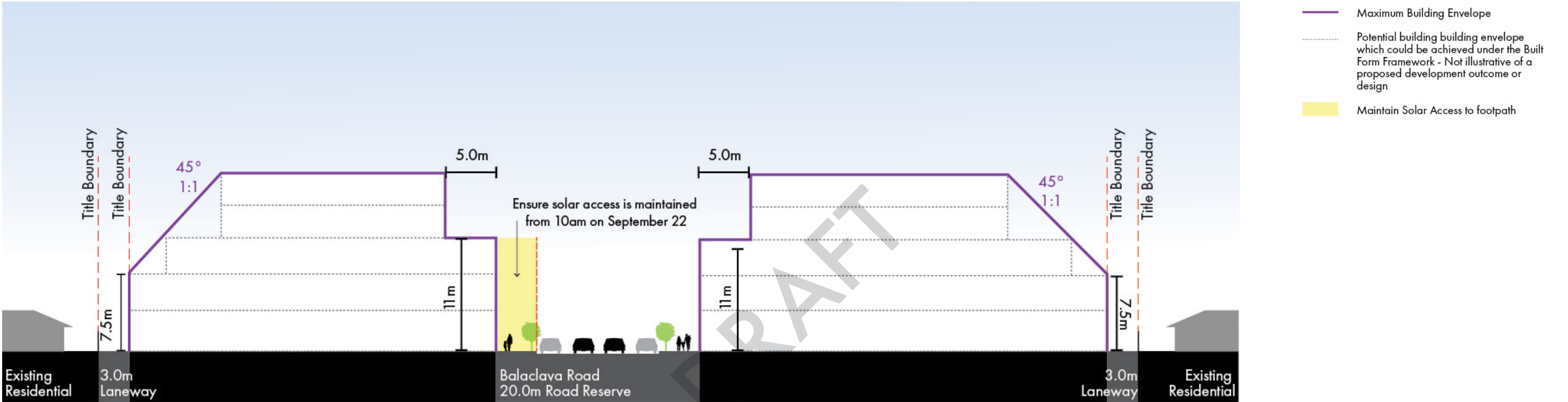


Figure 31. Precinct 2 - Cross Section A



Figure 33. Precinct 2 - Cross Section Key Plan

4.4.5 PRECINCT 2 - SKETCH VISUALISATION



Figure 34. Viewpoint 2 - Existing Conditions



Figure 36. Viewpoint 2 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

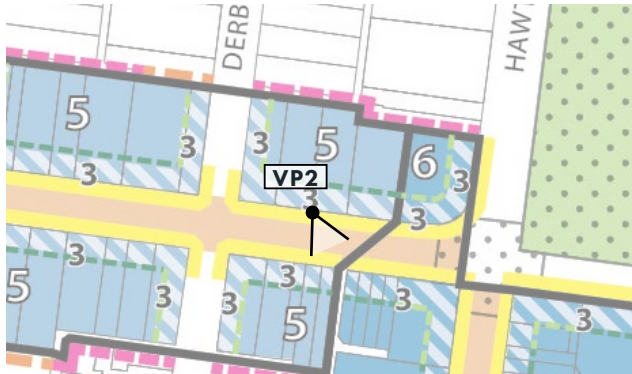


Figure 35. Viewpoint 2 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the street wall will integrate with the existing context, create a more cohesive streetscape and reinforce the important intersection of Hawthorn Road and Balaclava Road. The view shows upper levels of development in Precinct 1 visible where a 6 storey height limit is proposed.

4.4.6 PRECINCT 2 - SHADOW ANALYSIS

Figure 36 demonstrates the shadows cast by existing buildings within the Caulfield Park NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



10am - Sept 22



11am - Sept 22



12pm - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 37. Precinct 2 - Shadow Analysis

1pm - Sept 22



2pm - Sept 22



3pm - Sept 22



DRAFT

CAULFIELD SOUTH NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK

Tract



Prepared for Glen Eira City Council

DRAFT BUILT FORM FRAMEWORK

APRIL 2021

QUALITY ASSURANCE

Glen Eira NAC`s Built Form Frameworks
Caulfield South NAC Draft Built Form Framework

Prepared for
Glen Eira City Council

Project Number
320.0714.U.03

Revisions

Issue	Date	Description	Prepared By	Reviewed By	Project Principal
00	4 February 2021	Response to Council Officer comments	KXS	TS	MN
01	24 February 2021	Response to Council Officer comments	KXS	TS	MN
02	10 March 2021	Response to Council Officer comments	KXS	TS	MN
03	23 March 2021	Updated to reflect VCAT decision on 388-394 Hawthorn Road.	KXS	TS	MN
04	14 April 2021	Response to Council Officer comments	KXS	TS	MN
05	21 April 2021	Revisions from Council discussions	KXS	TS	MN

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INTRODUCTION

PROJECT OVERVIEW

STUDY AREA

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• Project Marketing
• Project Management
• Property Development

 **JARLLEY**
Property Group

722

72



JARLLEY
Property Group

POPLAR ST

CAULFIELD
PRIMARY SCHOOL

9523 9888
JARLLEY.COM.AU

Scachi hair studio

40

16TH
ST →
Actors
Studio

PROJECT OVERVIEW

This built form framework for the Caulfield South Neighbourhood Activity Centre (NAC) is one of three built form frameworks being prepared towards addressing an existing policy void. The other centres forming part of the project are the Caulfield Park Neighbourhood Activity Centre and the Bentleigh East Neighbourhood Activity Centre.

The Built Form Frameworks set out preferred development outcomes in each centre. This will ensure new development provides for the continual revitalisation of the shopping strips with high quality design whilst maintaining the valued character of each centre and protecting the amenity of adjoining residential areas.

The Frameworks provide guidance for building heights, setbacks and other design considerations. These frameworks are supported by rigorous analysis and guided by sound urban design principles appropriate to the context of each centre.

The Built Form Frameworks provide requirements around building heights, setbacks and other design considerations. These requirements are supported by rigorous analysis and guided by sound urban design principles appropriate to the context of each centre.

This Caulfield South NAC Built Form Framework consists of the following parts:

Part 1 - Context and Analysis - Provides analysis and background research to understand the NAC, its context and drivers for change.

Part 2 - The Built Form Framework - Provides the guiding principles and built form framework including building heights and setbacks.

Proposed planning controls have been developed to reflect the requirements of the Caulfield South NAC Built Form Framework. The controls are provided in the form of a Design and Development Overlay.

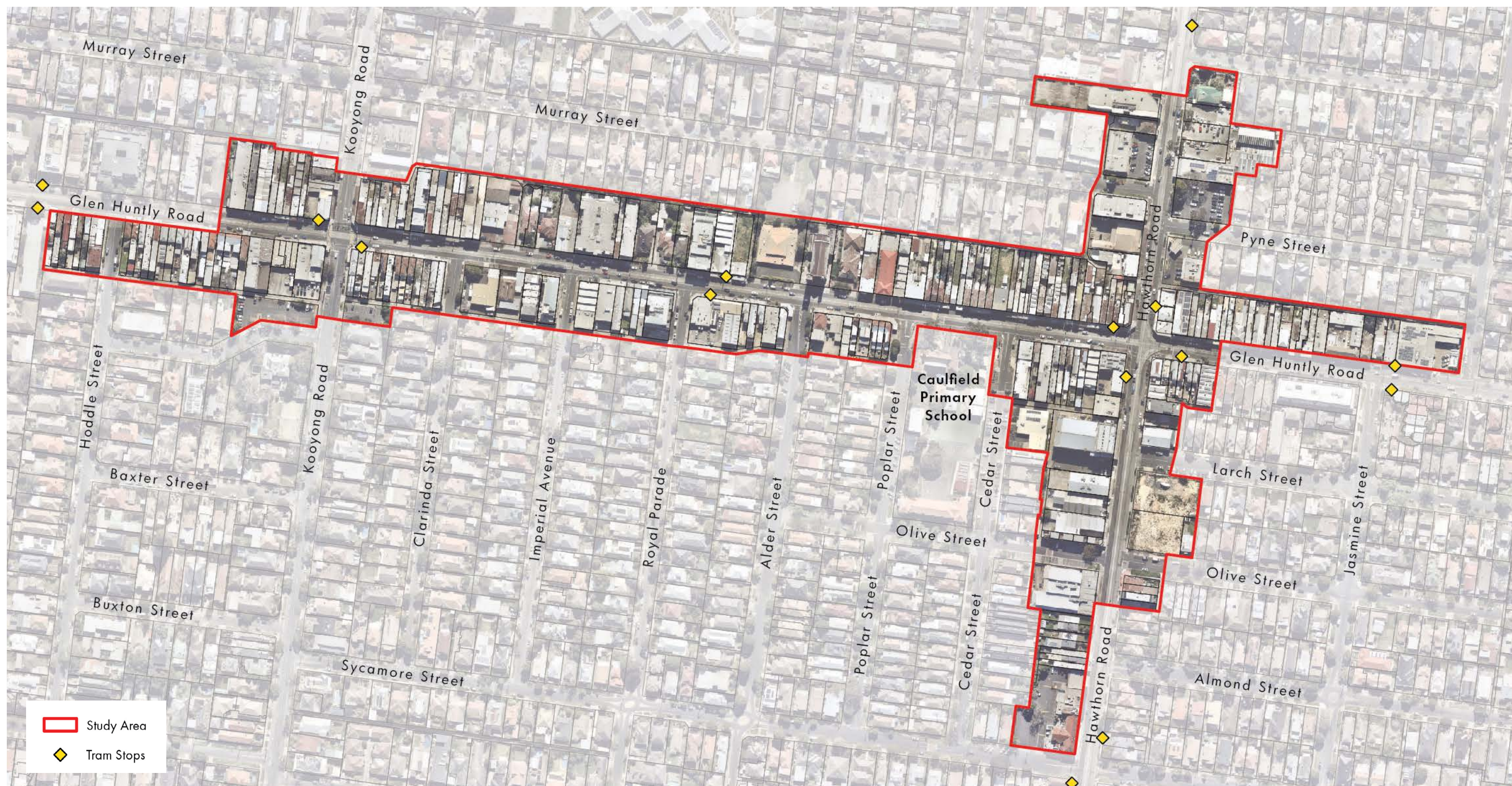
STUDY AREA

The study area generally includes the commercially zoned land within the Caulfield South NAC and part of the commercial land within a local centre at the intersection of Glen Huntly and Kooyong Roads. Its primary frontage is to Glen Huntly Road with a length of 1.1km and has a secondary frontage to Hawthorn Road with a length of 520m. The boundaries of the NAC generally align with the existing Commercial 1 Zoning. Refer to Figure 1 - Study Area.

The study area includes a range of retail, hospitality, services, office and community uses and has a number residents living above shops in newer, mixed use developments. A major supermarket is proposed along Hawthorn Road, which will further strengthen the retail role of the NAC.

Caulfield South is experiencing the greatest amount of development pressure out of the three Centres with a significant amount of development activity and approvals along Hawthorn Road.

The centre enjoys convenient access to public transport with tram routes along both Glen Huntly Road and Hawthorn Road.



PART 1 - CONTEXT & ANALYSIS

1. CONTEXT

- 1.1 STRATEGIC CONTEXT
- 1.2 STRATEGIC DOCUMENTS
- 1.3 PLANNING POLICY FRAMEWORK
- 1.4 LOCAL PLANNING POLICY FRAMEWORK
- 1.5 PLANNING ZONES
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- 1.7 RECENT DEVELOPMENT CONTEXT

2. ANALYSIS

- 2.1 TOPOGRAPHY
- 2.2 LAND USE & INTERFACES
- 2.3 BUILT FORM & CHARACTER
- 2.4 DEVELOPMENT ACTIVITY & CAPACITY





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RT Edgar

RT Edgar

AGE GREEK TAVERN

WINE & BEER

SMILE EAT DRINK
PARTY LAUGH SING

1 CONTEXT

1.1 STRATEGIC CONTEXT

Consistent with sustainable land use and development principles, Neighbourhood Activity Centres are identified in state and local planning policy and strategy as suitable locations for accommodating increased residential density. The policy framework including Plan Melbourne, City Plan and the relevant clauses of the Glen Eira Planning Scheme (State and local) establishes this clear direction.

1.1.1 PLANNING POLICY CONTEXT

Planning policy relating to Caulfield South Neighbourhood Activity Centre (NAC) identifies the centre as one of 11 NACs in Glen Eira. The higher order centres in Glen Eira are Major Activity Centres, of which there are six. Consistent with sustainable land use principles, NACs in the metropolitan areas are high amenity areas that should accommodate growth to meet the needs of existing and future communities. They differ from the Major Activity Centres identified in Figure 3, which offer a greater amount of transport options, retail and services. As such the level of development and change in the NACs is expected to be less than that experienced in the Major Activity Centres.

The following sections provide a brief outline of this policy context. As noted previously, the centre is largely in the Commercial 1 Zone.

In and around activity centres, a degree of change will occur, consistent with Plan Melbourne, City Plan and the Glen Eira Planning Scheme and commensurate with the level of amenity and accessibility provided by each centre and also taking into account locally-specific amenity and character considerations. Planning controls informed by the built form framework will provide Council with the necessary guidance to assess planning applications and to provide proponents and the community with clear parameters for design in centres.

Council aims to sustainably manage anticipated growth and amenity and to identify and preserve the valued features of the built environment as its activity centres evolve while also protecting the valued amenity of the areas surrounding the centres. For Council to implement built form guidance that is robust and defensible, the guidance must balance the strong

policy objectives of directing more intense development to well-located areas such as neighbourhood activity centres with the need to protect the key elements that make these centres desirable places in their own right. In the Victorian planning system, planning controls that do not achieve a reasonable balance cannot be introduced to the planning scheme.

1.1.2 BUILT FORM FRAMEWORKS IN CONTEXT

Built form frameworks, like structure plans, consider locally-specific elements of a centre such as land use patterns, built form and character, function and amenity and form a vision and built form controls for addition to the planning scheme. Figure 2 provides an overview of where built form frameworks sit in the planning framework relative to built form decisions and residential growth.

1.1.3 PURPOSE OF THE CAULFIELD SOUTH NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK

Recent development activity and planning applications at Caulfield South NAC and other NACs indicate the need for more detailed and specific built form policy and controls to guide development. While there are existing policies and controls applying to the Caulfield South NAC they generally consist of:

- High-level strategy and planning policy that supports increased densities in NACs.
- Zoning that predominantly guides land use rather than built form outcomes. The Commercial 1 Zone allows for more intense mixed use development and nearby residential zoning provides for a transition up from the residential hinterland towards activity centres.

This built form framework will provide the locally-relevant analysis of physical characteristics and a vision to inform the drafting of built form controls for the planning scheme and provide that more specific and detailed guidance.

The following issues are addressed by the built form framework:

- Preferred building heights based on built form and shadow analyses.
- Street wall and interfaces
- Building setbacks and modulation – for amenity and visual impact
- Transitions to less intense development

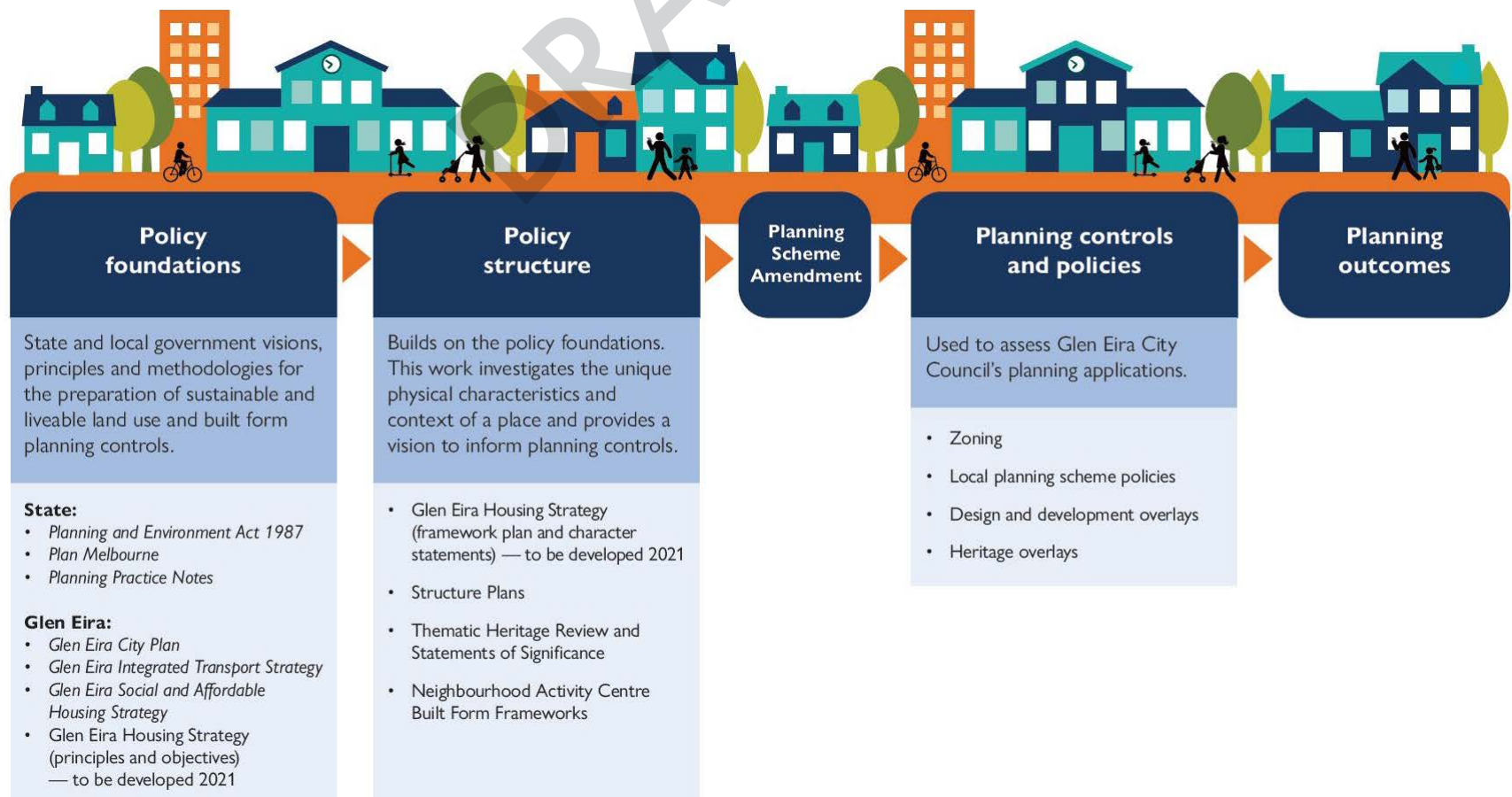


Figure 2. Built Form Framework Planning Context

1.1.4 CAULFIELD SOUTH NEIGHBOURHOOD
ACTIVITY CENTRE LOCATION AND PHYSICAL
CONTEXT

The Caulfield South Neighbourhood Activity Centre is located on Glen Huntly Road and Hawthorn Road in Caulfield South. The centre is located towards the north of the municipality in a predominantly residential context and enjoys access to fixed rail (tram) services along both roads. The centre is located approximately 9.0 km south-east of the Melbourne CBD.

The centre also benefits from the heritage character of the existing buildings, particularly along Glen Huntly Road. See the Built Form Analysis section of this report for an overview of the existing built form context. The areas immediately surrounding the centre are low-rise neighbourhoods in the General Residential Zone. The General Residential Zone allows for development of up to three storeys.

As mentioned previously, Council’s planning policies identify the centre as one of 11 NACs in Glen Eira, being second in the activity centre hierarchy in terms of suitability for increased density and height due to the level of amenity offered.



Figure 3. Context Plan

1.2 STRATEGIC DOCUMENTS

1.2.1 PLAN MELBOURNE 2017-2050

The updated Plan Melbourne document provides strategic direction and guidance for some of Melbourne’ state significant employment and activity centre clusters, along with housing and transport directions for greater Melbourne. While the document does not provide specific guidance to the Caulfield South Neighbourhood Activity Centre some of the more relevant directions are:

- Direction 2.1: Manage the supply of new housing in the right locations to meet population growth and create a sustainable city.
- Direction 5.1: Create a city of 20-minute neighbourhoods.

These directions aim to cluster new housing in activity centres and other places that offer good access to jobs, services and public transport and includes a policy for local governments to prepare structure plans for activity centres to accommodate growth.

There are significant opportunities for medium and higher density housing options in locations close to jobs and services including located around Major Activity Centres

The strategic direction closely aligns to the future outcomes envisaged for the NAC. Particularly providing housing opportunities close to transport enabling people to work and live in close proximity.

1.2.2 GLEN EIRA CITY PLAN

The Glen Eira City Plan provides a broad framework to manage growth and change in both housing and employment. Consistent with Plan Melbourne and several existing local planning policies, City Plan aims to direct the majority of new growth into activity centres towards sustainable land use and development outcomes.

To implement the strategic framework (introducing planning controls to the planning scheme), structure planning or built form frameworks need to be prepared for key areas suitable for growth such as major and neighbourhood activity centres and urban renewal areas. These activities produce the built

form, amenity and character material to assist to refine height controls and identify specific ways of managing amenity and character through the planning scheme.

Glen Eira City Plan provides an activity centre hierarchy which identifies Major Activity Centres at the top, followed by Neighbourhood Activity Centres and local centres. The highest levels of growth are to be directed to Major Activity Centres and urban renewal areas. Neighbourhood Activity Centres are identified as suitable for ‘moderate focus for housing growth’.

City Plan provides vision statements for growth locations. The vision for the Caulfield South NAC is:

- **Caulfield South** will be a convenient, diverse and attractive shopping strip that is vibrant both during the day and at night. Enhanced pedestrian and cycling amenity and greenery will foster an active, safe and friendly environment.

The Plan also seeks to nominate appropriate heights for the identified housing change areas with the neighbourhood activity centres identified as being suitable for up to between four storeys (for commercial areas with heritage overlays) and five storeys (for commercial areas without heritage overlays).

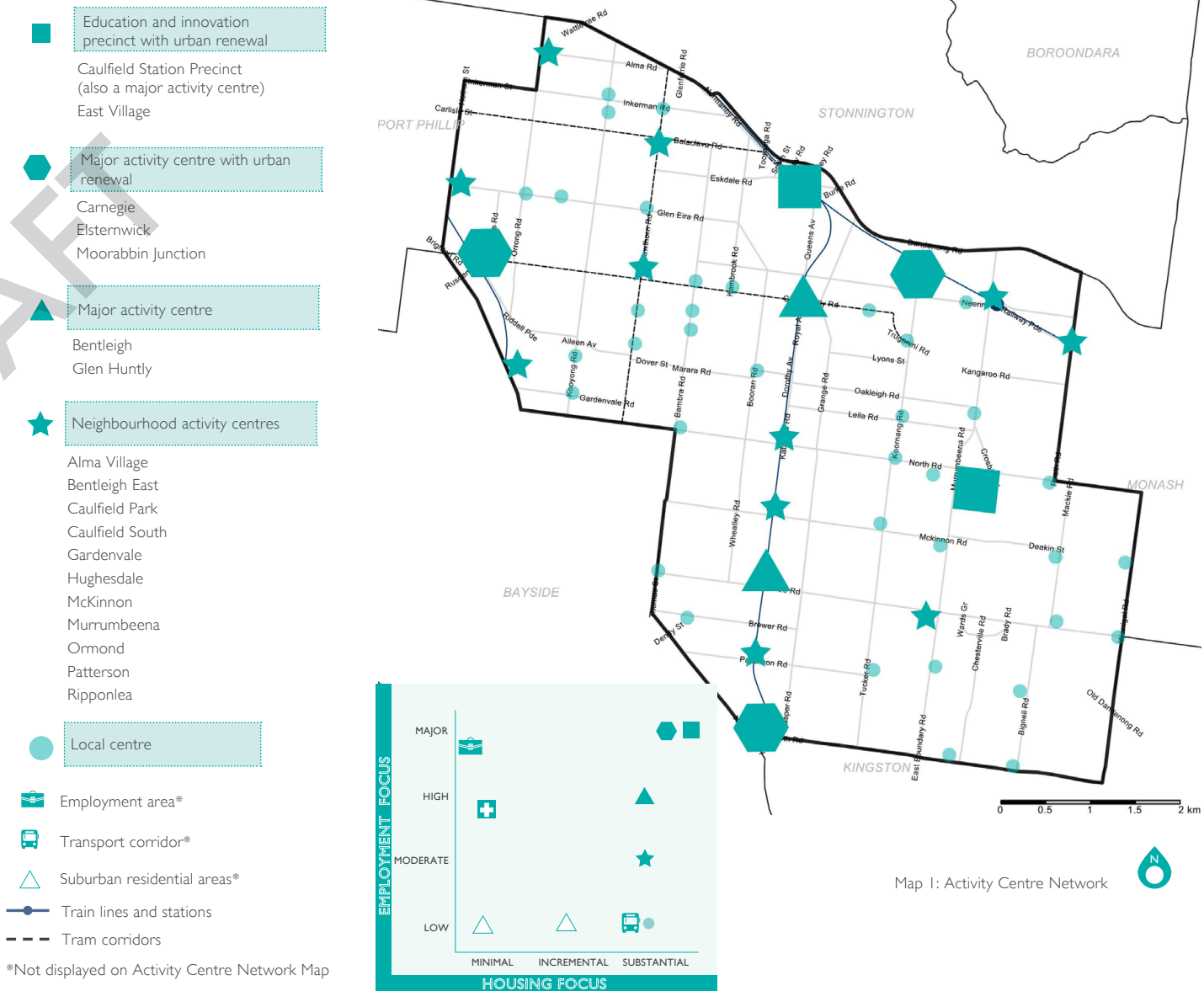


Figure 5. Glen Eira Activity Centre Network Map

1.3 PLANNING POLICY FRAMEWORK

1.3.1 PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following state policies of the Glen Eira Planning Scheme, summarised as follows:

1.3.2 CLAUSE 11 – SETTLEMENT

Seeks to anticipate and respond to the needs of existing and future communities through the provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure.

Clause 11.03-1S (Activity Centres)

Seeks to encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

Clause 11.03-1R (Activity Centres – Metropolitan Melbourne)

Aims to support the development and growth of Metropolitan Activity Centres by encouraging the location of new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.

1.3.3 CLAUSE 13 (ENVIRONMENTAL RISK AND AMENITY)

Seeks to strengthen the resilience and safety of communities by adopting a best practice environmental management and risk management approach.

13.03-1S (Floodplain management)

- Seeks to assist the protection of:
- Life, property and community infrastructure from flood hazard.
 - The natural flood carrying capacity of rivers, streams and floodways.

1.3.4 CLAUSE 15 – BUILT ENVIRONMENT

Seeks to ensure that all new land uses and development appropriately responds to its landscape, valued built form and cultural context, and protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value.

Clause 15.01-1S (Urban design)

Aims to create urban environment that are safe, functional and provide good quality environments with a sense of place and cultural identity.

Clause 15.01-1R (Urban design – Metropolitan Melbourne)

Aims to create a distinctive and liveable city with quality design and amenity by supporting the creation of well-designed places that are memorable, distinctive and liveable.

Clause 15.02-1S (Energy and resource efficiency)

Aims to encourage land use and development that is energy and resource efficient, supports a cooler environment and minimises greenhouse gas emissions through strategies such as greening urban areas, buildings, transport corridors and open spaces with vegetation to reduce urban heat island effects.

Clause 15.03-1S (Heritage conservation)

Seeks to ensure the conservation of places of heritage significance by retaining elements that contribute to the importance of the heritage place.

1.3.5 CLAUSE 16 – HOUSING

Seeks to provide for housing diversity and ensure the efficient provision of supporting infrastructure. However, the quantum of development intensification remains relatively open, with appropriate controls to be defined through local planning based on local context.

Clause 16.01-1S (Housing supply)

Seeks to facilitate well-located, integrated and diverse housing that meets community needs through measures such as increasing the proportion of housing in designated locations in established urban areas (including under-utilised urban land) and reduce the share of new dwellings in greenfield, fringe and dispersed development areas and encouraging higher density housing development on sites that are well located in relation to jobs, services and public transport.

Clause 16.01-1R (Housing Supply – Metropolitan Melbourne)

Aims to manage the supply of new housing to meet population growth and create a sustainable city by developing housing and mixed use development opportunities in established areas. Those locations include neighbourhood activity centres - especially those with good public transport connections.

1.4 LOCAL PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following local policies of the Glen Eira Planning Scheme, summarised as follows:

Please note that while this content is correct at the time of writing, Council is in the process of updating its local planning policy. While there will be changes to specific policies it is not expected that the general intent of policy relating to NACs will change.

1.4.1 CLAUSE 21.04 – HOUSING AND RESIDENTIAL DEVELOPMENT

This policy provides objectives and strategies to guide housing and residential development in Glen Eira. The following policies are considered relevant to this built form framework:

Objective 4

- To stimulate and improve the vitality of Glen Eira’s commercial centres.

Strategies

- Encourage a mix of housing types, increased residential densities and mixed use developments within urban villages and neighbourhood centres.
- Encourage residential development, where considered appropriate, subject to consultation with traders and the wider community, above or below existing car parks in urban villages and neighbourhood centres, whilst maintaining or enhancing the existing level of parking.
- Ensure that developments in commercial areas that adjoin residential areas are sensitively designed to protect residential amenity.
- Ensure residential development in commercial areas does not contribute to traffic and car parking problems.

1.4.2 CLAUSE 21.06 – BUSINESS

This policy provides objectives and strategies to guide the development of identified urban villages and neighbourhood centres to cater for a wider variety of commercial use while retaining a hub of convenience shops within Glen Eira. The following policies are considered relevant to this built form framework:

Objectives (amongst others)

- To maintain a mix of commercial centres that cater for the needs of the Glen Eira community.
- To enhance and further develop urban villages and neighbourhood centres as the focus for community life.
- To maintain pleasant and safe public environments in commercial centres in partnership with business.

Strategies (amongst others)

- Maintain the hierarchy of commercial centres so that centre functions are easily definable and to ensure that retail and other requirements are suitably provided at each level in the hierarchy.
- Ensure that new or expanded land uses are able to be accommodated in existing commercial centres, including the encouragement of non-retail businesses, where appropriate.
- Ensure commercial activities in mixed use zone complement other commercial activities nearby and not undermine the commercial hierarchy.
- Ensure that, wherever possible, additional population growth can be accommodated (for example through multi-unit development) so existing facilities in commercial centres can continue to be supported.
- Ensure that commercial centres have a high standard of urban design and are attractive and appealing to potential customers, traders and investors.

1.4.3 CLAUSE 22.07 – HOUSING DIVERSITY AREA POLICY

This policy identifies the areas where housing diversity will be encouraged. It also seeks to ensure that the density, mass and scale of development is consistent with the role, capacity and constraints of each centre.

Objectives (amongst others)

- To encourage housing diversity in preferred strategic locations that have good access to public transport, commercial, community, educational and recreational facilities.
- To ensure that the density, mass and scale of residential development is appropriate to the location, role and neighbourhood character of the specific housing diversity area.
- To ensure that the siting and design of new residential development takes account of its interface with existing residential development on adjoining sites.
- To ensure that the design of new residential development is sensitive to and respectful of the scale of existing residential development on adjoining sites.

Strategies for Neighbourhood Activity Centres

- Recognise neighbourhood centres as locations which provide significant opportunities for housing diversity, but at a lesser scale and density than developments in urban villages and the Phoenix Precinct.
- Recognise that different development outcomes are sought in the commercial and residential areas of neighbourhood centres.

Strategies for commercial areas of centres (amongst others)

- Ensure that the density, mass and scale of development is appropriate to the scale, character and physical size of the neighbourhood centre.
- Ensure that any new residential development is designed and sited to minimise its adverse impacts on adjoining existing residential properties by way of building bulk, overlooking and overshadowing.

- Ensure that residential buildings are encouraged to step down at the rear to achieve a transition to residential areas.
- Ensure that building heights are compatible with their site context and the wider neighbourhood centre.
- Ensure that where the new building is greater in height than the prevailing building height or where significant changes in building height are proposed for residential buildings:
- There is a graduated transition in building height between the proposed building and adjoining buildings.
- The resulting height, mass and scale of the building does not dominate or visually intrude on the streetscape and takes account of views from the wider neighbourhood and at a distance.
- The upper storeys are recessive so that the visibility of upper storeys is reduced when viewed from the footpath opposite or residential properties to the rear.
- Provision be made for the establishment of new landmark buildings on strategic redevelopment sites that are made available through the removal of a level crossing.
- Ensure that the retail function and active frontage of centres is retained on the ground floor.

Strategies for the residential areas of centres (amongst others)

- Recognise that these areas offer opportunities for multi-unit development, but at a lower scale and density than development in the commercial and mixed use areas of neighbourhood centres.
- Ensure that the density, mass and scale of residential development is appropriate to that of the neighbourhood centre.
- Encourage a decrease in the density of residential development as the proximity to the commercial area of the neighbourhood centre decreases.
- Ensure that the siting and design of residential development responds positively to its interface with existing residential development in minimal change areas.

1.4.4 CLAUSE 21.10 – HERITAGE

This policy provides objectives and strategies to guide the development that does not threaten the heritage aspects of Glen Eira. The following policies are considered relevant to this study:

Objectives

To identify, protect, enhance and promote understanding of Glen Eira’s heritage.

Strategies

- Protect places identified as having architectural, cultural or historical significance.
- Ensure sympathetic redevelopment and renovation of areas and places identified as having architectural, cultural or historic significance in the municipality.
- Enhance knowledge and popular understanding of Glen Eira’s architectural, cultural and historic heritage.

1.4.5 CLAUSE 22.01 – HERITAGE POLICY

This policy applies to all land within the Heritage Overlay and seeks to identify, protect, enhance and promote understanding of Glen Eira’s heritage. More specifically, this Clause builds upon the basis of Clause 15.03 (Heritage) and Clause 21.10 (Heritage) and provides guidance on a number of elements:

Demolition (amongst others)

- Retain significant and contributory buildings.
- Allow the partial demolition of significant and contributory buildings where the fabric to be demolished is of no significance, or for the purpose of additions if the addition will not affect the heritage significance of the building and is sympathetic in its scale and form.
- To retain significant trees, and/or garden layouts that have been identified for their historical significance.

New buildings, Alterations and Additions (Commercial Heritage Areas)

- Encourage the conservation of setbacks that impart significance to those buildings that are surrounded by open space (including but not limited to churches and schools).
- Ensure any new upper level additions and works are respectful to the scale and form of the heritage place or contributory elements of the place and, where relevant, the heritage precinct as a whole.
- Encourage higher building additions to be well set back from the front wall of the building unless the specific context of the site recommends otherwise.
- Encourage the retention, restoration or reconstruction of original shopfronts and verandahs.
- Ensure commercial infill buildings adopt a contemporary architectural form or simplified interpretation of nearby contributory buildings.
- Discourage the introduction of architectural features, where it is known that these features were not originally present.
- Discourage signage above the verandah if it results in visual clutter in the streetscape and obscures views of the subject building and nearby contributory buildings.
- Ensure retention of signage deemed to have heritage value.
- Discourage sky signs, reflective signs, animated signs and electronic signs within heritage precincts

1.5 ZONES

1.5.1 CLAUSE 34.01 COMMERCIAL 1 ZONE (C1Z)

The NAC is predominantly included within the Commercial 1 Zone the purpose of which is (amongst others) to support vibrant mixed use commercial centres for retail, office, business, entertainment and community uses, and additionally provides for residential uses at densities complementary to the role and scale of the commercial centre.

The zone contains decision guidelines that ask decision makers to consider certain aspects of development when deciding planning applications. The most relevant decision guidelines for this zone are:

Amenity

- Consideration of the interface with adjoining zones, especially the relationship with residential areas.
- Consideration of the overlooking and overshadowing as a result of building or works affecting adjoining land in a General Residential Zone, Neighbourhood Residential Zone, Residential Growth Zone or Township Zone.
- The objectives, standards and decision guidelines of Clause 54 and Clause 55. This does not apply to an apartment development. For an apartment development, the objectives, standards and decision guidelines of Clause 58.

Built form

- Consideration of the streetscape, including the conservation of buildings, the design of verandahs, access from the street front, protecting active frontages to pedestrian areas, solar access, the treatment of the fronts and backs of buildings and their appurtenances, illumination of buildings or their immediate spaces and the landscaping of land adjoining a road.

Transport

- Considerations relating to the movement of pedestrians and cyclists, and vehicles providing for supplies, waste removal, emergency services and public transport.

1.5.2 CLAUSE 32.08 GENERAL RESIDENTIAL ZONE (GRZ1 AND GRZ2)

A small number of allotments within south-west of the Glen Huntly Road and Hawthorn Road intersection are included within Schedule 1 to the General Residential Zone while five allotments to the east of Hawthorn Road are included within Schedule 2 to the General Residential Zone (see Figure 6).

The General Residential Zone encourages a diversity of housing types and housing growth particularly in locations offering good access to services and transport, and additionally to allow educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

Schedule 1 to the General Residential Zone is designated 'Neighbourhood Centre Areas'.

Schedule 2 to the General Residential Zone is designated 'Tram Routes, Selected Main Roads and Residential Transition Areas'.

In respect to dwellings and residential buildings on land subject to the GRZ, residential developments must not exceed a height of 11 metres in the GRZ1 and GRZ2.

The relevant decision guidelines of the General Residential Zone ask decision makers to consider:

Residential development

- For the construction and extension of two or more dwellings on a lot, dwellings on common property and residential buildings, the objectives, standards and decision guidelines of Clause 55. Clause 55 addresses issues such as neighbourhood character, amenity impacts and on-site amenity.
- Whether subdivision will result in development which is not in keeping with the character and appearance of adjacent buildings, the streetscape or the area.

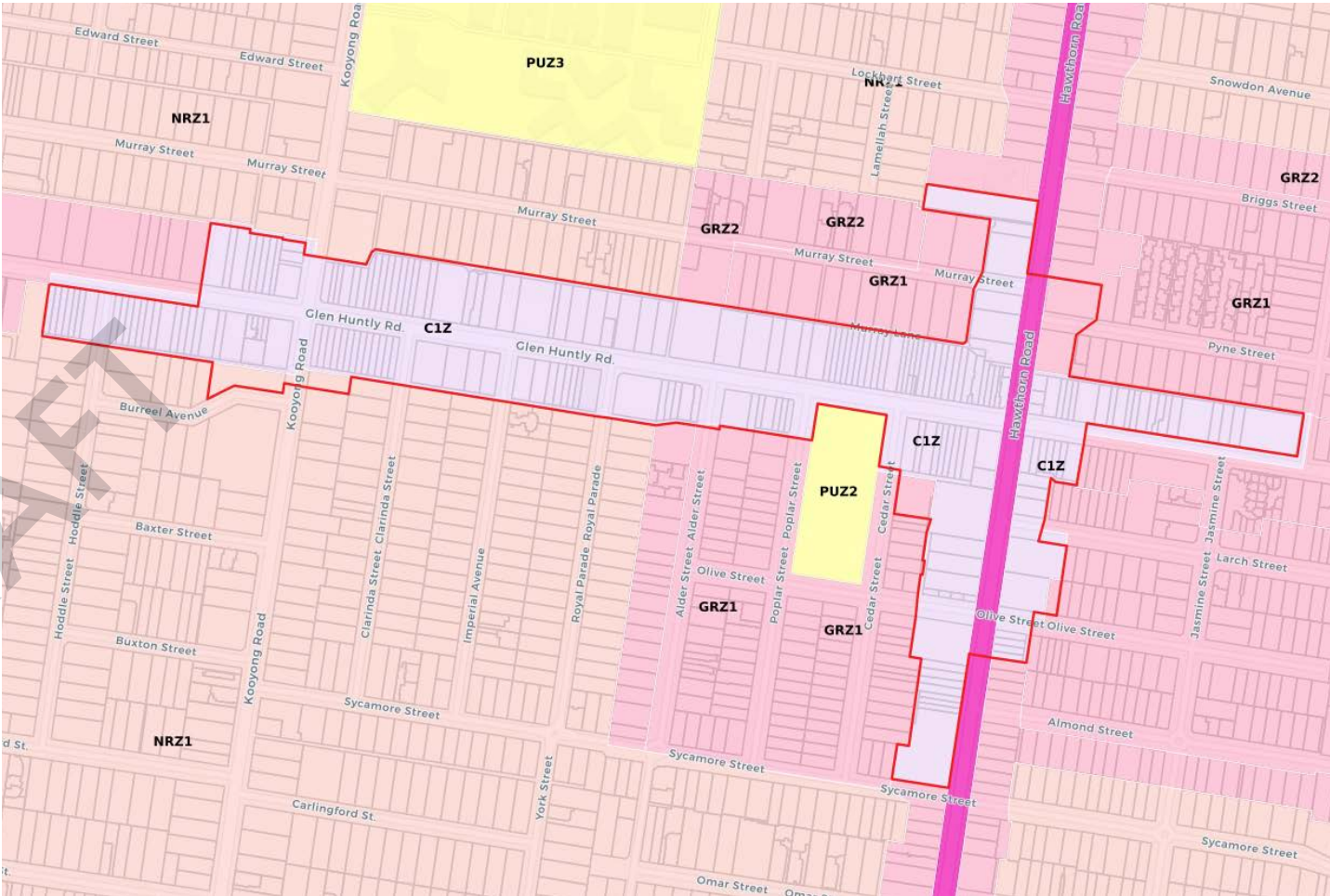


Figure 6. Planning Zones

1.6 OVERLAYS

Non-residential use and development

- Whether the use or development is compatible with residential use.
- Whether the use generally serves local community needs.
- The scale and intensity of the use and development.
- The design, height, setback and appearance of the proposed buildings and works.
- The safety, efficiency and amenity effects of traffic to be generated by the proposal.

All development

- Whether opportunities exist to avoid a building being visually obtrusive through the use of alternative building designs, particularly for developments with overall building heights in excess of 10.5 metres.
- The layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed off-street car parking.

1.5.3 CLAUSE 32.09 NEIGHBOURHOOD RESIDENTIAL ZONE (NRZ1)

Two allotments within the southern portion of the Study Area, intersected by Kooyong Road, are included within Schedule 1 to the Neighbourhood Residential Zone. The purpose of the Zone is to recognise areas of predominantly single and double storey residential development, and to ensure development respects the identified neighbourhood character, heritage, environmental or landscape characteristics; whilst allowing educational, recreational, religious, community and a limited range of other non-residential uses to serve local community needs in appropriate locations.

Schedule 1 to the Neighbourhood Residential Zone is designated 'Minimal Change Areas'.

In respect to dwellings and residential buildings on land subject to Schedule 1 to the NRZ, a new dwelling or residential building must not exceed a height of 9 metres, and additionally must not contain more than two (2) storeys at any point.

The relevant decision guidelines of NRZ ask decision makers to consider:

Development

- The impact of overshadowing on existing rooftop solar energy systems on dwellings on adjoining lots in a General Residential Zone, Mixed Use Zone, Neighbourhood Residential Zone, Residential Growth Zone or Township Zone.
- For the construction and extension of one dwelling on a lot, the objectives, standards and decision guidelines of Clause 54.
- For the construction and extension of two or more dwellings on a lot, dwellings on common property and residential buildings, the objectives, standards and decision guidelines of Clause 55.
- Whether a subdivision proposal has sufficient land area and minimum dimensions to be in keeping with the surrounding subdivision and development patterns.
- Whether opportunities exist to avoid a building being visually obtrusive through the use of alternative building designs, particularly for developments with overall building heights in excess of 8 metres (where permitted).
- The layout and appearance of areas set aside for car parking, access and egress, loading and unloading and the location of any proposed off-street car parking.

Use

- In the local neighbourhood context:
- Whether the use or development is compatible with residential use.
- Whether the use generally serves local community needs.
- The scale and intensity of the use and development.
- The design, height, setback and appearance of the proposed buildings and works.
- The proposed landscaping.
- The provision of car and bicycle parking and associated accessways. Any proposed loading and refuse collection facilities.
- The safety, efficiency and amenity effects of traffic to be generated by the proposal.

1.5.4 CLAUSE 43.01 HERITAGE OVERLAY (HO66)

The Heritage Overlay aims to conserve and enhance heritage places of natural or cultural significance and to ensure that development does not adversely affect the significance of heritage places.

A portion of the Study Area spanning Alder Street from the west to Hawthorn Road in the east is affected by the Heritage Overlay, Schedule 66.

HO66 is an area specific Schedule to the Heritage Overlay, namely the 'Caulfield South Shopping Centre and Environs, South Caulfield'.

The relevant decision guidelines ask decision makers to consider:

Heritage significance

- The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.
- Any applicable statement of significance (whether or not specified in the schedule to this overlay), heritage study and any applicable conservation policy.

Built form

- Whether the location, bulk, form or appearance of the proposed building will adversely affect the significance of the heritage place.
- Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.
- Demolition, works and subdivision
- Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place.
- Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.
- Whether the proposed subdivision will adversely affect the significance of the heritage place.

- Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.

Solar energy systems

- Whether the location, style, size, colour and materials of the proposed solar energy system will adversely affect the significance, character or appearance of the heritage place.

1.5.5 CLAUSE 44.05 SPECIAL BUILDING OVERLAY

The Special Building Overlay applies to land predominantly north of Balaclava Road, including Balaclava Road. The Overlay identifies land in urban areas liable to inundation by overland flows from the urban drainage system and seeks to ensure development maintains the free passage and temporary storage of floodwaters, minimises flood damage, is compatible with the flood hazard and local drainage conditions and will not cause any significant rise in flood level or flow velocity.

Applications are assessed against a set of decision guidelines and are referred to the relevant floodplain management authority for comment.

1.5.6 CLAUSE 45.03 ENVIRONMENTAL AUDIT OVERLAY

The Environmental Audit Overlay applies to a cluster of sites within the southern portion of the Study Area. The purpose of the EAO is to ensure that potentially contaminated land is suitable for a use which could be significantly adversely affected by any contamination.

The EAO requires a certificate of environmental audit or a statement to be issued prior to the use of the land for a sensitive use (residential use, childcare centre, pre-school centre or primary school), or before the construction or carrying out of buildings and works in associated with a sensitive use commences.

1.5.7 CLAUSE 45.09 PARKING OVERLAY (PO2)

Schedule 2 to the Parking Overlay applies to the entire NAC. Schedule 2 to the Parking Overlay is titled ‘Student Housing in Specific Areas’. Where student housing is proposed in this area, the schedule aims to provide car parking at a rate commensurate with the reduced ownership pattern of students, allowing a reduced rate of on-site car parking of 0.5 spaces per student housing bed provided.



Figure 7. Heritage Overlay and Neighbourhood Character Overlay Plan



Figure 8. Special Building Overlay Plan



Figure 9. Parking Overlay and Environmental Audit Overlay Plan

1.7 DEVELOPMENT CONTEXT

The Caulfield South NAC is a desirable location for more intense development, particularly residential. It is well-located, well serviced by public transport and offers a range of services and facilities. Planning policy at both the state and local level support the more intense development of this type of centre.

As a result, several development applications have been applied for, approved or constructed. Without detailed built form guidance in place, resultant development can appear ad-hoc and potentially not in keeping with the vision for a particular centre.

Currently more intense development has been centred on the Caulfield South NAC with some activity also occurring in the Caulfield Park NAC. Given the policy direction to locate more intense development within neighbourhood activity centres, it is prudent to develop detailed guidance for all the neighbourhood activity centres.

Notable development in the Caulfield South NAC includes:

348-354 Hawthorn Road, Caulfield South

An application for a nine storey building was refused by Council. The permit applicant appealed to the Tribunal who approved the building at eight levels based on amended plans substituted at the hearing. In its decision, the Tribunal found that the amended proposal substantially met all relevant provisions in the Glen Eira Planning Scheme and that the size of the site, its location within the NAC and lack of sensitive interfaces meant that the proposal would make a positive contribution to the centre. There was significant discussion around what was the most appropriate street wall height and commentary made that an appropriate street wall height will be different on Hawthorn Road than it is on Glen Huntly Road.

In particular, the Tribunal noted that acceptable building heights could not be based on heights developed for other centres. Any preferred heights needed to be based on detailed work for the specific centre taking into account its strategic and contextual basis.



348-354 Hawthorn Road, Caulfield South

371-377 Hawthorn Road, Caulfield South

This site has had a number of applications. A proposal for a nine storey building was refused by Council and that refusal upheld by the Tribunal. Amended plans were substituted at the hearing that reduced the overall height of the building to seven storeys. Although the Tribunal upheld Council’s refusal their main concern related to the design of the upper levels of the building and not the overall concept of the development.

The Tribunal noted the lack of detailed built form guidance and stated that any decision made on development needed to be context specific. The Tribunal also re-iterated comments that acceptable building heights could not be based on heights developed for other centres. The decision of the Tribunal contained speculation on preferred outcomes for the site, seemingly a consequence of the lack of overall direction.

A subsequent proposal for a six storey building was approved via consent at a Compulsory Conference.



371-377 Hawthorn Road, Caulfield South

380 Hawthorn Road, Caulfield South

This site has had a permit approved by the Tribunal (through consent at a Compulsory Conference) at five storeys.

388-394 Hawthorn Road, Caulfield South

An application for a seven storey building was refused by Council. The permit applicant appealed to the Tribunal and it was subsequently approved by VCAT on 15 January 2021.

As the Tribunal can only make decisions on the proposals it has before them, it is not in a position to best determine an overall and consistent approach to the built form guidance for the centres. However, it is clear that for Council to implement built form controls that are robust and defensible it needs to develop guidance that is based on both the policy context and the physical context and capacity of the centres. This will also likely result in areas within the same centre having higher built form outcomes than other areas due to site context and capacity.



380 Hawthorn Road, Caulfield South

2 ANALYSIS, ISSUES AND OPPORTUNITIES

The following chapter outlines analysis, issues and opportunities to be addressed through the development of the Built Form Framework for the Caulfield South NAC.

2.1 TOPOGRAPHY

Figure 10 reveals the elevation of landform within the NAC and surrounds, and identifies key topographic features.

The landform of Caulfield South rises gradually to a central ridgeline where the Holy Cross Church is located. This high point is noticeable in views along Glen Huntly Road from the west and east. Development near the hilltop will need to be of a high quality to enhance long views to the NAC.

Although the topography falls, it is relatively flat making it easy to walk along both Glen Huntly and Hawthorn Roads.

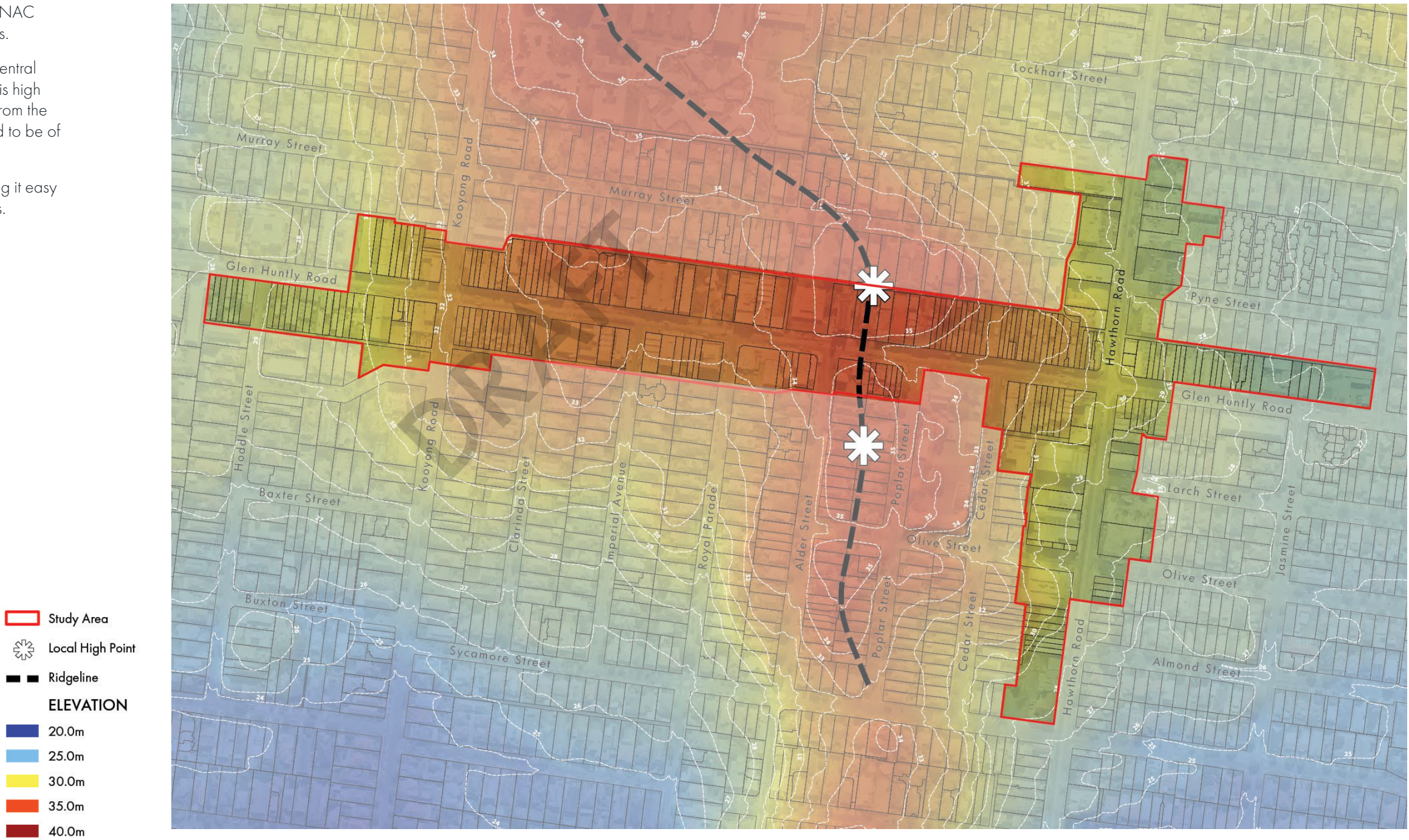


Figure 10. Caulfield South - Elevation

Figure 11 provides an assessment of land slope within the NAC.

The slope is generally subtle for properties along Glen Huntly Road with falls generally in the 0-2.5% range and some localised areas of 2.5-5.0% fall. There is a greater amount of slope across properties on the western side of Hawthorn Road with falls of approximately 2-4 metres across individual sites.

It will be important for development to respond to the fall across sites to ensure the ground level of buildings matches the street level as closely as possible.

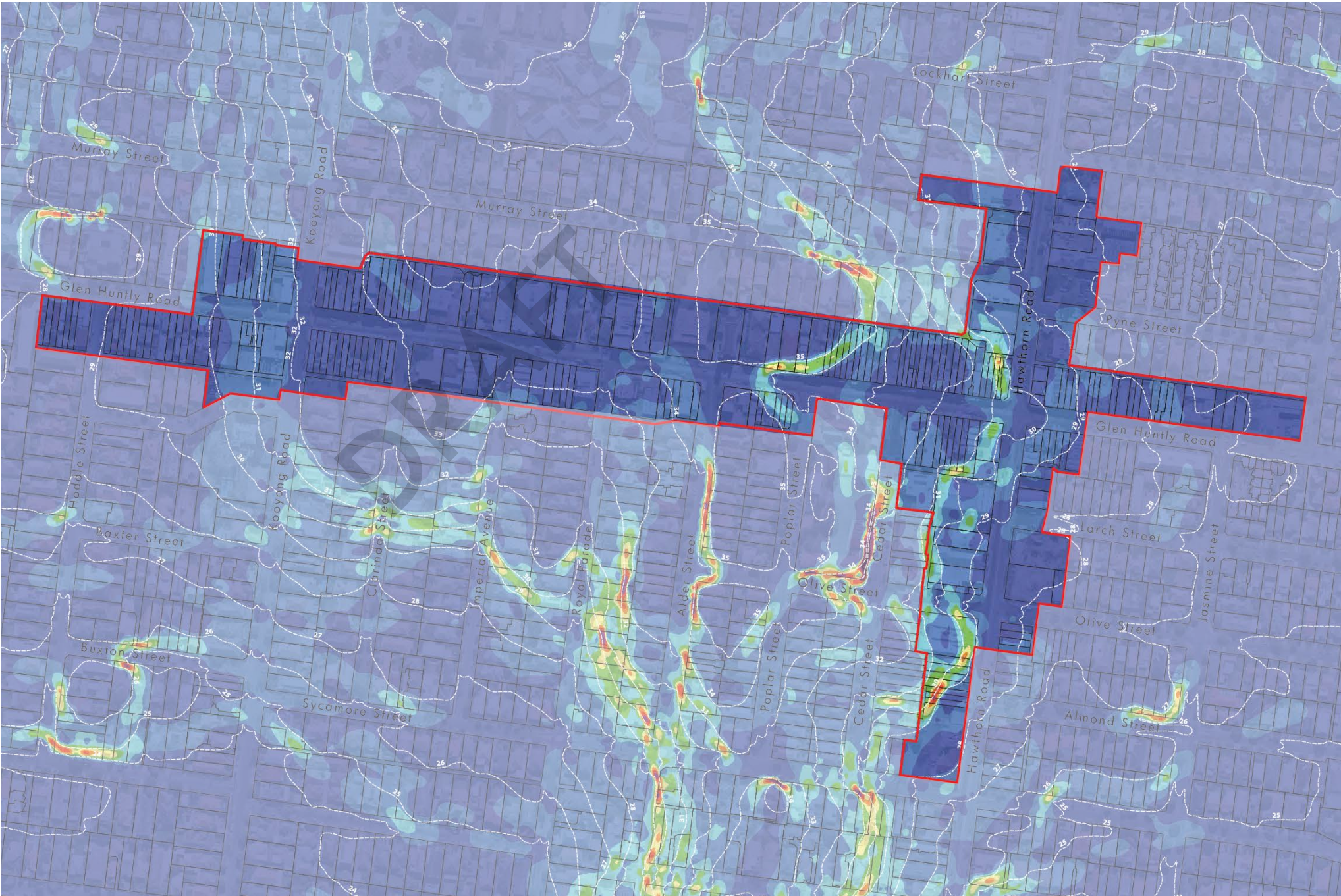
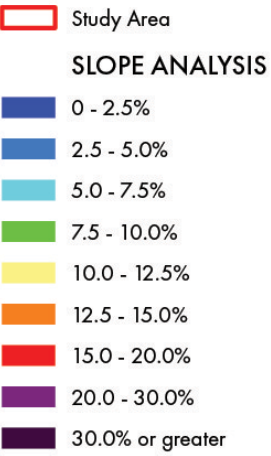


Figure 11. Caulfield South - Slope Analysis

2.2 LAND USE & INTERFACES

Figure 12 identifies land use precincts and range of interface conditions across the NAC.

The study area extends along Glen Huntly Road for a length of 1.1km and Hawthorn Road for a length of 520m. Its provides a local retail, hospitality and service role for the community (see Image 1). It is also very well served by public transport with two tram routes running through the centre.

The centre is broadly divided into four activity areas. Primary retail and hospitality activity is focused around the Glen Huntly Road and Hawthorn Road intersection, and the Glen Huntly Road and Kooyong Road intersection (see Image 3). Activity gradually reduces further away from these areas.

There is a secondary retail area along Glen Huntly Road, which sits between the two primary retail areas. Land uses within this area includes lower order retail, personal and business services and offices. There are several newly constructed mixed use developments and a small number of detached residential dwellings within the area. The mix of setbacks and uses diminishes street activity through the area.

Hawthorn Road is the least active part of the NAC. It includes a range of office uses, warehouse and personal and business services (see Image 4). Buildings are larger format with limited windows and active frontages to the footpaths. This is likely to improve with a number of approvals for mixed use developments. One approval includes a major supermarket which could significantly increase street activity along Hawthorn Road.

The NAC has a predominant interface with residential uses. There is a mix of laneways separating residential and commercial uses, and direct abuttal between the uses. There is a large section of the NAC with residential uses located to the south of commercial properties. Potential shadow impacts from new development will be the greatest in this location.

2.1.1 KEY ISSUES AND OPPORTUNITIES

- There are significant opportunities to enhance Hawthorn Road through new, mixed use developments that provide more active ground level uses.
- The central sections of Glen Huntly Road are less active due to a mix of setbacks and uses. There is an opportunity to enhance this area with new development that provides for greater activation of the footpaths (see Image 5).
- The residential interface to the south, west and east provide constraints for future building heights and setbacks. Development will require a sensitive design response to transition to adjoining residential areas (see Image 6).



Image 1. A mix of retail, hospitality and personal services activate the retail areas along Glen Huntly Road near the Hawthorn Road junction.



Image 3. Hospitality uses help to activate the western end of the NAC, near Kooyong Road.



Image 5. Activity diminishes in the central sections of Glen Huntly Road as this area is further away from the arterial road junctions and provides less customer focuses businesses.



Image 2. A prominent unarticulated side wall of a commercial building on Hawthorn Road. There are opportunities for highly visible buildings at entries to NAC to provide better articulation and enhance views to the centre



Image 4. Hawthorn Road's warehouse and commercial uses reduce pedestrian activity. There are significant opportunities to enhance this strip



Image 6. An interface between a recent mixed use development and residential. The development scales effectively and provides surveillance of the laneway. The development also provides a ground level setback to assist with servicing.

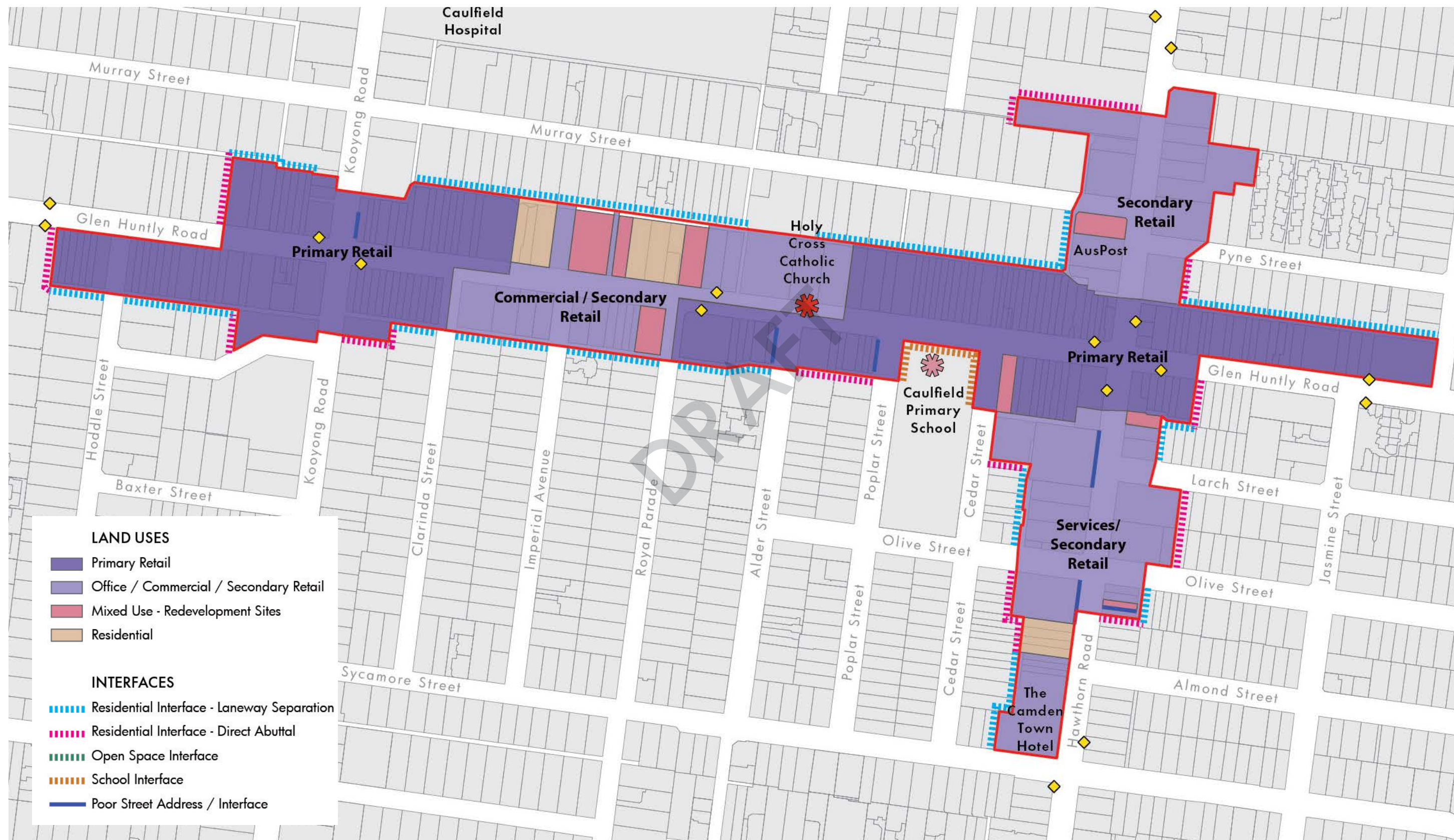


Figure 12. Land Use and Interface Analysis

2.3 BUILT FORM CHARACTER & PUBLIC REALM

Figure 13 maps a range of built form character and public realm elements across the Caulfield South NAC.

Understanding the existing and emerging built form and character of the Caulfield South NAC is important in order to identify heritage and character elements that should be protected, how built form could be improved and assess the impact of new, taller development on the streetscapes.

The Caulfield South NAC has a mixed built form character. The existing heritage precinct on Glen Huntly Road exhibits the strongest character in the centre with consistent double storey shopfronts from the interwar era (see images 7 and 9). The fine-grain character of these buildings provides for visual interest in the streetscape. The Heritage Precinct terminates at the tower of Holy Cross Catholic Church, which is located on a high point and is visually prominent.

This fine grain double storey shopfront character also exists on Glen Huntly Road east of Hawthorn Road however there are fewer interwar shopfronts than in the heritage precinct (see image 14). Similarly, east and west of Kooyong Road includes a number of double storey shopfronts from the interwar period (see image 13).

Outside of these areas on Glen Huntly Road, the built form character is mixed. Building setbacks are varied, original shopfronts are generally single storey and have less of a presence in the streetscape.

There are a number of more recent developments of up to five storeys dispersed along Glen Huntly Road. These buildings have a mixed impact on the streetscape. On the north side of Glen Huntly Road, recent development has varied upper level setbacks and does not create a cohesive street wall. These developments also have large vehicle entries to basement car parks which disrupts the retail activity (see images 11 and 17). On the south side of the road there is one recent development which makes a more positive contribution to the streetscape by retaining the existing two storey street wall and locating basement car park access to the rear of the site (see image 12).

Hawthorn Road also lacks a cohesive character and buildings generally provide a poor address to the street. This character will change, with a number of newly completed mixed use developments over three-storey, as well as other development applications and permits submitted and issued.

Existing awnings provide weather protection to footpaths along the primary retail areas of Glen Huntly Road. Weather protection is however limited along other parts of Glen Huntly Road and Hawthorn Road. This is a reflection of existing land uses in the NAC.

The footpaths along Glen Huntly Road and Hawthorn Road are key public spaces. They have good access to sunlight and provide good opportunities for on street dining and gathering. Although there is currently limited pedestrian activity along Hawthorn Road, this is expected to change as redevelopment occurs.

2.2.1 KEY ISSUES AND OPPORTUNITIES

- The consistent two storey shopfronts within the heritage precinct and other sections of Glen Huntly Road is an important character element which also provide a good sense of 'human scale' to the streets. There is an opportunity to adopt this street wall scale in new development (see Image 7).
- The street wall along Hawthorn Road is changing with a number of approvals for a three storey street edge. There is an opportunity to continue this scale in new development to create a cohesive streetscape.
- The heritage precinct is protected by Schedule 66 to the Heritage Overlay (HO66). It will be important to maintain the prominence of the heritage shopfronts within the streetscape by providing appropriate heights and upper level setbacks for new development.
- The Holy Cross Church is an important landmark in the NAC with clear views to the Church's spires along sections of Glen Huntly Road. The ground level setbacks on the neighbouring sites allow the tower to be seen from a distance. It will be important to maintain views to the Church and its spires (see Image 8).
- There are opportunities to strengthen the built form character on Hawthorn Road with high quality architecture and development that provides an improved interface with the footpath.
- The narrow or 'fine-grain' shopfronts focused along parts of Glen Huntly Road, are an important part of the streetscape. This should be retained and reflected in new development across the NAC to provide visual interest and a diversity of land uses.
- It will be important to maintain solar access to key pedestrian street in the activity centre particularly the southern footpath of Glen Huntly Road and the east and west footpaths on Hawthorn Road and Kooyong Road. This will support the role of the streets as key public spaces.
- There is generally a lack of "green" public spaces in the core retail area, and no public open space near the activity centre. There are opportunities to create more green elements in the streetscape, widen footpaths in strategic locations, and encourage public plazas and spaces through the redevelopment of larger sites.
- There are a number of recent developments on Glen Huntly Road with large basement car park entries to the street, which disrupts street activity and pedestrian movement. New development should provide access from existing laneways where they exist to prevent this from occurring.
- As the existing laneways are typically only 3 metres wide and do not support two way traffic, this may present constraints in the long-term for car parking and service access.
- There is no north-south pedestrian connection on the north side of Glen Huntly Road between Hawthorn Road and Kooyong Road apart from narrow laneways. There may be opportunities to create mid-block linkage and widen /activate laneways through the redevelopment of properties.



Image 7. The existing heritage shopfronts provide a consistent double storey street wall. It will be important maintain the prominence of the heritage buildings through the development of building heights and setbacks.



Image 8. The Holy Cross Church is visually prominent along Glen Huntly Road, particularly the two spires. The ground level setbacks on the neighbouring properties helps to keep this view open. It will be important to maintain views to this important landmark.

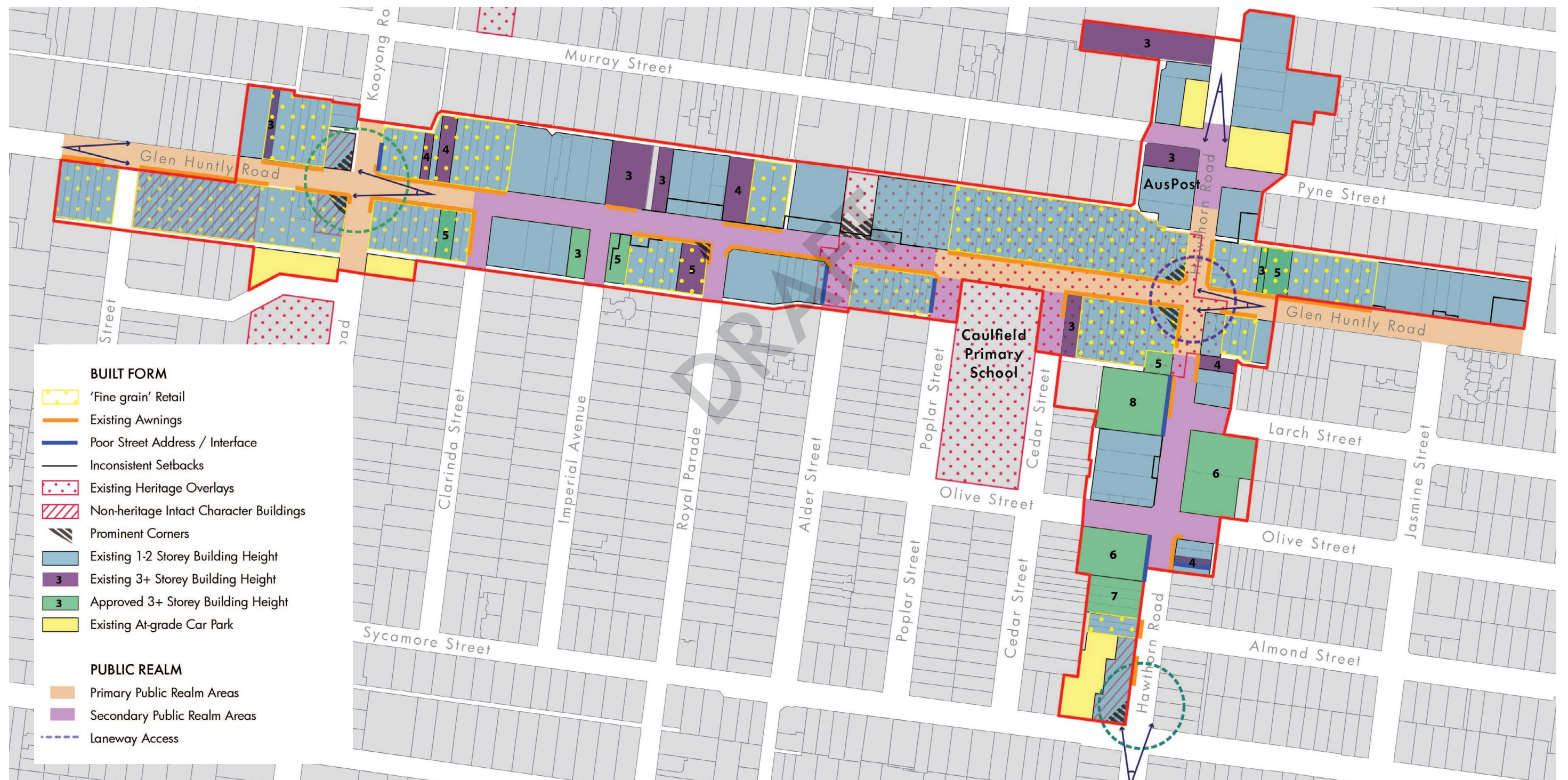


Figure 13. Built Form Character & Public Realm Analysis



Image 9. The double storey buildings in the Heritage Precinct create a consistent street wall height that should be maintained with new development



Image 10. A recent development at the western edge of the heritage precinct which exceeds the height of the heritage street wall. The building activates the secondary street frontage with active uses at ground level and weather protection



Image 11. The central section of Glen Huntly Road has a mixed built form character. Recent development has varied upper level setbacks and does not create a consistent street wall. Existing detached dwellings with ground level setbacks diminish street activity.



Image 12. A recent four storey development on Glen Huntly Road, near Kooyong Road which retains the original double storey shopfront and recesses additional development behind.



Image 13. The two images above show consistent two storey street buildings on Glen Huntly Road, east and west of Kooyong Road. These buildings are not covered by a Heritage Overlay however they make an important contribution to the streetscape character.



Image 14. The two images above show consistent two storey street buildings on Glen Huntly Road, east of Hawthorn Road. These buildings are not covered by a Heritage Overlay however they make an important contribution to the streetscape character.



Image 15. The retail areas of Glen Huntly Road provide a good pedestrian environment with a diversity of uses, outdoor dining and continuous weather protection. There are opportunities to fill the activity gaps along Glen Huntly Road and Hawthorn Road through new development



Image 16. Hawthorn Road provides an undesirable pedestrian environment with tired concrete paving, inactive building frontages and inconsistent awnings for weather protection. There are opportunities for new development to enhance the streetscape.



Image 17. Recent mixed use development on Glen Huntly Road with a major entry to a basement car park. The entry disrupts the footpath and shopfront activity. Access should be provided from the rear where possible to avoid this outcome.



Image 18. Panorama of Hawthorn Road Northern section

2.3.1 EXISTING SHADOW ANALYSIS

Figure 14 demonstrates the shadows cast by existing buildings within the Caulfield South NAC. Development above 3 storeys and recent approvals are shown separately in green.

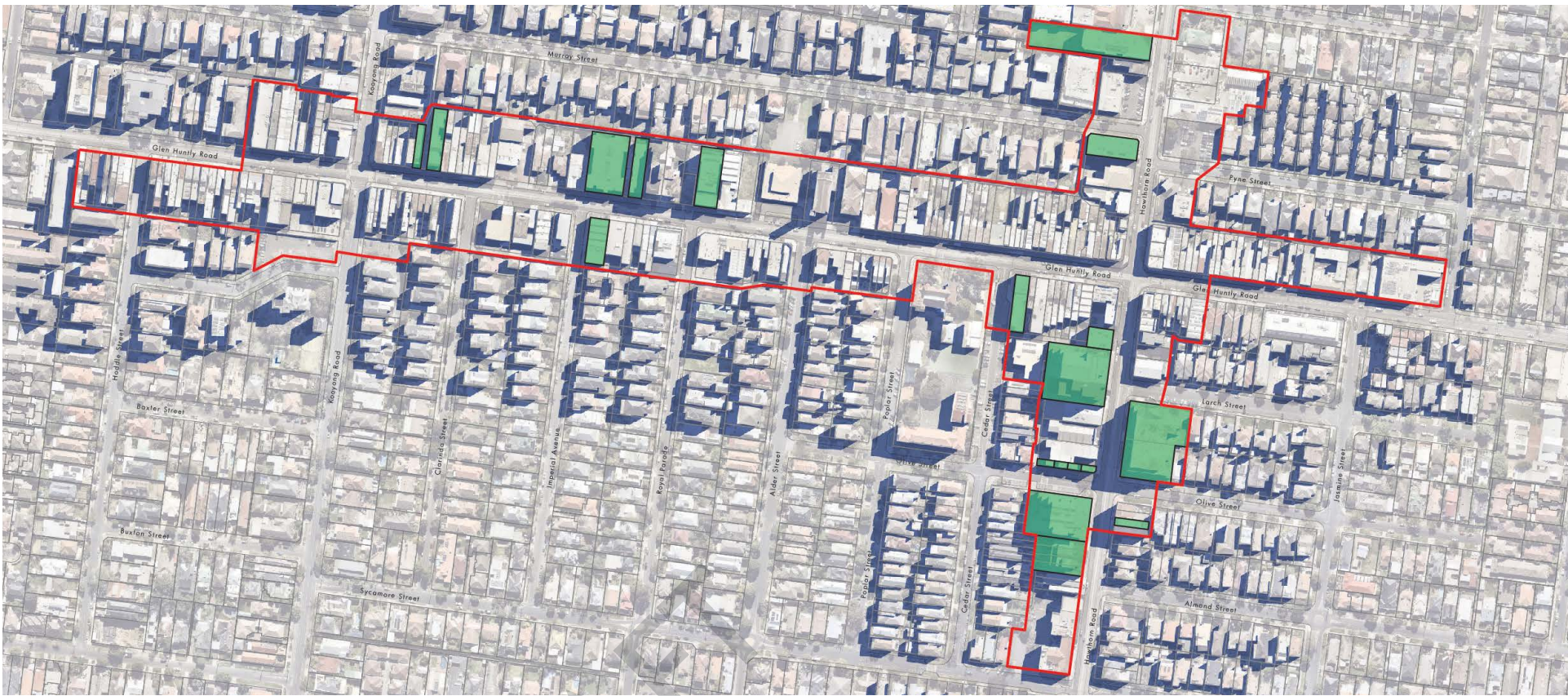
The date selected for the shadow analysis is September 22 - the equinox. This represents the mid-point between the summer solstice and winter solstice. It is the date commonly used in planning assessments to assess the impact of shadows.

The analysis demonstrates that more recently developed buildings on Hawthorn Road shadow the footpath at 9am and from 3pm onwards at Equinox. The lower scale shopfronts have minimal shadow impact on footpaths.

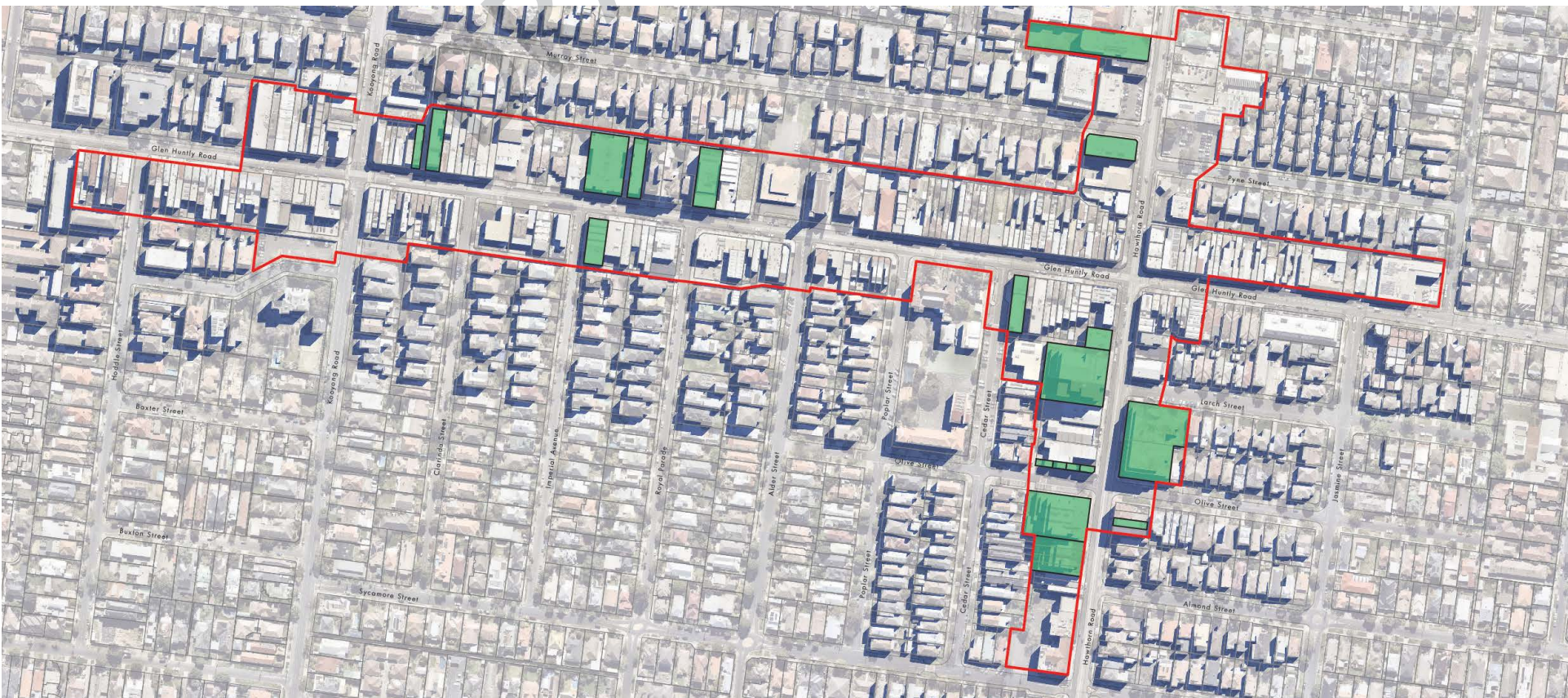
The analysis also demonstrates there is minimal shadow impact on existing residential areas from commercial buildings.

The 3D model for the shadow analysis has been developed using PSMA Geoscape building footprint data which provides an outline of the building footprint and overall roof height. The building footprints have been extruded up to the overall roof height and integrated into a topographic model.

9am - Sept 22



10am - Sept 22






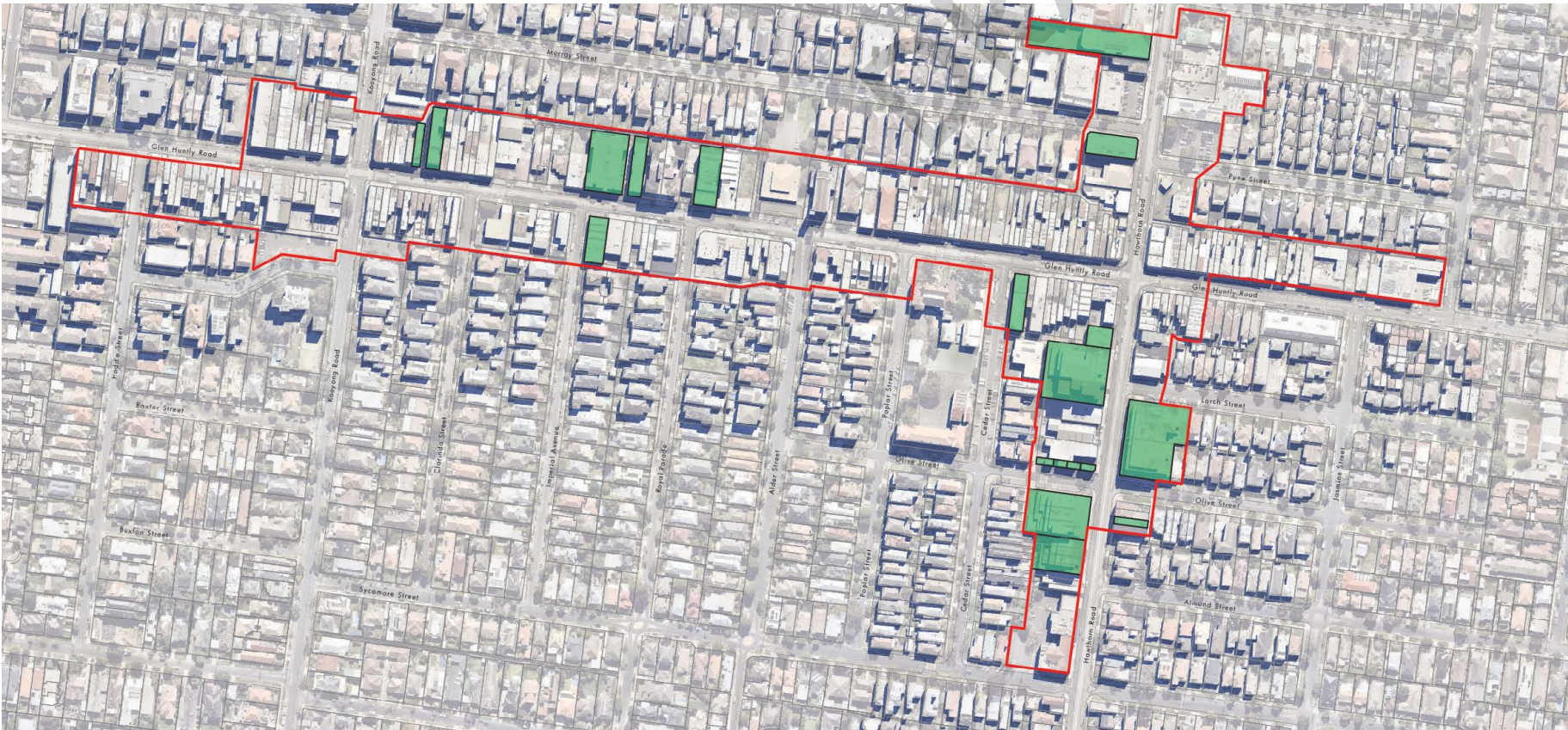
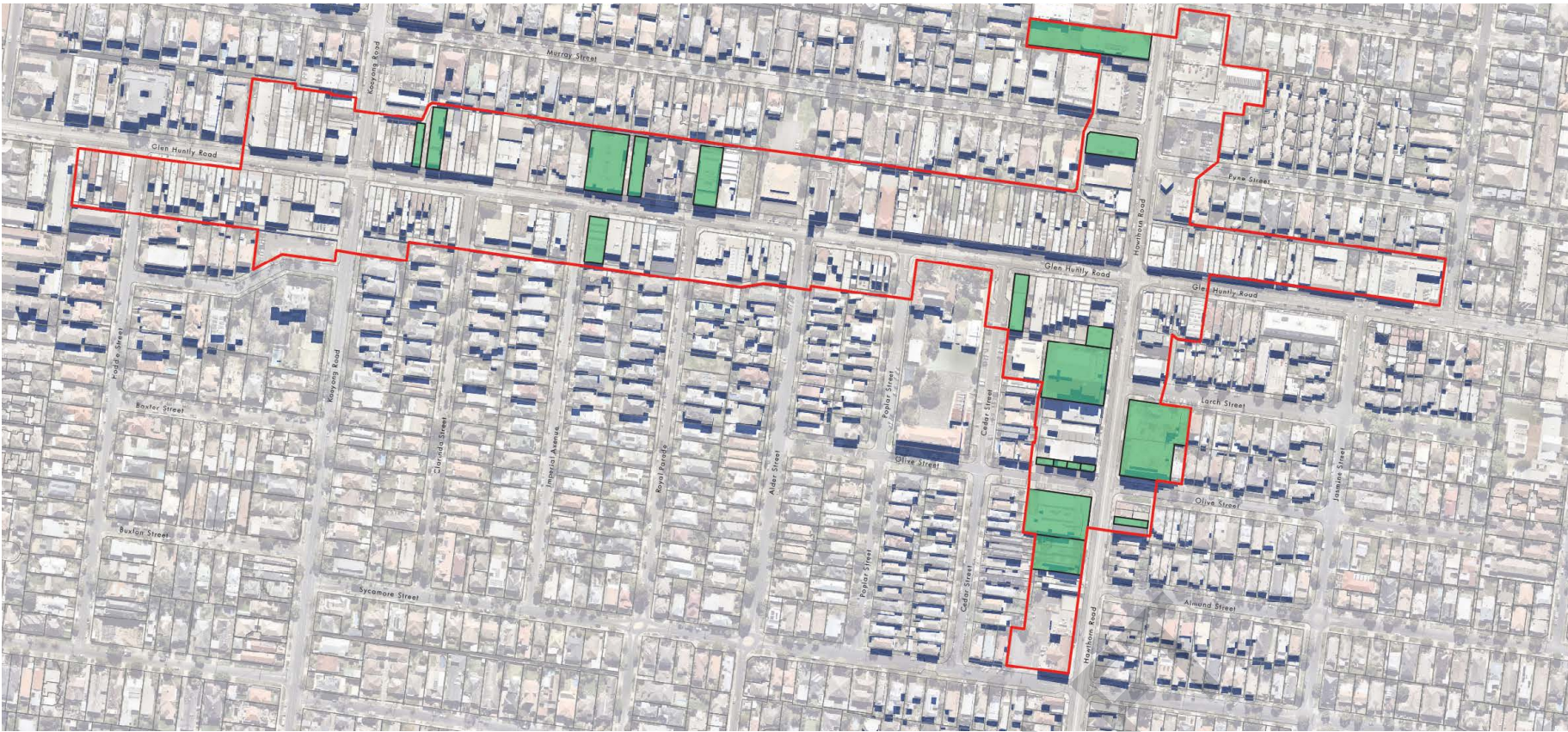
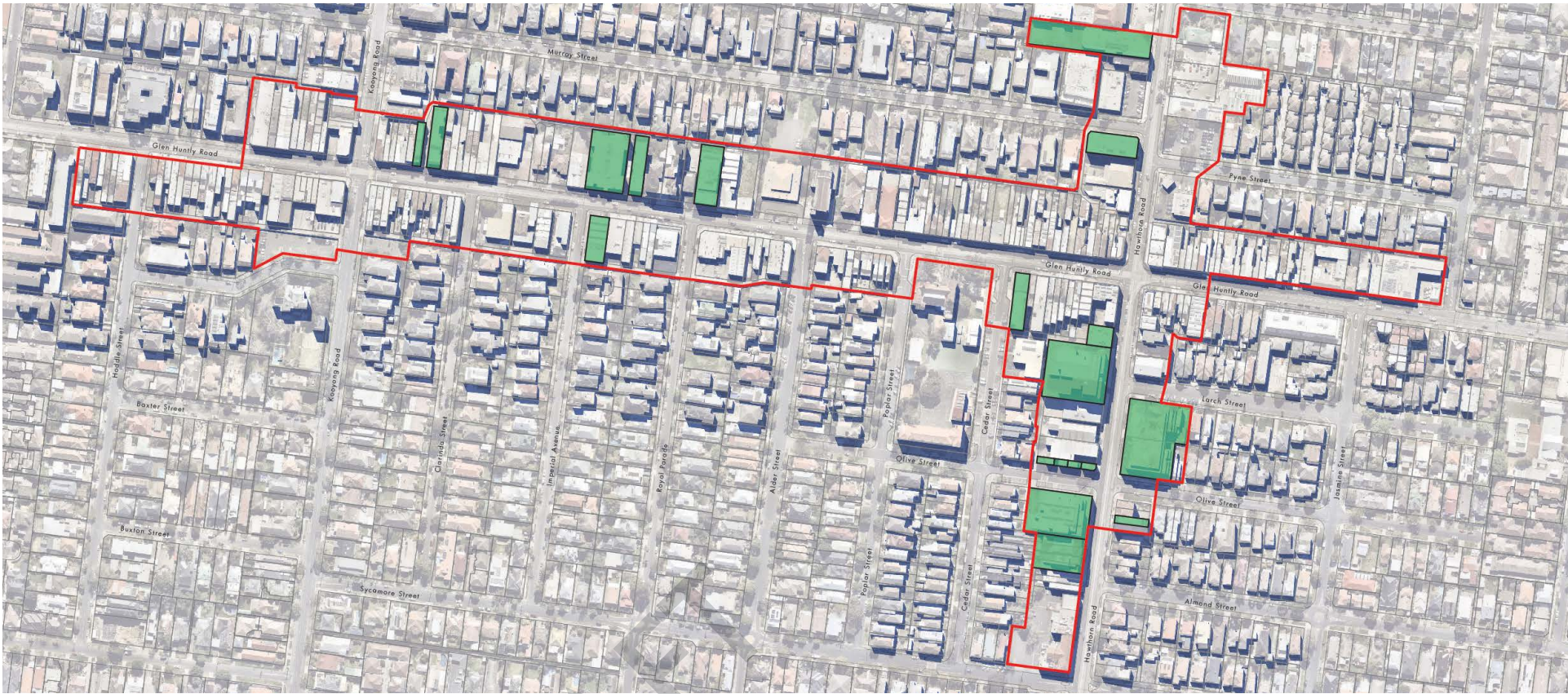
-  NAC Boundary
-  Approved / Constructed Development of 3 storeys or greater
-  Shadow from existing and approved buildings

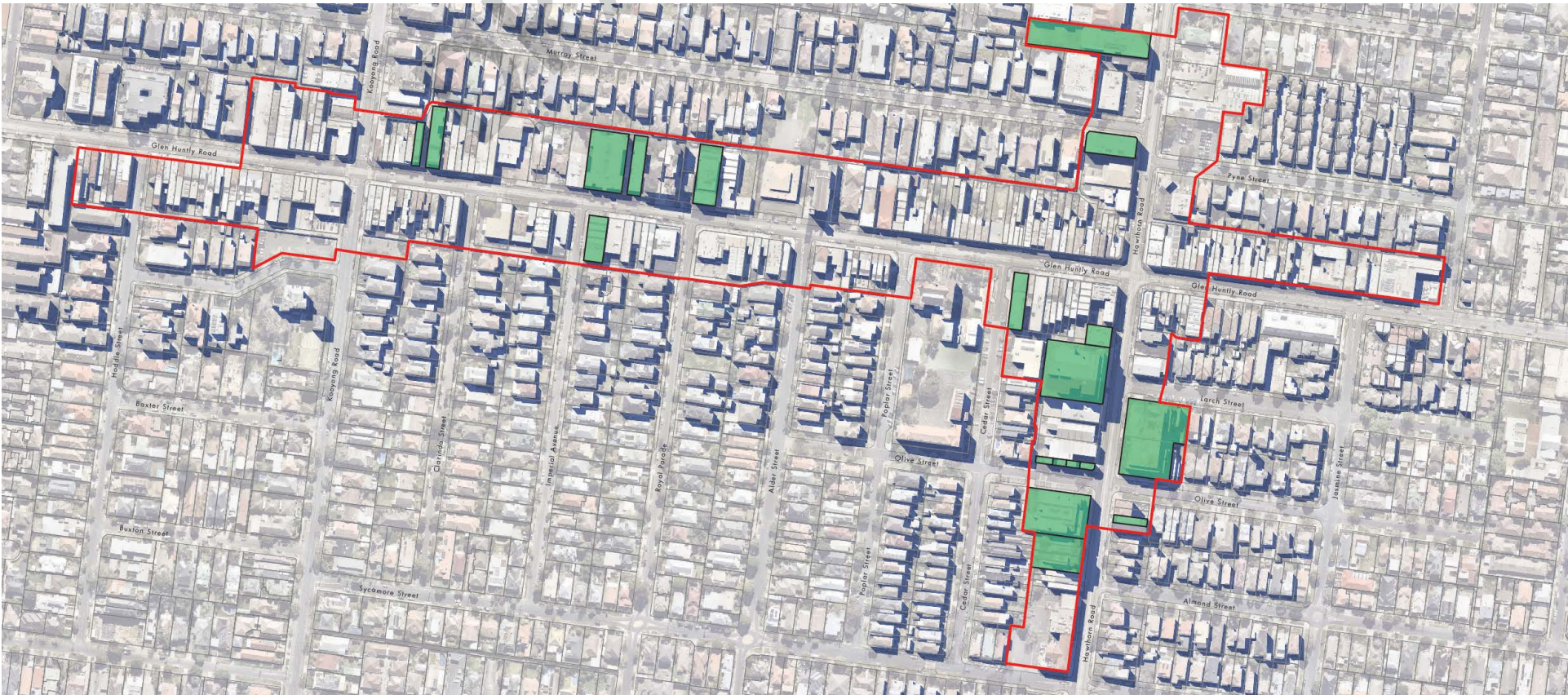
Figure 14. Existing Shadow Analysis

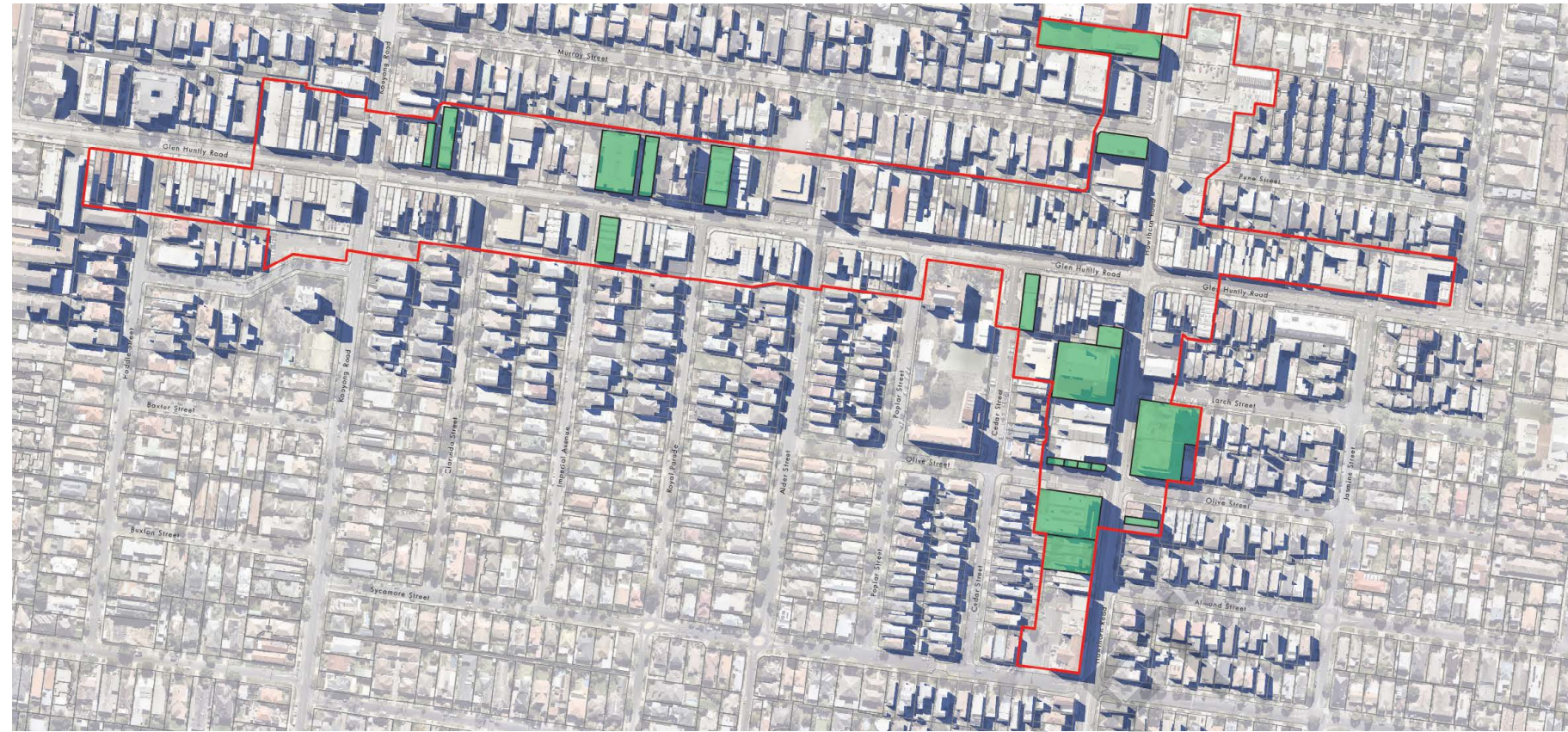


1pm - Sept 22



2pm - Sept 22





2.4 DEVELOPMENT ACTIVITY AND CAPACITY

Figure 16 maps recent development activity within the Caulfield South NAC, sites that are constrained for development and sites that present good development opportunities.

Development activity and capacity is an important consideration in preparing the Built Form Framework. Recent development can indicate the type of development that the centre will attract in the future. Analysing the potential capacity of the centre is also important to understand the scale and type of development that could potentially be accommodated.

Development activity within the Caulfield South NAC is generally focused along Hawthorn Road which has seen a number of recent development applications and VCAT decisions. The approved heights range between 5-8 storeys. A number of large sites remain along Hawthorn Road which will further change the character of the centre if redeveloped.

The central section of the centre between Kooyong Road and Royal Parade is another focus for development with a number of constructed buildings. Heights are generally lower than Hawthorn Road ranging between 3 and 5 storeys however there is one development of 8 storeys which was not supported by Council.

2.4.1 KEY ISSUES AND OPPORTUNITIES

- The mapping opposite identifies areas at the western and eastern ends of Glen Huntly Road where future development may be constrained, due to lot size, width and depth. Lot consolidation may be required to support development
- The existing heritage overlay area is likely to present a constraint to development as the prominence of the heritage buildings will need to be retained (see Image 20).
- There are a number of sites that present good opportunities for development based on lot size. These include a number of sites on Hawthorn Road (see Image 21) and sites in the central sections Glen Huntly Road. These sites generally benefit from laneway access or a secondary street frontage which can assist with future vehicle and service access.
- The eastern half of the centre has an interface with the General Residential Zone which allows development of up to three storeys. The western half has an interface with the Neighbourhood Residential Zone which limits development to two storeys. It will be important for future building heights to transition sensitively to the surrounding residential areas.

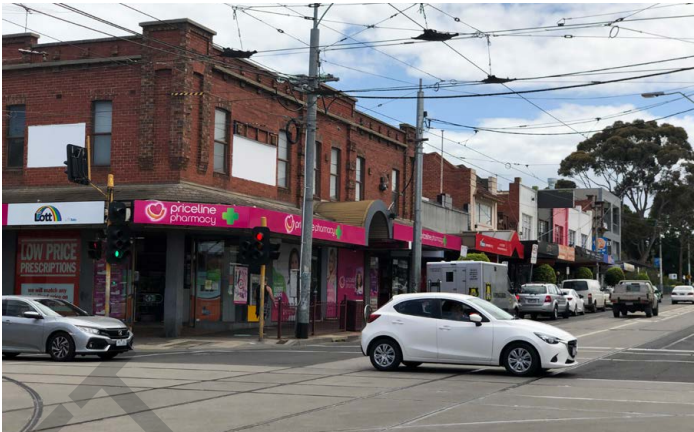


Image 20. Heritage properties on Glen Huntly road should be retained and will present a constraint for future development. These lots are also narrow in width.



Image 22. Narrow sites at the western end of Glen Huntly Road that may be constrained for development unless they are consolidated.



Image 21. Large sites on Hawthorn Road provide redevelopment opportunities.



Image 23. A recent development on Hawthorn Road of three storeys.



Figure 16. Development Activity and Capacity Analysis

2.5 LOT WIDTH ANALYSIS & TYPOLOGIES

Figure 17 provides analysis of lot widths across the Caulfield South NAC and Figure 18 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot width.

The typology analysis has been undertaken on the basis of a residential development outcome. Commercial development would be less constrained with less of a need to provide daylight into habitable rooms.

Front and rear facing apartments provide the simplest development outcome for narrow lots, as they can facilitate 1-2 apartments facing the street, and one facing the rear of the property, on each floor (subject to width). This type of development occupies the entire width of the lot, and does not provide any articulation or activation to either side of the development (which is assumed will develop in a similar manner).

Wider properties can accommodate a different model of development, podium and tower, which can have an outlook to other developments either side. This type of development can provide for a greater range of apartment types with varying aspects. It does however create issues of apartment separation for privacy and daylight that need to be considered.

The analysis indicates that there are mixed lot widths across the centre. Existing lot widths will limit development opportunities in the heritage precinct and at the western end of the centre, west of Clarinda Street.

Elsewhere lot widths will support a range of different outcomes for apartment developments. Hawthorn Road provides significant opportunities with the widest allotments.

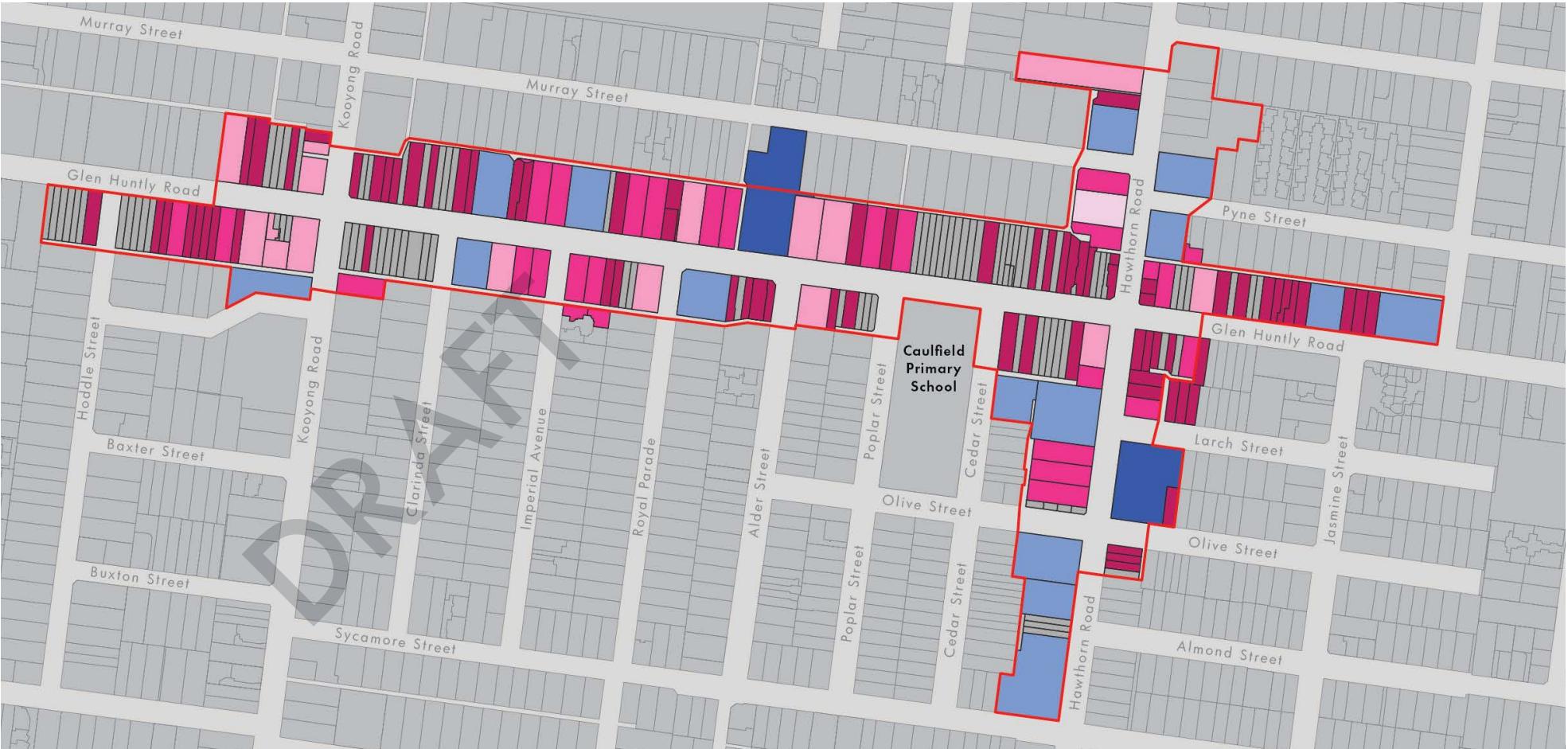


Figure 17. Caulfield South - Lot Width Analysis

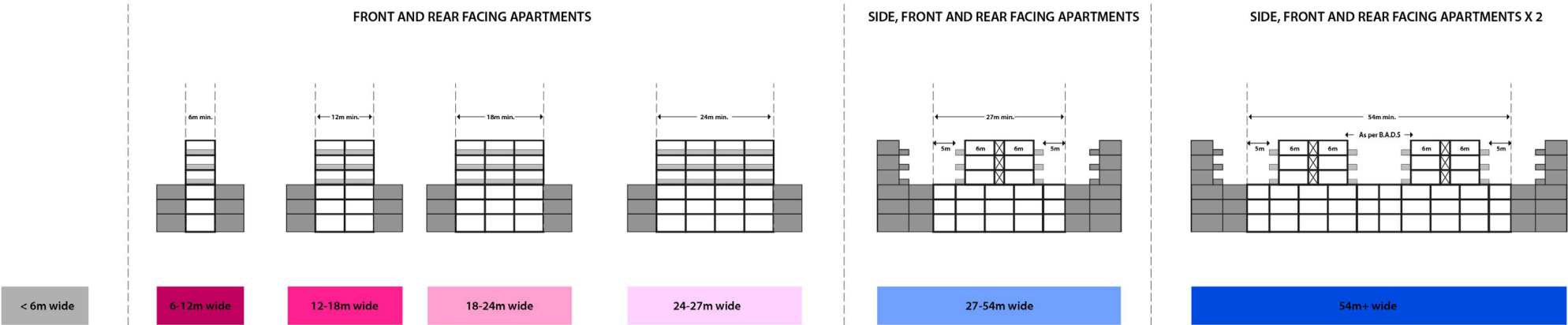


Figure 18. Lot Width Typologies

2.6 LOT DEPTH ANALYSIS & TYPOLOGIES

2.6.1 LOT DEPTH ANALYSIS & TYPOLOGIES

Figure 19 provides an analysis of lot depths across the NAC and Figure 20 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot depth.

Similar to lot width, the depth of a property has an impact on the type of development that can be provided as shown on the typologies opposite.

The typologies indicate that shallow lots provide a limited opportunity for development, and allow for only single aspect apartments. This takes into account a typical setback to a residential interface, and a suitable upper level setback to the primary street frontage.

Lots which are deeper provide opportunities for dual aspect apartments, addressing the primary street, and the rear of the lot.

Lots greater than 55m (approx.) could allow for two buildings with both front and rear facing apartments. This outcome does create apartment separation issues that need to be considered.

Lot depths across Caulfield South would generally be able to support front and rear facing apartments. Lots on the north side of Glen Huntly Road are deeper which will allow development to transition in scale to the adjoining residential uses.

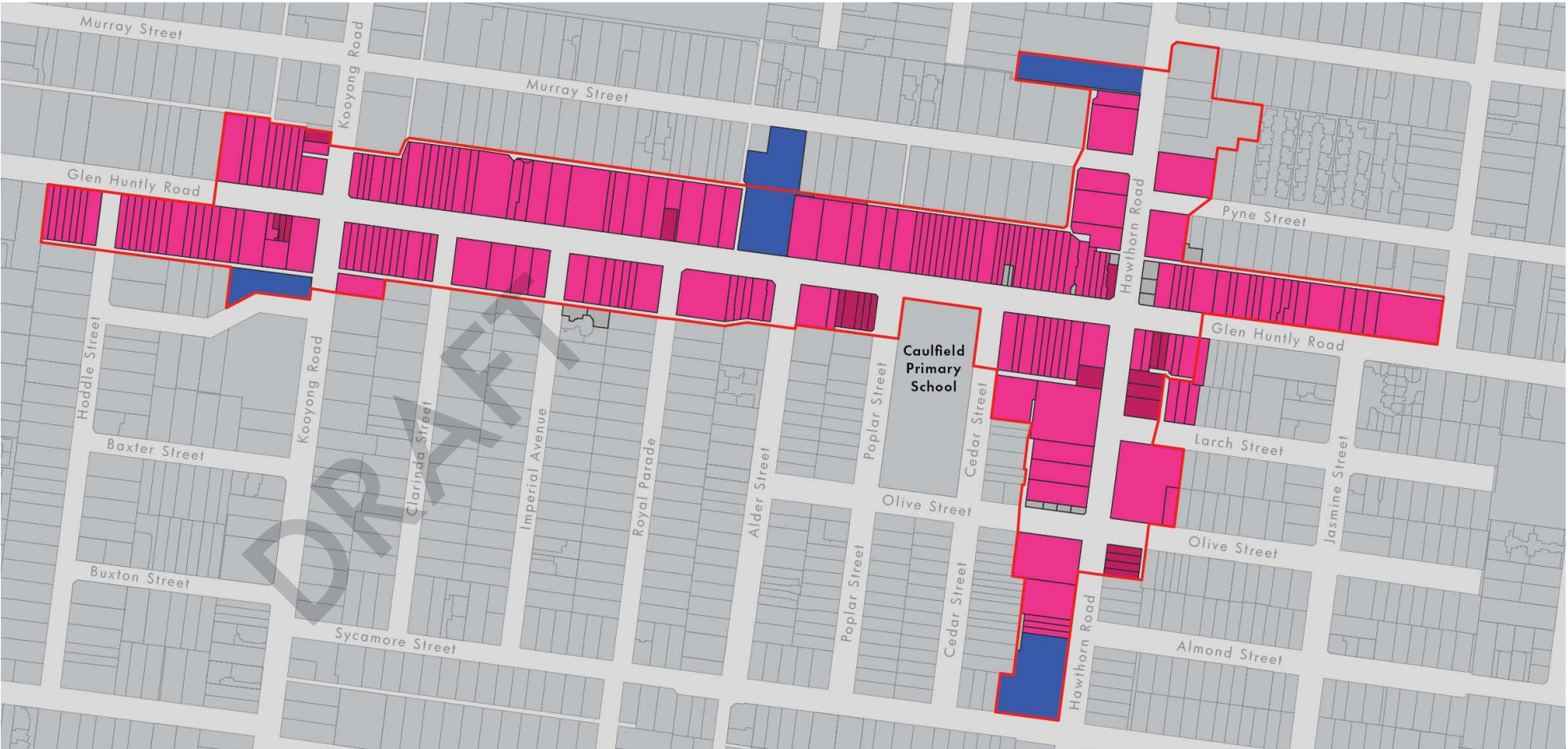


Figure 19. Caulfield South - Lot Depth Analysis

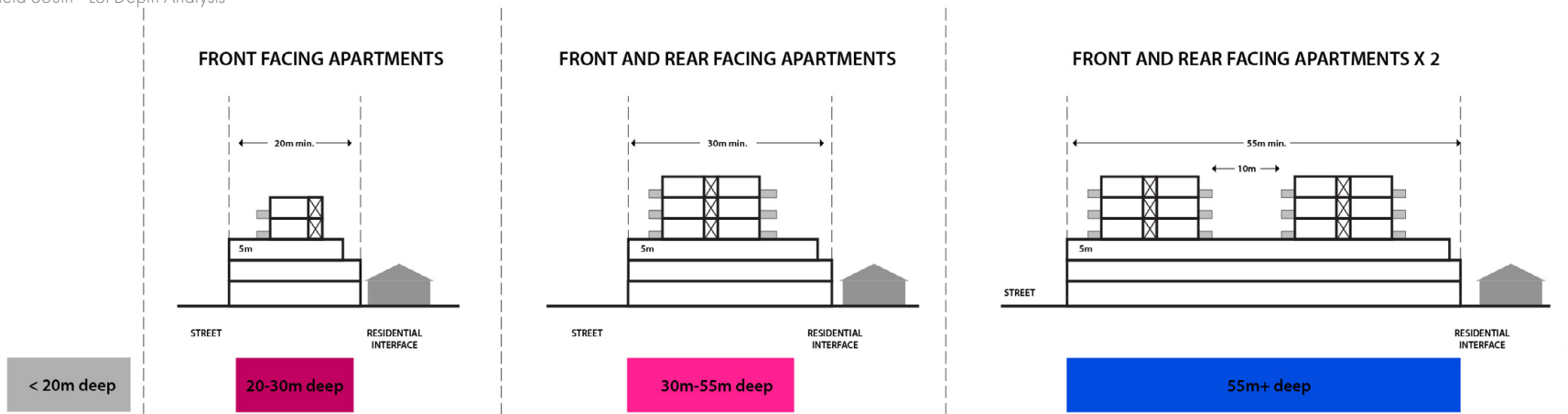


Figure 20. Lot Depth Typologies

PART 2 - THE BUILT FORM FRAMEWORK

3 DESIGN PRINCIPLES

3.1 THE PRINCIPLES

4 BUILT FORM FRAMEWORK

4.1 BUILT FORM FRAMEWORK

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

4.3 PRECINCT 1 - HAWTHORN ROAD

4.4 PRECINCT 2 - GLEN HUNTLY ROAD EAST

4.5 PRECINCT 3 - GLEN HUNTLY ROAD HERITAGE

4.6 PRECINCT 2 - GLEN HUNTLY ROAD WEST





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3 DESIGN PRINCIPLES

The following over-arching principles have been prepared to guide the drafting of design objectives and requirements for the Caulfield South NAC. The principles aim to respond to the issues and opportunities identified in Part 1 relating to amenity and character, and an understanding of the centre’s strategic role in meeting future housing and employment needs.

3.1 THE PRINCIPLES

PRINCIPLE 1 - A VIBRANT NEIGHBOURHOOD CENTRE WITH A MID-RISE CHARACTER THAT VALUES HERITAGE

MID-RISE CHARACTER

The approach for Caulfield South is to establish a mid-rise character providing for building heights in the 4-6 storey range. These heights will support further growth of the centre whilst sensitively integrating into the surrounding residential context and responding to the important heritage precinct along Glen Huntly Road and the Local Centre at the western end of the study area. The proposed heights also recognise the emerging character along Hawthorn Road, which is reflected in heights of 6 storeys in this location.

Glen Eira includes a mix of both Major Activity Centres (MACs) and Neighbourhood Activity Centres. The MACs play a significant role for the community providing a number of public transport options, and access to retail, services and employment opportunities. The NACs play an important, though less significant role in meeting future needs for housing and employment as they are smaller in size with less retail and employment opportunities. They tend to serve the surrounding local neighbourhoods.

The Glen Eira City Plan outlines the preferred focus for the NACs and notes that they will have a ‘medium’ focus for employment growth and a ‘moderate’ focus for housing growth. The majority of Caulfield South is identified as a Neighbourhood Activity Centre and as such is intended to experience moderate growth. However the western end of the Centre near Kooyong Road is identified as a local centre with ‘low opportunity for housing growth’.

The Caulfield South NAC is somewhat constrained for development. It includes a substantial heritage precinct, some areas of smaller lots, a residential interface along most of its edges and key footpaths which need to be protected from overshadowing. Combined together, these constraints will limit development opportunities.

PROTECT AND ENHANCE HERITAGE

Caulfield South includes a substantial heritage precinct recognised for its Inter-war shopfronts. It also includes the Holy Cross Church site, which is a major landmark along Glen Huntly Road.

The Built Form Framework will ensure these heritage buildings and their prominence in the streetscape is maintained.

This will be achieved by matching the street wall height of existing contributory and individually significant heritage buildings and adequately recessing upper levels to ensure the street wall is the dominant element in the streetscape. Overall building heights are also lower within the heritage precinct and mandatory building heights are proposed. Additional design details will ensure the new buildings and additions do not compete with the heritage facades.

For the Holy Cross Church, additional ground level setbacks are proposed on neighbouring sites to ensure it remains a dominant feature when looking east and west along Glen Huntly Road.



PRINCIPLE 2 - AN ENJOYABLE AND SAFE PLACE TO SHOP, WORK, GATHER, DINE AND EXERCISE

REINFORCE THE HUMAN SCALE TO KEY STREETS

Existing buildings within the Caulfield South NAC are generally low scale, between one and two storeys in height. The low scale character of the centre will change over time.

It is important that new, taller buildings are designed in a way that integrates with the existing low scale of the centre and do not dominate the streetscape. This is possible by providing a lower scale building at the street edge and setting the taller elements further behind.

A two-to-three storey building height at the street is recommended for the Caulfield South NAC, which will maintain a scale that is both compatible with existing heritage and non-heritage shopfronts, and does not overwhelm the streetscape. This has been evidenced by recent developments in the NAC that provide a three storey street wall.

Another aspect to the human scale in the Caulfield South NAC is the ‘fine-grain’ narrow shopfronts, which provide visual interest and provide for a greater diversity of uses and experiences. It is recommended this character is continued through new developments across the centre.

MAINTAIN SOLAR ACCESS TO KEY FOOTPATHS

The Caulfield South NAC is limited in terms of its public gathering spaces such as squares and parks. Therefore the footpaths play an important role as spaces for people to gather and interact. Maintaining sunlight to key footpaths is important to encourage outdoor dining and street based retail, and support the vitality of the centres.

The Built Form Framework Plan for the Caulfield South NAC identifies the key footpaths where access to sunlight is considered to be important. This includes the southern footpath of Glen Huntly Road, where retail and hospitality activity is currently focused, and the east and west footpaths of Hawthorn Road and Kooyong Road.

Sunlight access is often measured at the equinox (22 September) in Planning Schemes across Victoria. This date sits mid-point between the winter solstice (June 22) where shadows are at their longest, and the summer solstice (December 22) where shadows are at their shortest. For the Caulfield South Built Form Framework, the Equinox was selected as the date to measure solar access.

The following measures for solar access have been adopted for the Built Form Framework. These time periods will ensure sunlight is provided to the footpaths at the most active times of the day, which will help to support hospitality and retail uses.

These measures were tested and considered to provide a balance between providing good solar access whilst not unreasonably limiting development opportunities in the centre.

Maintain sunlight to key footpaths as follows:

- Southern footpaths of Glen Huntly Road - Solar access from 10am on September 22
- Western footpaths of Hawthorn Road and Kooyong Road - Solar access from 10am - 12pm on September 22
- Eastern footpaths of Hawthorn Road and Kooyong Road - Solar access from 12pm - 2pm on September 22

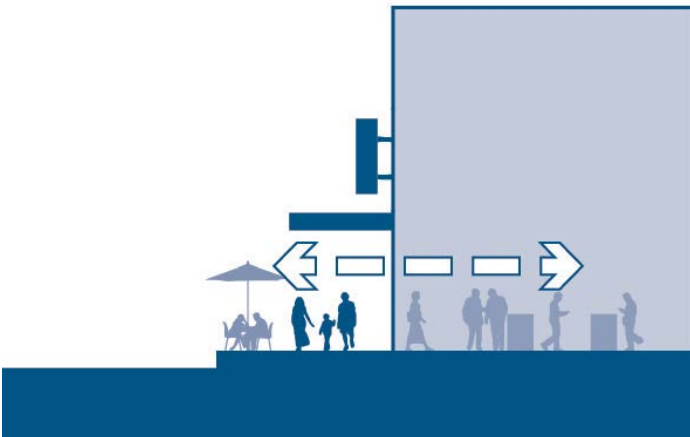
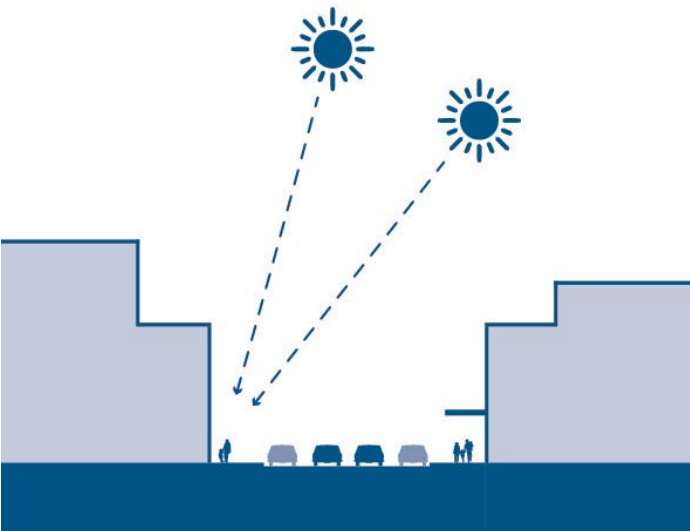
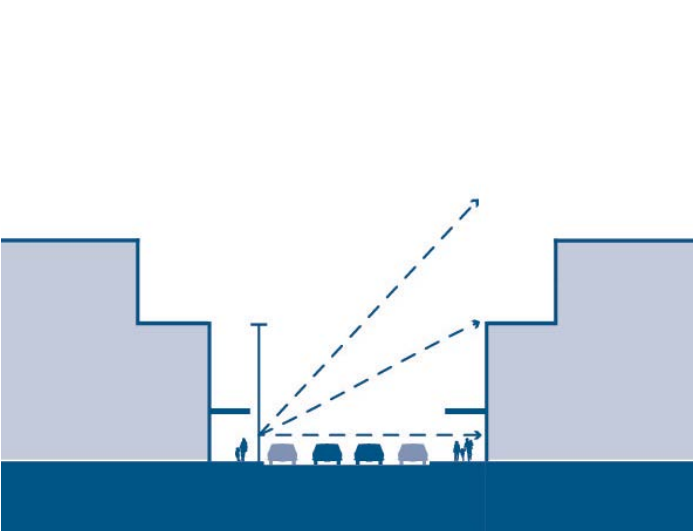
PROVIDE FOR A CONTINUOUS NETWORK OF ACTIVE FRONTAGES WITH WEATHER PROTECTION

Continuous retail and business activity across the Caulfield South NAC is key to providing a positive pedestrian experience. Locations where there are blank walls, car park or loading areas to the street, disrupt the flow of retail activity and provide limited passive surveillance of the footpaths.

It is recommended that all of Glen Huntly Road, Hawthorn Road and Kooyong Road are treated as active frontages, with windows at ground level, and uses at the front of the building that provide for customer engagement. Buildings on corners would provide active frontages to both streets. In addition, uses above the ground level are encouraged to address the street with windows and balconies.

All buildings should be constructed with floor to ceiling heights that would support retail, commercial and hospitality uses at ground level. This will allow for buildings to be easily adapted for such uses into the future and further strengthen street based activity.

Existing awnings provide weather protection along the majority of properties on sections of Glen Huntly Road within the core retail areas. New developments should continue this element along both Glen Huntly Road, Hawthorn Road and Kooyong Road, with awnings placed at lowered heights to reinforce an intimate pedestrian environment.



PRINCIPLE 3 - A CENTRE THAT EMERGES SENSITIVELY FROM THE SURROUNDING NEIGHBOURHOOD AND CONSIDERS RESIDENTIAL INTERFACES BOTH WITHIN AND OUTSIDE THE CENTRE

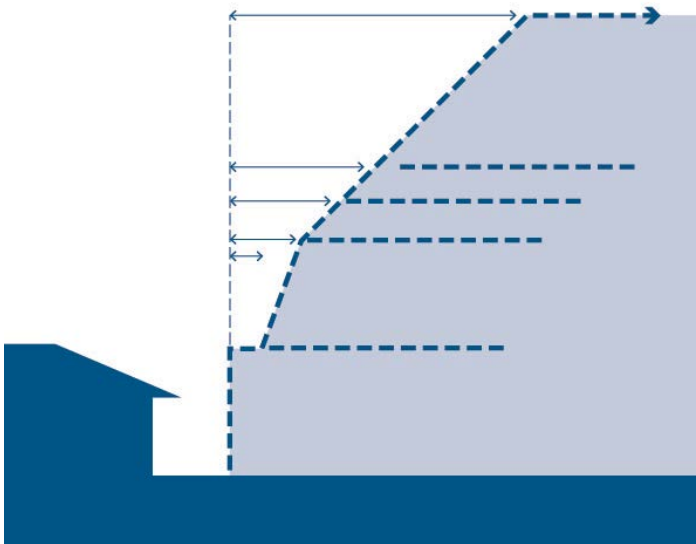
ENHANCE THE RESIDENTIAL INTERFACE

The Caulfield South NAC abuts residential uses along most of its edges. The interface varies between direct abuttal to a residential property or a laneway separating the uses.

It is important that development in the NAC transitions appropriately to the residential edge to protect the amenity of these areas.

For locations where there is a direct abuttal to residential uses, a single storey built form edge is proposed, which is then recessed to minimise visual impact and reduce the impacts of overlooking and overshadowing. Where a laneway exists, a two storey built form edge is proposed to the laneway which then recesses considerably.

In addition to the upper level setbacks, development will need satisfy relevant requirements from Clause 54, 55 and 58 of the Glen Eira Planning Scheme to further protect residential amenity.

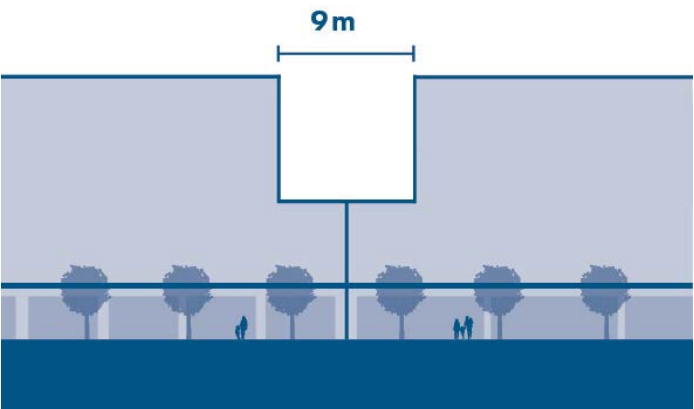


PROVIDE FOR EQUITABLE ACCESS TO AMENITY

Development across the Caulfield South NAC is likely to be sporadic with potential for new, taller buildings to be located adjacent to existing, low scale buildings for a substantial period of time. It is important to have measures in place to ensure the future development potential of adjoining sites is not significantly compromised by the first development.

A key consideration in equitable access is ensuring adjoining buildings have sufficient separation, to limit overshadowing and ensure adequate privacy for apartments and access to daylight. The centre-wide Requirements provide for a 9 metre separation distance between apartment developments where they have balconies or windows of habitable rooms facing each other.

However, for 1 to 3 storeys, zero side and rear setbacks are recommended in most cases. Land uses on these levels will typically be retail or office uses where access to sunlight and privacy issues are less critical.



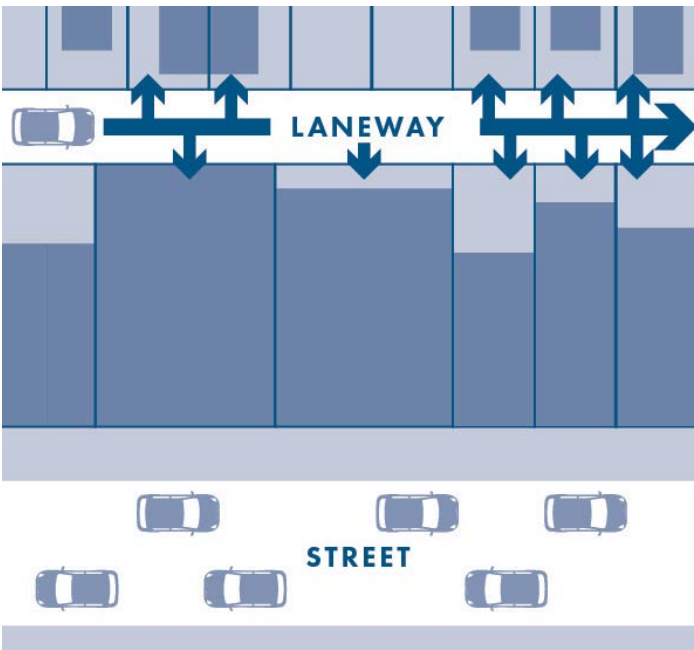
PRINCIPLE 4 - A WELL SERVICED AND ACCESSIBLE CENTRE

ENSURE ADEQUATE SERVICING OF EXISTING AND NEW DEVELOPMENTS

As the Caulfield South NAC grows and intensifies with additional development, so do its servicing and access requirements for buildings. This includes access to car parking for residents, workers and customers, access for service vehicles, and pedestrian and cycle access.

It is important that new development takes advantage of existing service access arrangements through laneways in order to avoid vehicle disruption to footpaths, and to prioritise ground floor active frontages and land uses rather than access and parking infrastructure. Where no laneway exists, driveway crossovers should be located on secondary frontages and minimised in width.

Another important consideration is ensuring new development can be accessed adequately by pedestrians and cyclists.



4 BUILT FORM FRAMEWORK

This section outlines Centre-Wide Design Objectives and Requirements that will apply to all development within the NAC, and more specific building height, setback and design requirements that apply to four Built Form form precincts identified in Figure 21.

4.1 BUILT FORM FRAMEWORK

Figure 21 - Built Form Framework Plan, identifies the preferred and mandatory heights, setbacks and other built form requirements across the Caulfield South NAC.

The proposed building heights have been developed through application of the design principles outlined above, responding to key issues and opportunities identified in Part 1 of this report, and testing of the built form outcomes through 3D modelling. The recommended heights have taken into account a number of considerations including responding to sensitive interfaces, ensuring solar access to key footpaths and public spaces, and understanding the development potential of properties based on size and heritage requirements.

This plan identifies the key public realm areas, where good access to sunlight is required. These areas include existing footpaths where there is a high amount of pedestrian activity. Upper level setbacks are recommended at these locations to maintain good access to sunlight.

The strategic justification and rationale for the application of building heights and setbacks in each precinct is discussed in detail in Sections 4.3 to 4.6.

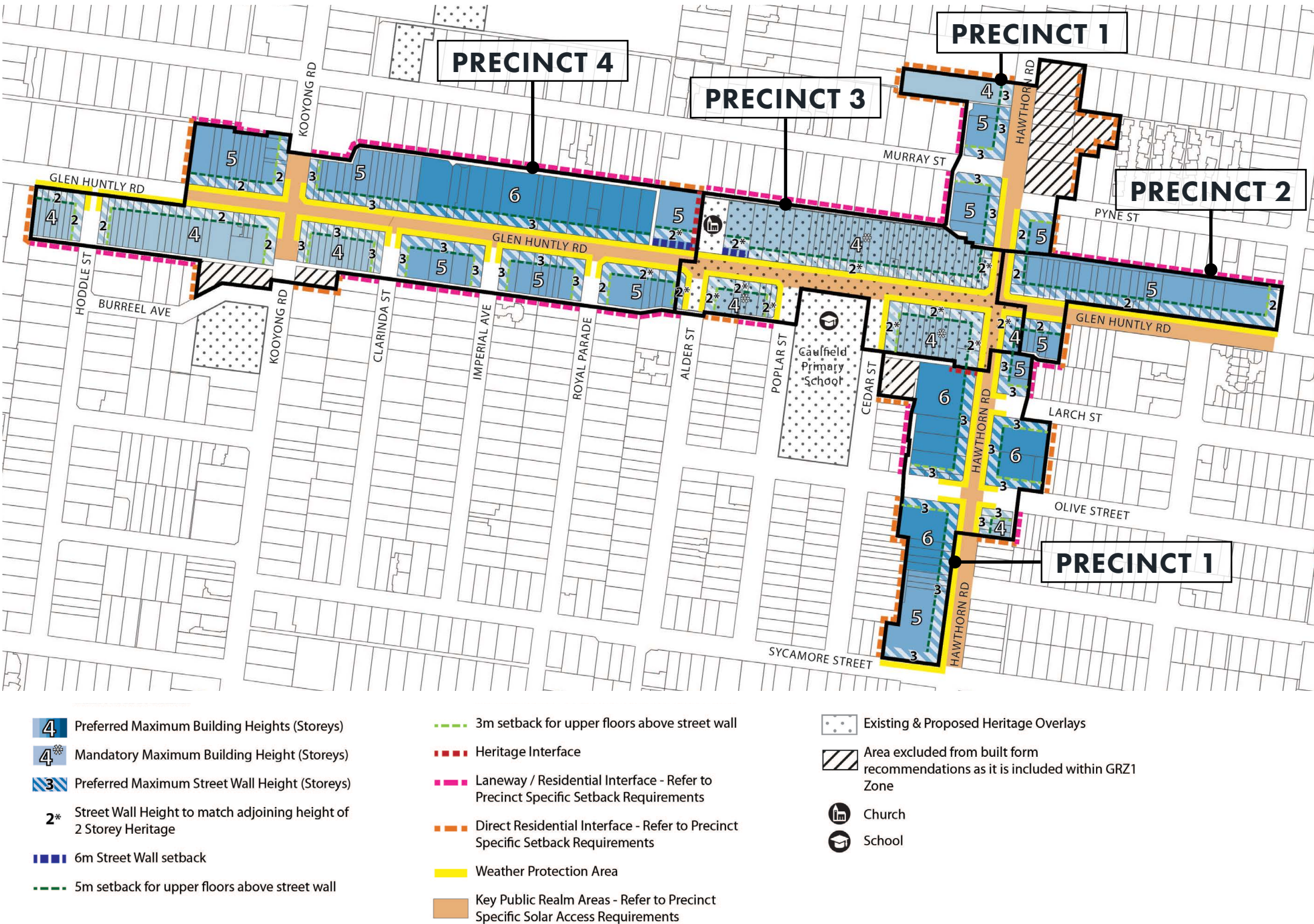


Figure 21. Built Form Framework Plan

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

4.2.1 DESIGN OBJECTIVES

- To support a new mid-rise scale built form character for the centre with lower built form at the street interface and to adjoining residential areas.
- To provide a more cohesive built form character and improved street interface along Hawthorn Road
- To ensure development maintains the prominence of heritage buildings within HO66 and respects the heritage the Holy Cross Church.
- To ensure development enhances the pedestrian experience through improved activation at ground floor and maintaining sunlight to the Glen Huntly Road, Hawthorn Road and Kooyong Road footpaths.
- To ensure development protects existing residential amenity and does not overwhelm adjoining residential properties.

4.2.2 DESIGN REQUIREMENTS

Building heights

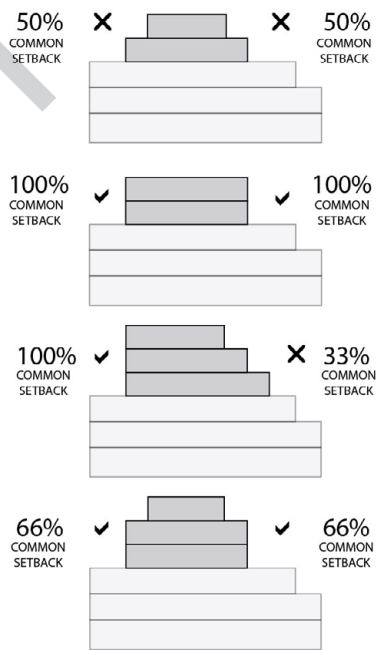
- The preferred maximum building height excludes rooftop services which should be hidden from view from any adjoining public space or designed as architectural roof top features. Roof top services includes but is not limited to plant rooms, air conditioning, lift overruns and roof mounted equipment.
- Buildings should be designed with a floor-to-floor dimension of a minimum of 4.0 metres at ground floor to enable adaptation for other uses in the future.

Building separation

- Where development shares a common boundary, upper level development should:
 - Be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
 - Be setback a minimum of 3.0m from the common boundary where a commercial or non-habitable window is proposed.

Building form and design

- Building design should minimise the visual bulk of large buildings through significant breaks and recesses in building massing.
- Development should avoid repetitive stepped building forms by providing a common street and rear setback for a minimum of 65% of the upper levels above street wall. Refer to diagram below:



- Buildings should reflect the existing fine grain pattern (narrow shopfronts) within the shopping strip by incorporating separate ground floor tenancies and vertically and horizontally modulated forms that integrate with the streetscape context.
- Roof forms should be integrated with the overall building façade design.
- Buildings should utilise materials that do not generate glare, and can withstand the effects of weathering.
- All new buildings are to incorporate best practice Environmentally Sustainable Development (ESD) principles. Refer to the Sustainable Design Assessment in the Planning Process (SDAPP) Framework.

Heritage Design

- These requirements apply to properties within an existing or proposed Heritage Overlay or abutting an existing or proposed Heritage Overlay.
- Facade design and details for infill buildings and new buildings should:
 - Be simple and not compete with the elaborate detailing of the adjoining heritage buildings.
 - Respect the vertical proportions of the heritage streetscape and adjoining heritage buildings.
 - Avoid large expanses of glazing except for ground floor shopfronts.
 - Adaptation and reuse of heritage buildings should:
 - Maintain existing openings and avoid highly reflective glazing in historic openings.
 - Encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings.
 - Maintain the inter-floor height of the existing building and avoid new floor plates and walls cutting through historic openings.
 - Development above the street wall on land within a Heritage Overlay or adjoining a Heritage Overlay should:
 - Be visually recessive and not dominate the heritage building and streetscape.
 - Utilise materials and finishes that are recessive in texture and colour.
 - Incorporate simple architectural detailing that does not detract from the heritage buildings and streetscape
 - Reflect the fine-grain pattern of subdivision of the streetscape.

Street interface

- Provide a minimum of 80% of the building façade at ground floor level with a visually permeable façade incorporating windows and door openings with clear glazing to ‘Key Public Realm Areas’.
- Buildings on corner sites should be designed to actively address both frontages at street level.
- Upper levels of buildings should be designed to provide habitable rooms with windows or balconies that overlook the public realm.
- Buildings fronting laneways should be designed for passive surveillance with a permeable façade, including windows and door openings.
- Ensure car parking is not visible from the street by incorporating design measures such as sleeving, or alternatively providing as basement parking.

Residential Interface

- Development should provide upper level setbacks in accordance with the precinct specific setbacks (refer to sections 4.3 to 4.6).
- Development must satisfy the objectives and standards of Clauses, 54,55 or 58, which aim to protect the amenity of adjoining residential areas.

Weather protection

- Provide verandahs on all buildings located in the ‘Weather Protection Areas’.
- Verandahs should be at an appropriate height above the footpath to avoid damage whilst still providing effective weather protection, generally between 3.0 and 4.0m and consistent with adjoining sites.
- Verandahs should be designed to mitigate the potential for visual clutter effects from light fittings, service cables and under awning signage.

Access and services

- Pedestrian entries to buildings should be clearly visible and easily identifiable from the street and accessible for all abilities.
- Residential entries should be distinguished from retail and commercial entries.
- Loading, service access and car park access should be provided from laneways and secondary streets. Where this not possible, vehicle crossovers should be minimised to reduce disruption to the footpaths and active frontages and located to avoid street trees if present.
- Provide appropriate setbacks at the rear of the building to laneways ensure adequate space for car park access and servicing. Further details at Clause 52.06 of the Glen Eira Planning Scheme.
- Screen air conditioning services, antennas and other utilities from public view using balcony treatments / roof structures / architectural elements. Avoid using walls to screen services.
- Avoid and minimise building services and utilities at ground floor street frontages to prioritise active frontages at these locations. Integrate services and utilities with the building design.
- Waste storage, loading and recycling facilities should be screened from public view. They should be easily accessed by residents and well ventilated.

Landscaping

- Communal garden spaces should be provided at podium and rooftop levels where appropriate to create amenity for residents, workers and visitors. The gardens should take into consideration, aspect, materials and solar orientation.
- Large development sites are encouraged to contribute to improved pedestrian amenity of the centre by providing for new private/public pedestrian/cycling links through the centre, where sites present an appropriate and useful opportunity to do so.

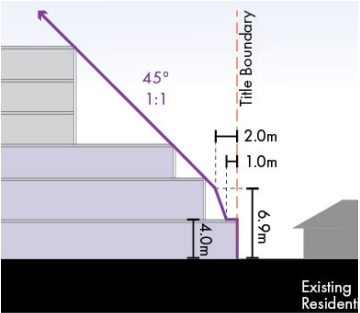
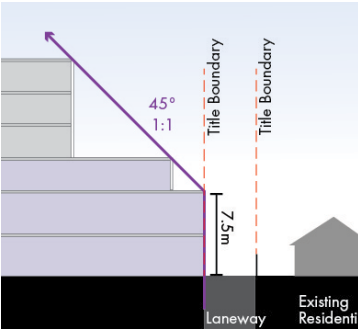
4.3 PRECINCT 1 - HAWTHORN ROAD

4.3.1 PRECINCT OVERVIEW

Hawthorn Road will continue to evolve into an active street providing more cohesive built form and a continuous street wall.

The large lots will accommodate significant development of up to six storeys and transition sensitively to adjoining residential areas.

4.3.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
<div>Preferred Maximum Heights</div> <div><ul style="list-style-type: none">Up to 4 storeys (15.0m) for land at 379-385 Hawthorn Road and 306 Hawthorn Road.Up to 5 storeys (18.0m) for land at 365-369 Hawthorn Road, 312-336 Hawthorn Road, 355 Hawthorn Road and 396-414 Hawthorn Road.Up to 6 storeys (21.0m) elsewhere.</div>	<div>Preferred Street wall Height</div> <div><ul style="list-style-type: none">3 storeys (11.0m).</div>	<div>Preferred Setbacks</div> <div><ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:</div> <div></div> <div><ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below:</div> <div></div>	<div>Solar Access</div> <div><ul style="list-style-type: none">Ensure solar access is maintained to the entire eastern footpath of Hawthorn Road from 12pm - 2pm on September 22.Ensure solar access is maintained to the entire western footpath of Hawthorn Road from 10am - 12pm on September 22.</div>

4.3.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 1 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.2:

- The key footpaths along Hawthorn Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to the entire western footpath from 10am - 12pm at the equinox and the eastern footpath will be in sunlight from 12pm - 2pm. This will support the role of the footpaths as key public spaces at times where they are likely to be more active.
- The desired low to mid rise scale of the centre will be achieved with a maximum building height of 6 storeys. This height supports additional density in the centre, responds to the emerging character along Hawthorn Road while balancing character and amenity considerations. The proposed 6 storey building height is also consistent with some recent development approvals in the precinct. The larger lots within this precinct could support this height and transition appropriately to the adjoining residential areas.
- Moving south along Hawthorn Road, building heights step down from 6 to 5 storeys, reducing the scale of the built form to respond to the low rise residential areas on the opposite side of Hawthorn Road and Sycamore Street.
- A lower building height of five storeys is proposed for areas north of Glen Huntly Road. This responds to the existing lower scale context and the more constrained sites than south of Glen Huntly Road.
- Setbacks of 5 metres for development above 3 storeys will ensure the street wall is the dominant element in the streetscape when viewed from opposite footpaths, rather than the overall building height.
- The long cross section shown in Figure 25 and Figure 26 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised.

- Although this height is taller than existing buildings, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.
- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
 - The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

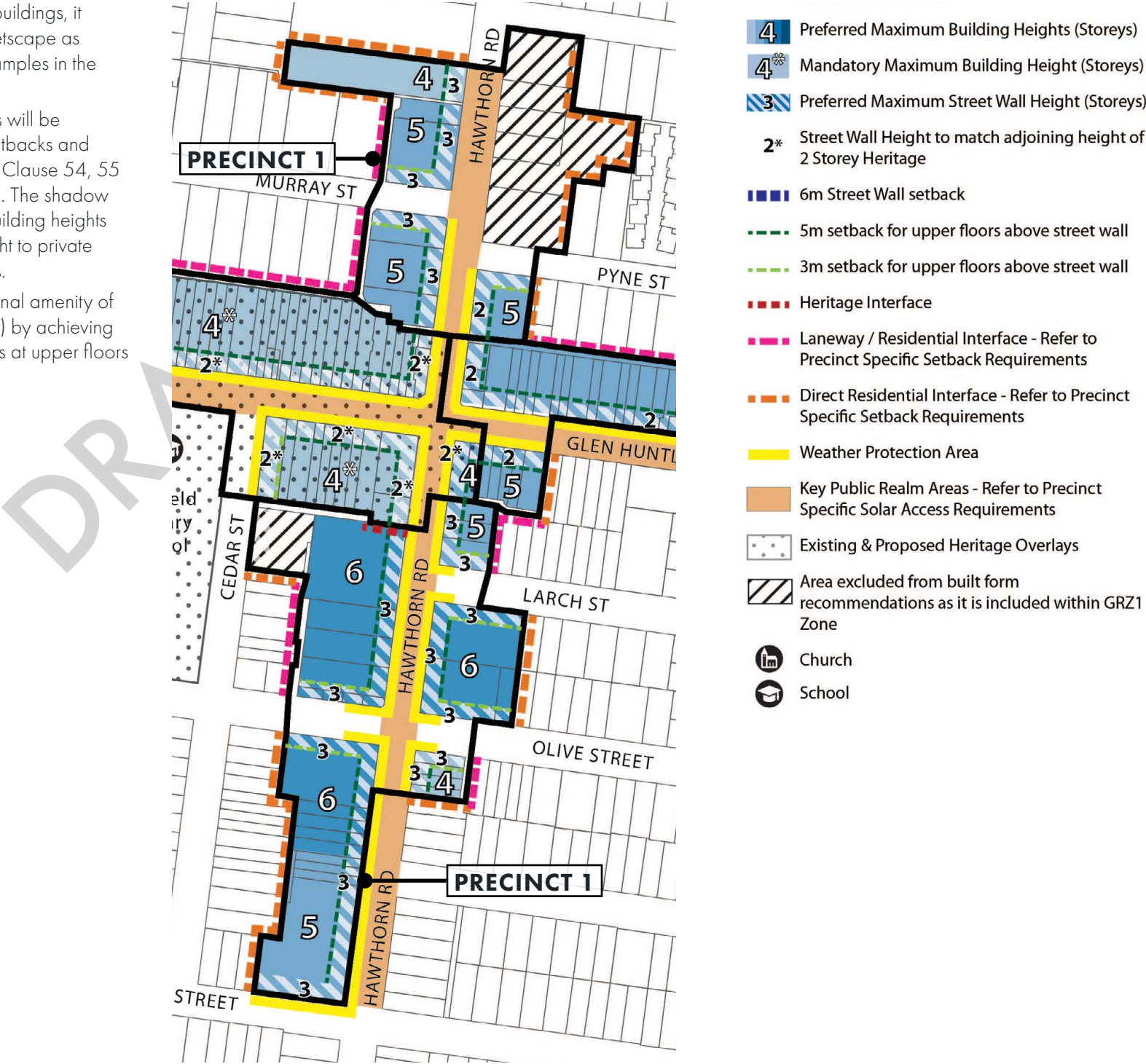


Figure 22. Built Form Framework - Precinct 1

4.3.4 CROSS SECTIONS

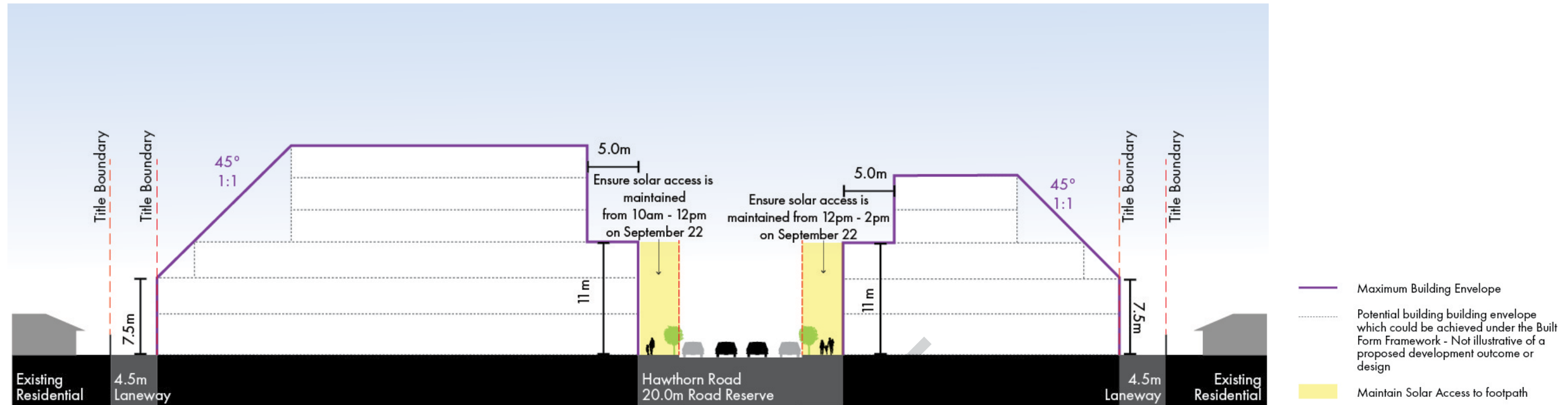


Figure 23. Precinct 1 - Cross Section A

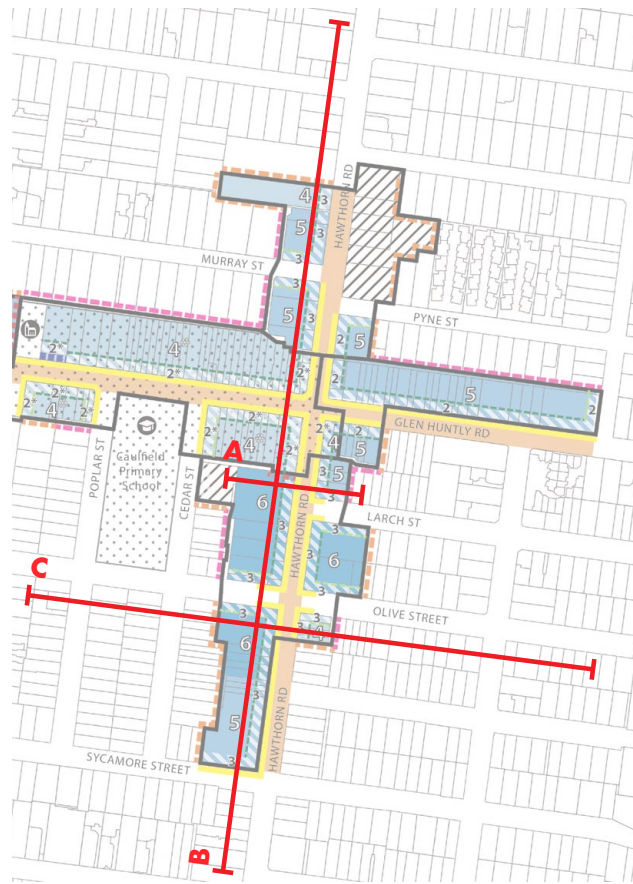


Figure 24. Precinct 1 - Cross Section Key Plan

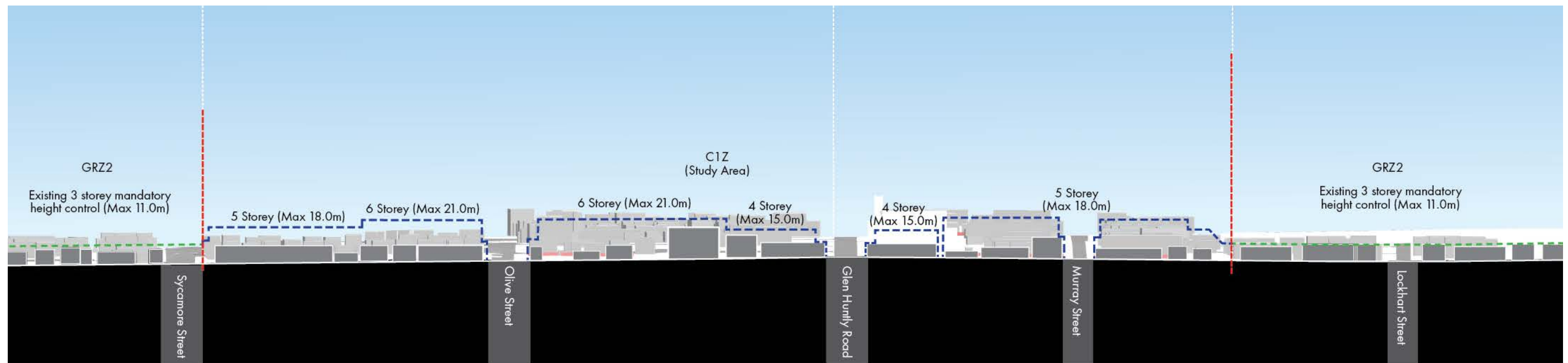


Figure 25. Precinct 1 - Cross Section B

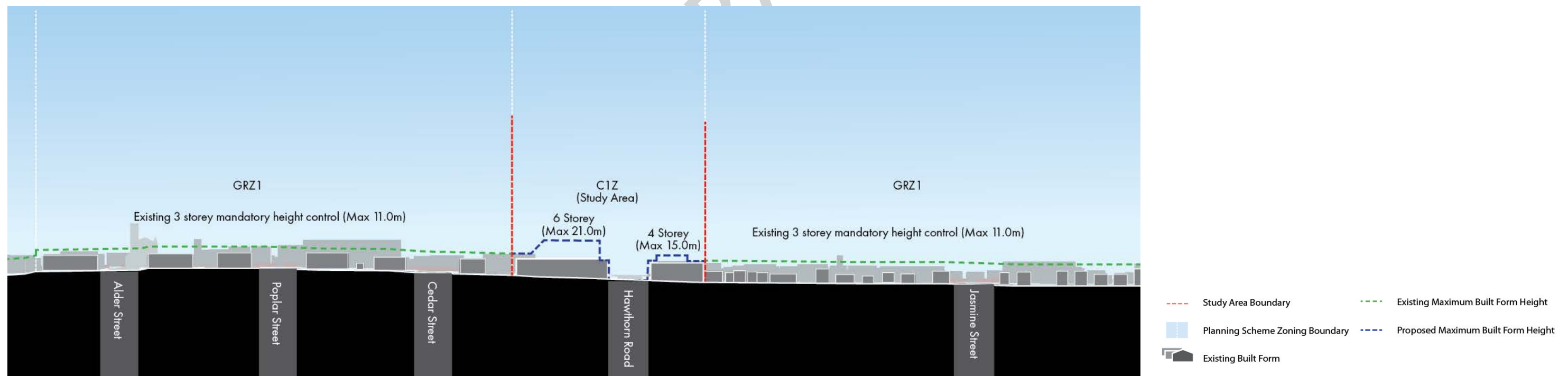


Figure 26. Precinct 1 - Cross Section C

4.3.5 SKETCH VISUALISATION



Figure 27. Viewpoint 1 - Existing Conditions



Figure 28. View 1 - With examples of approved and supported street wall and building height shown

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows a transition in scale from four storeys in the heritage precinct into 6 storeys in Precinct 1. The street wall and upper levels have a strong presence in the viewline.

- Street Wall Maximum Height
- Building Maximum Height
- Existing Heritage Overlay

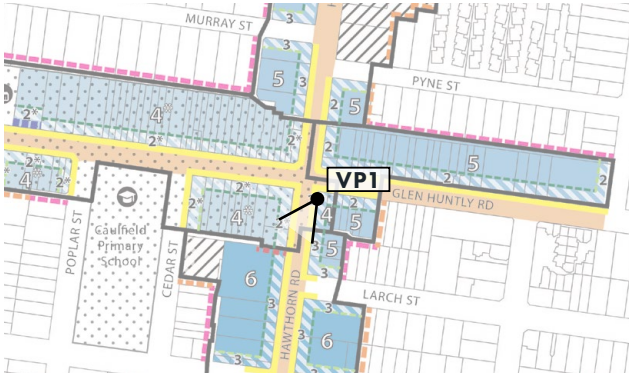


Figure 29. Viewpoint 1 Location Plan

4.3.6 PRECINCT 1 - SHADOW ANALYSIS

Figure 30 demonstrates the shadows cast by existing buildings within Precinct 1 of the Caulfield South NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

9am - Sept 22



10am - Sept 22



11am - Sept 22



Figure 30. Precinct 1 - Shadow Analysis

12pm - Sept 22



1pm - Sept 22



2pm - Sept 22



3pm - Sept 22

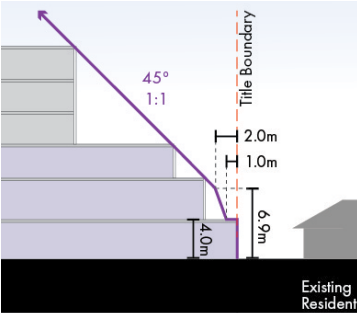
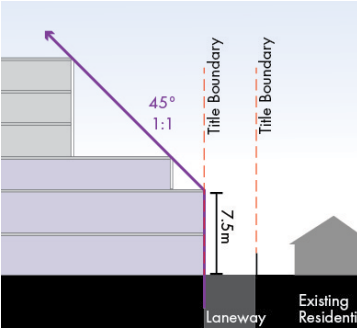


4.4 PRECINCT 2 - GLEN HUNTLY ROAD EAST

4.4.1 PRECINCT OVERVIEW

The eastern approach to the Caulfield South NAC provides a range of redevelopment opportunities that will support the activation of the eastern end of the NAC. The established two storey street wall will be strengthened whilst supporting development of up to five storeys, which is set back from the street.

4.4.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 5 storeys (18.0m).	Preferred Street wall Height <ul style="list-style-type: none">Up to 2 storeys (9.0m) or match the height of the adjoining double storey buildings.	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Glen Huntly Road from 10am on September 22.
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below:	
			

4.4.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 2 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.2:

- The southern footpath along Glen Huntly Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to the entire southern footpath from 10am at the equinox. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired low to mid rise scale of the centre will be achieved with a maximum building height of 5 storeys. This height supports additional density in the centre whilst integrating with the proposed 4 storey heights in the heritage precinct. The shallower sites in this precinct also present a constraint for taller development.
- Setbacks of 5 metres for development above 2 storeys will ensure the street wall is the dominant element in the streetscape when viewed from opposite footpaths, rather than the overall building height.
- The long cross section shown in Figure 34 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.

- The predominant double storey street wall was identified as an important character element in Part 1 of this report. This will be maintained through the proposed two storey street wall height. Additional requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised.
- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

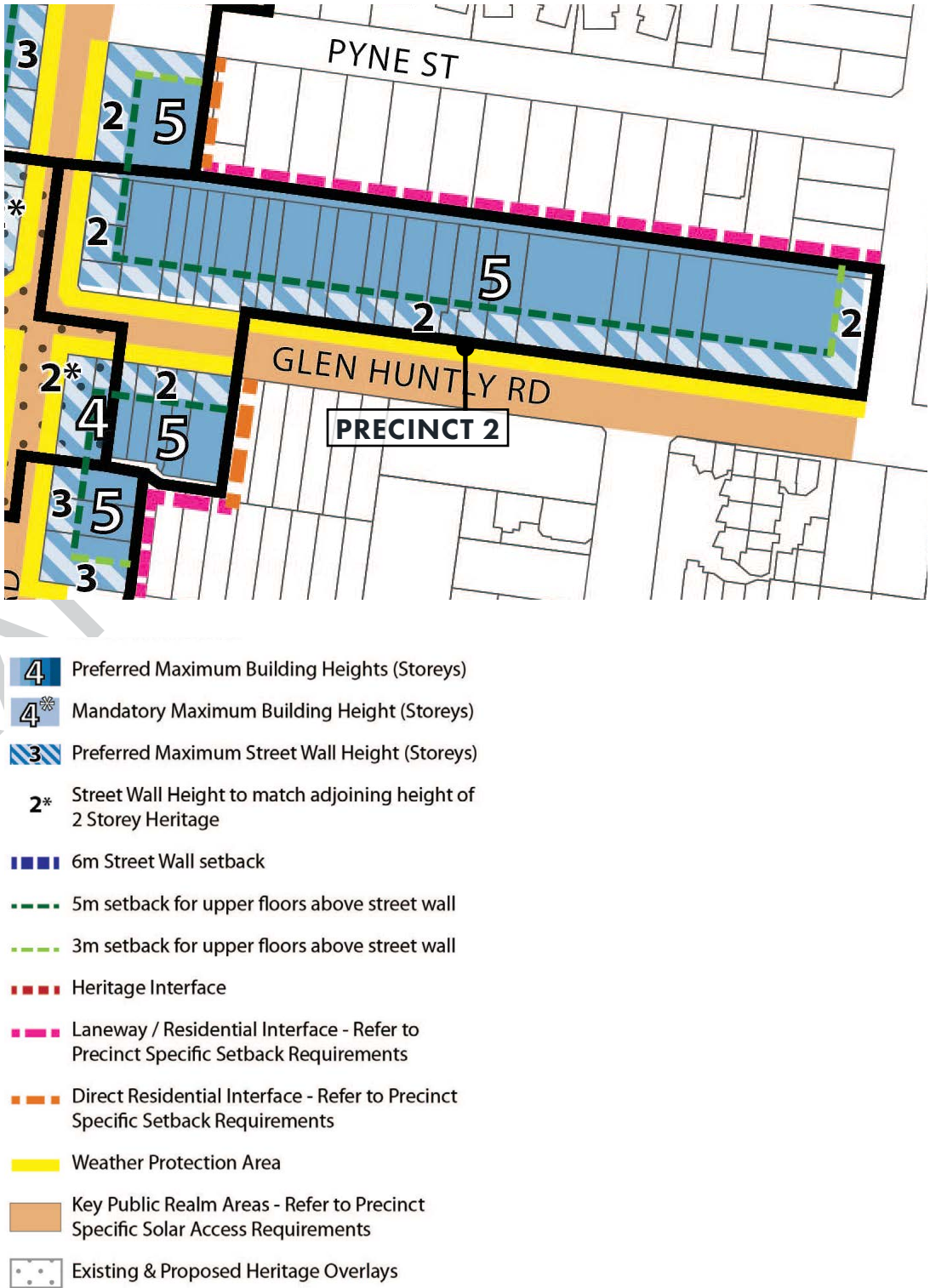


Figure 31. Built Form Framework - Precinct 2

4.4.4 CROSS SECTIONS

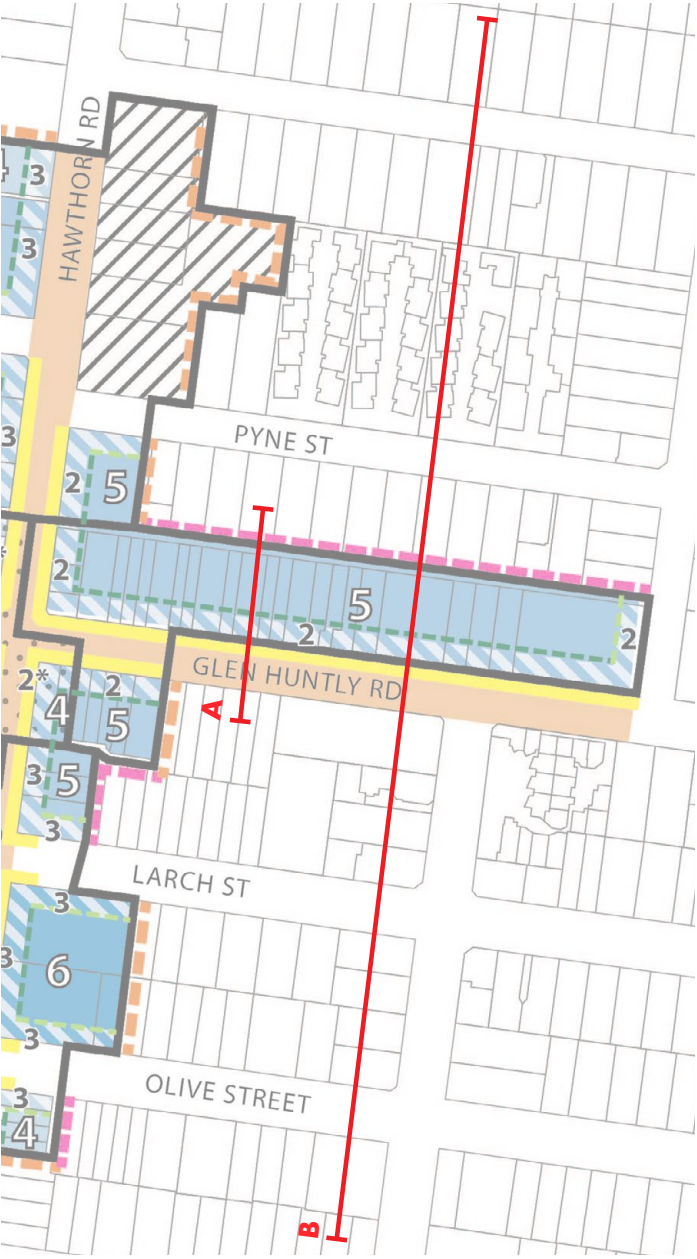


Figure 32. Precinct 2 - Cross Section Key Plan

- Study Area Boundary
- Existing Maximum Built Form Height
- Planning Scheme Zoning Boundary
- Proposed Maximum Built Form Height
- Existing Built Form

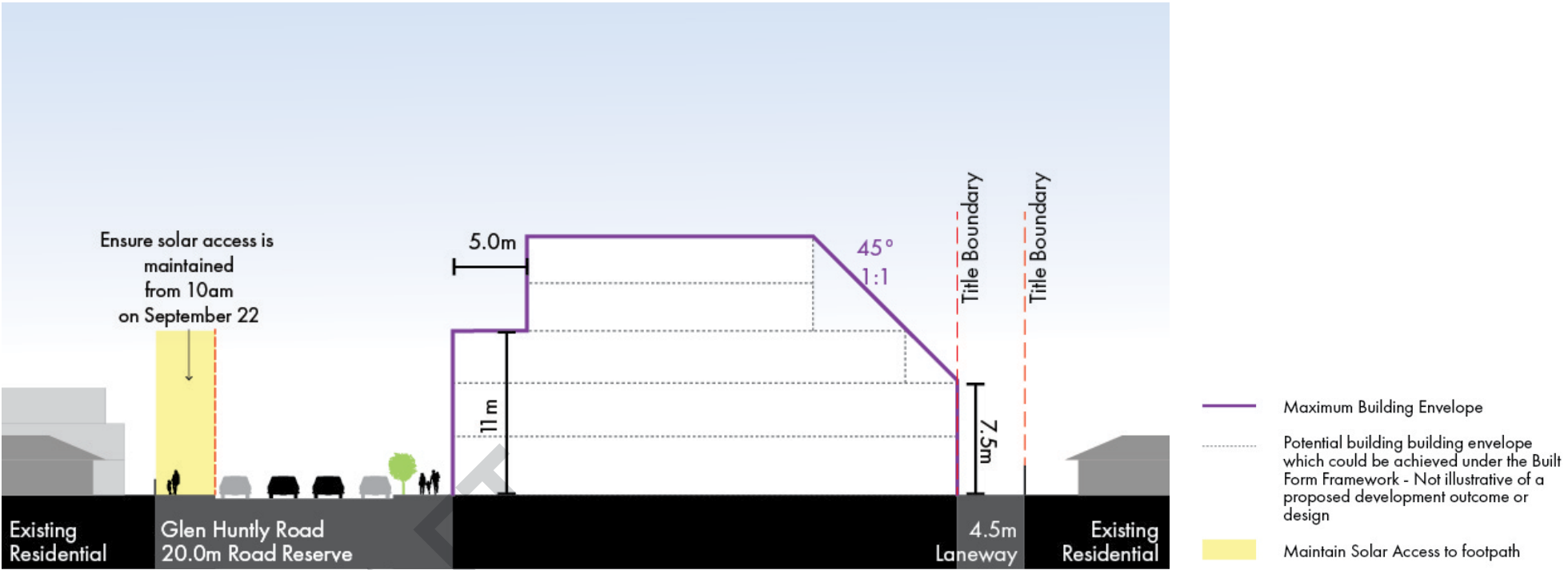


Figure 33. Precinct 2 - Cross Section A

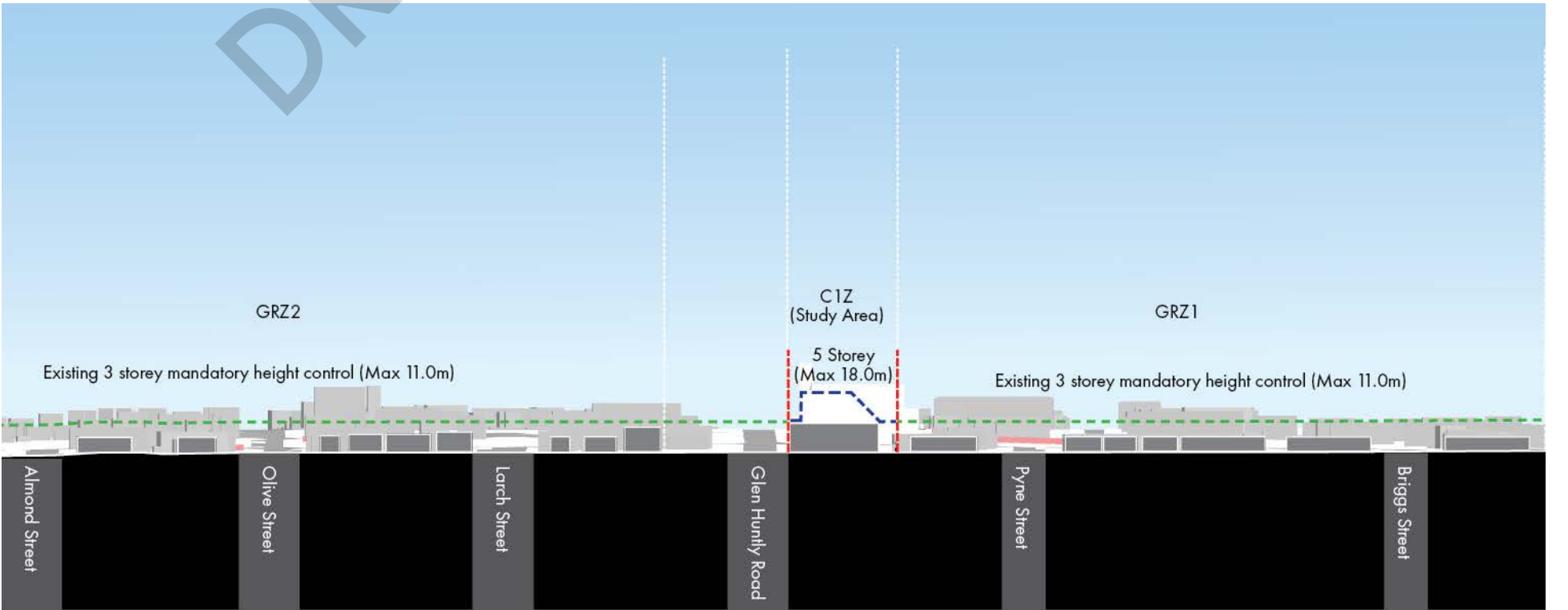


Figure 34. Precinct 2 - Cross Section B

4.4.5 SKETCH VISUALISATION



Figure 35. View 2 - Existing Conditions



Figure 36. View 2 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

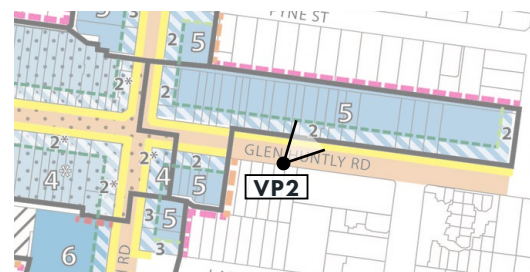


Figure 37. Viewpoint 2 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the established two storey street wall will be maintained with new developments. The upper levels are visible however do not overwhelm the streetscape.

4.4.6 PRECINCT 2 - SHADOW ANALYSIS

Figure 38 demonstrates the shadows cast by existing buildings within the Precinct 2 of the Caulfield South NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



11am - Sept 22



10am - Sept 22



12pm - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 38. Precinct 2 - Shadow Analysis

1pm - Sept 22



3pm - Sept 22



2pm - Sept 22



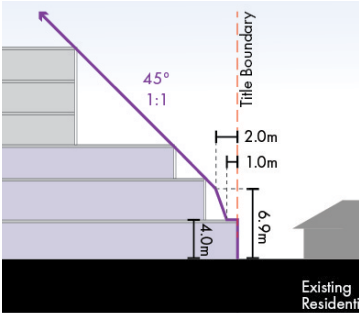
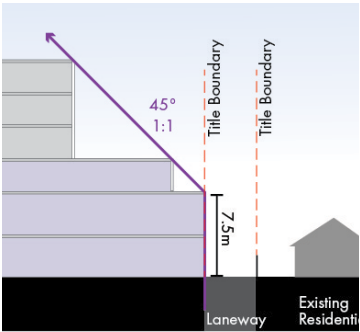
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4.5 PRECINCT 3 - GLEN HUNTLY ROAD HERITAGE

4.5.1 PRECINCT OVERVIEW

The Glen Huntly Road heritage precinct will continue to be known for its impressive inter-war architecture. New development of up to five storeys will sensitively integrate with the heritage shopfronts and ensure they remain the prominent in the streetscape.

4.5.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
<div>Mandatory maximum Heights<ul style="list-style-type: none">Up to 4 storeys (15.0m)</div>	<div>Preferred Street wall Height<ul style="list-style-type: none">Up to 2 storeys (9.0m) or match the height of the adjoining heritage building</div>	<div>Preferred Setbacks<ul style="list-style-type: none">6.0m front setbacks for land at 707 Glen Huntly Road for development up to the height of street wall. Side setbacks for development up to the height of the street wall to respond to the Holy Cross Church setting and built form.Zero front and side setbacks for development up to height of the street wall elsewhere5.0m front setback for development above the street wall3.0m setback to the secondary street frontage for development above the street wall on corner sitesDevelopment directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:</div> <div></div> <div><ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below:</div> <div></div>	<div>Heritage<ul style="list-style-type: none">Development should significant and contributory buildings within the heritage precinct.Development should respect views to the Holy Cross Church.Retain individually significant and contributory heritage buildings in accordance with HO66.</div> <div>Solar Access<ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Glen Huntly Road from 10am on September 22.</div>

4.5.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 3 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.2:

- The southern footpath along Glen Huntly Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to the entire southern footpath from 10am at the equinox. This will support the role of the footpath as a key public spaces at times where it is likely to be more active.
- The valued heritage buildings will retain their prominence in the streetscape. It is recommended that contributory and significant heritage facades are retained where possible and that the street wall for new, infill buildings will match the height of existing heritage buildings with a height of two storeys. The overall mandatory building height of four storeys and upper level setbacks of 5 metres will

maintain a dominant two storey heritage street wall. This is demonstrated in Figure 40 which displays a dominant heritage streetscape when viewed from the opposite footpath with upper levels partially concealed. The section shows that 71% of the view will be to the heritage wall and 29% to upper levels. Building heights of greater than four storeys would start to diminish the dominance of the heritage street wall. Because of the heritage significance of the buildings, and the potential for taller development to reduce the prominence of the heritage street wall, a mandatory height control was considered necessary. The Centre Wide Recommendations will provide addition design guidance to ensure the heritage streetscape is retained.

- The long cross section shown in Figure 42 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The Holy Cross Church is identified as an important landmark in the NAC and is currently prominent in the streetscape when viewed from the east and west. The ground level setbacks on the neighbouring properties at 701-703 and 707 Glen Huntly Road allow for clear views to the church and its spires. The built form framework recommends that a landscaped ground level setback is provided on these properties to maintain views to the church. The recommended setback distance of 6.0m matches the rear walls of the spires which will maintain clear views to them.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements of Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

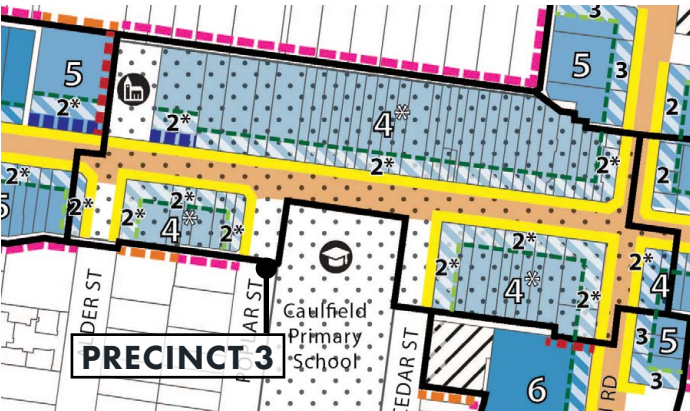


Figure 39. Built Form Framework - Precinct 3

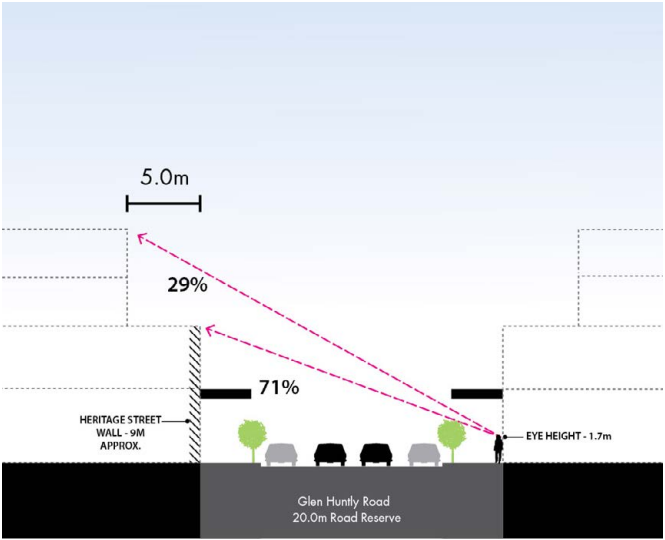
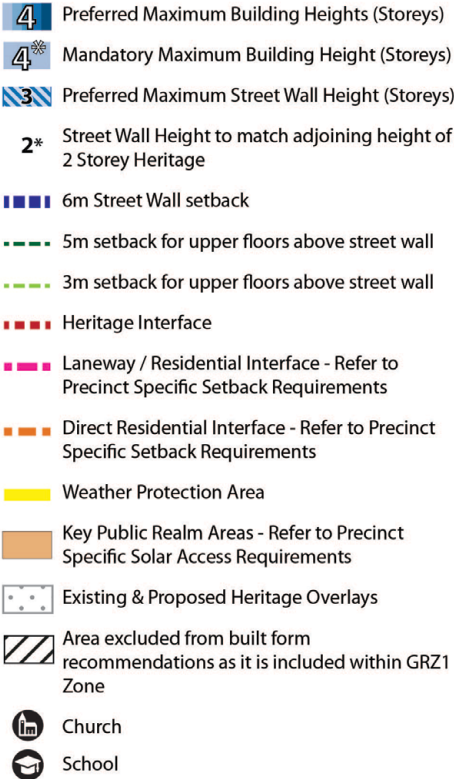


Figure 40. Proportion of view occupied by the heritage street wall

4.5.4 CROSS SECTIONS

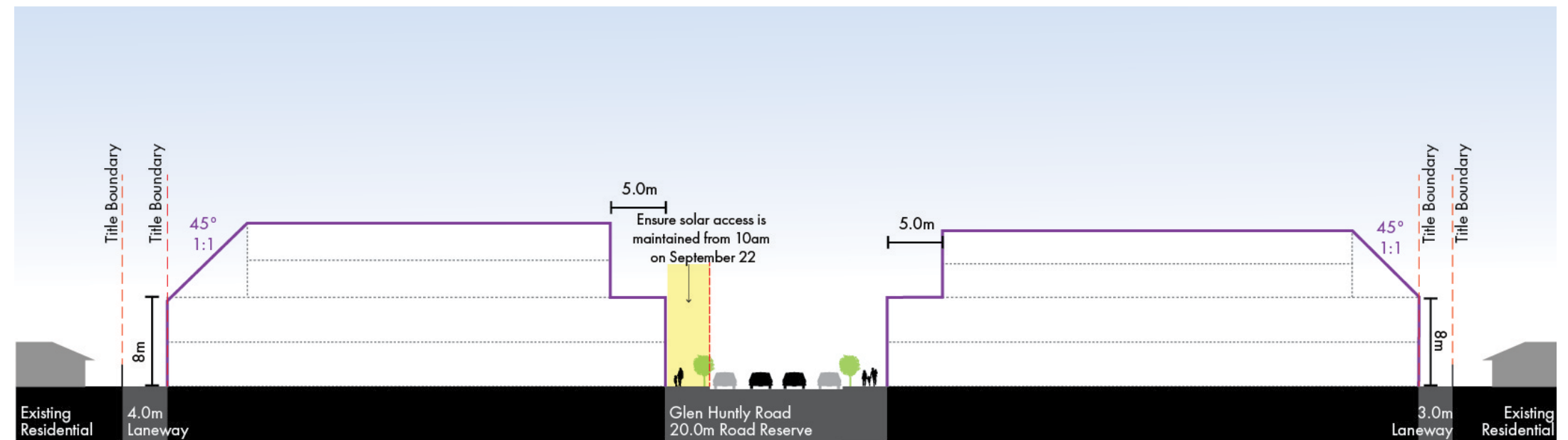
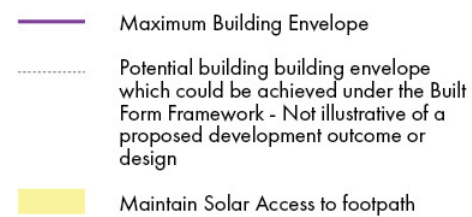


Figure 41. Precinct 3 - Cross Section A

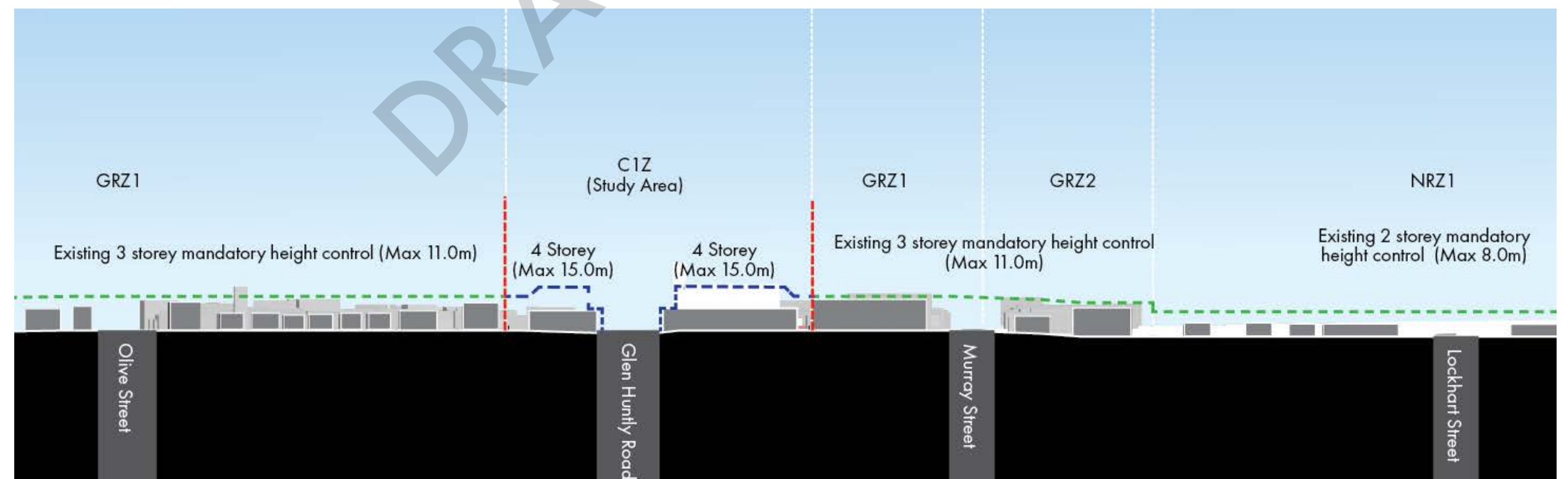


Figure 42. Precinct 3 - Cross Section B



Figure 43. Precinct 3 - Cross Section Key Plan

4.5.5 SKETCH VISUALISATION



Figure 44. Viewpoint 3 - Existing Conditions



Figure 46. View 3 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height
- Existing Heritage Overlay

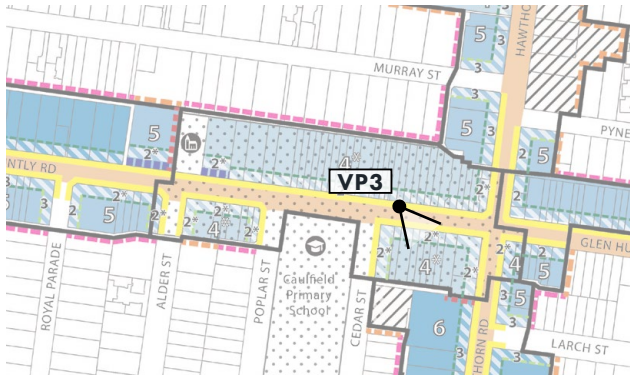


Figure 45. Viewpoint 3 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the existing heritage street wall will be retained to maintain the heritage significance of the precinct. Levels 3 and 4 are set back and have minimal visual impact. The heritage buildings remain the dominant element in the streetscape.

4.5.6 PRECINCT 3 - SHADOW ANALYSIS

Figure 47 demonstrates the shadows cast by existing buildings within the Precinct 3 of the Caulfield South NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



11am - Sept 22



10am - Sept 22



12pm - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 47. Precinct 3 - Shadow Analysis

1pm - Sept 22



2pm - Sept 22



3pm - Sept 22



4.6 PRECINCT 4 - GLEN HUNTLY ROAD WEST

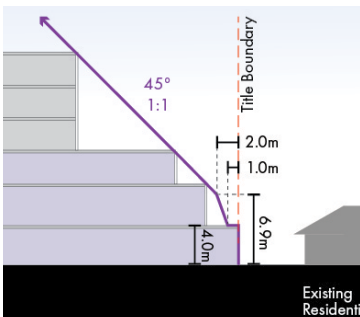
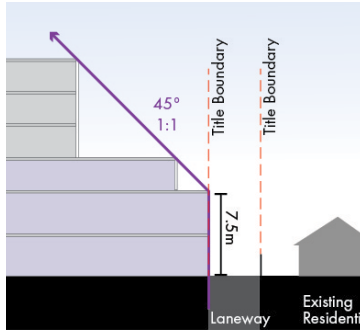
4.6.1 PRECINCT OVERVIEW

The western end of Caulfield South NAC will provide for development of up to six storeys on the northern side of Glen Huntly Road where the large lots can support higher scale development and transition to adjoining residential areas.

The southern side of Glen Huntly Road will support a maximum of five storeys and transition to residential areas to the south.

The established two storey street wall to the western end of Glen Huntly Road will be maintained with new development.

4.6.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 6 storeys (21.0m) for land at 649-697 Glen Huntly Road.Up to 5 storeys (18.0m) elsewhere.Up to 4 storeys (15.0m) on the southern side of Glen Huntly Road.	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m) for land at 627-697 Glen Huntly Road and 612-672 Glen Huntly Road.Up to 2 storeys (9.0m) or match the height of the adjoining double storey buildings elsewhere.	Preferred Setbacks <ul style="list-style-type: none">6.0m front setbacks for land at 701-703 Glen Huntly Road for development up to the height of street wall. Side setbacks for development up to the height of the street wall to respond to the Holy Cross Church setting and built form.Zero front and side setbacks for development up to height of the street wall elsewhere5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Heritage <ul style="list-style-type: none">Development should respect views to the Holy Cross Church. Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Glen Huntly Road from 10am on September 22.Ensure solar access is maintained to the entire eastern footpath of Kooyong Road from 12pm - 2pm on September 22.Ensure solar access is maintained to the entire western footpath of Kooyong Road from 10am - 12pm on September 22.
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below:	
			

4.6.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 4 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.2:

- The southern footpath along Glen Huntly Road and the eastern and western footpaths of Kooyong Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to the entire southern footpath from 10am at the equinox and the eastern and western footpaths of Kooyong Road between 10am and 2pm. This will support the role of the footpaths as key public spaces at times where they are likely to be more active.
- The desired low to mid rise scale of the centre will be achieved with a maximum building height of 6 storeys. This height is located on the north side of Glen Huntly Road where the lot sizes are large and there will be no shadow impact on residential properties as they are located to the north.
- Building heights to the south of Glen Huntly Road, that are within the local centre boundary, as identified in the Glen Eira City Plan, are limited to 4 storeys, which responds to the directions of the City Plan, the smaller lot sizes and residential areas located to the south of commercial properties.
- The long cross section shown in Figure 50 demonstrates that the proposed building scale transitions appropriately to the surrounding Neighbourhood Residential Zoned areas, which has a maximum allowable building height of 2 storeys.
- The predominant double storey street wall at the western end of the Glen Huntly Road was identified as an important character element in Part 1 of this report. This will be maintained through the proposed two storey street wall height. Additional requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Elsewhere in the precinct a 3 storey street wall is proposed which provides a scale which will not overwhelm the streetscape and is consistent with more recent developments.
- Setbacks of 5 metres for development above the street wall will ensure the street wall is the dominant element in the streetscape when viewed from opposite footpaths, rather than the overall building height.
- The Holy Cross Church is identified as an important

landmark in the NAC and is currently prominent in the streetscape when viewed from the east and west. The ground level setbacks on the neighbouring properties at 701-703 and 707 Glen Huntly Road allow for clear views to the church and its spires. The built form framework recommends that a landscaped ground level setback is provided on these properties to maintain views to the church. The recommended setback distance of 6.0m matches the rear walls of the spires which will maintain clear views to them.

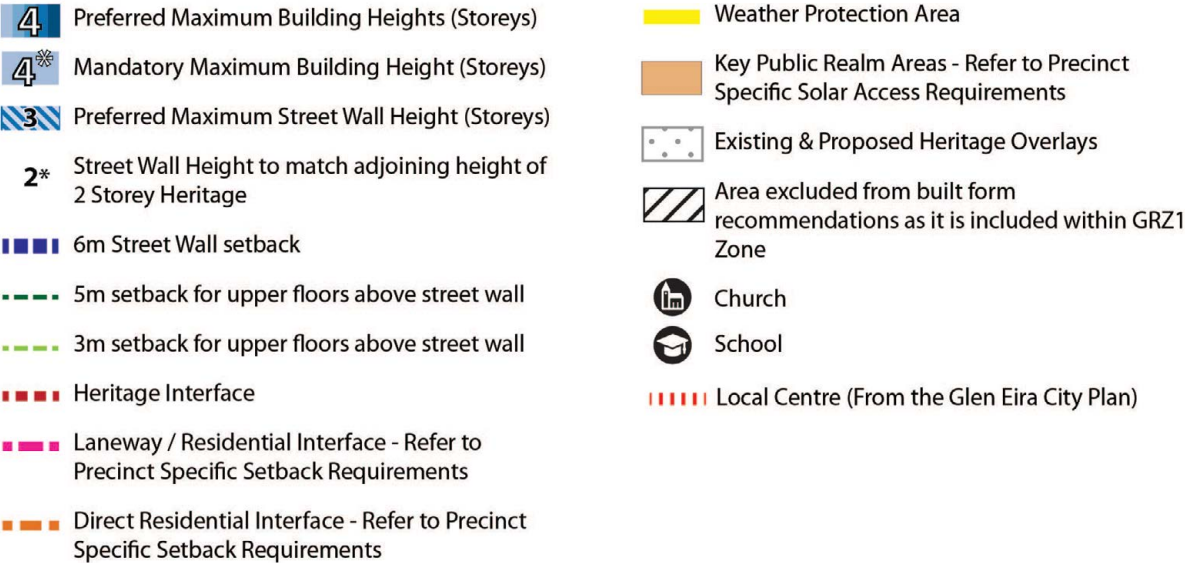
- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of

dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback). This will be particularly important for this precinct as there are no intersecting side streets on the northern side of Glen Huntly Road to provide breaks in built form. Development would benefit from upper level side setbacks on larger sites to provide gaps and views through to the sky.

- The pedestrian link located east of 697 Glen Huntly Road is important as it is the only mid-block link between Kooyong Road and Hawthorn Road. New development will activate this link to make it safer and more enjoyable to use.



Figure 48. Built Form Framework - Precinct 4



4.6.4 CROSS SECTIONS

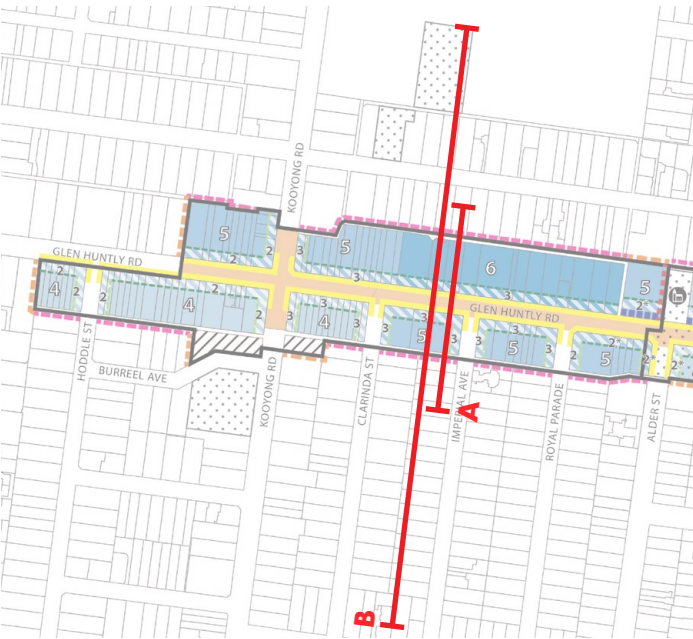


Figure 51. Precinct 3 - Cross Section Key Plan

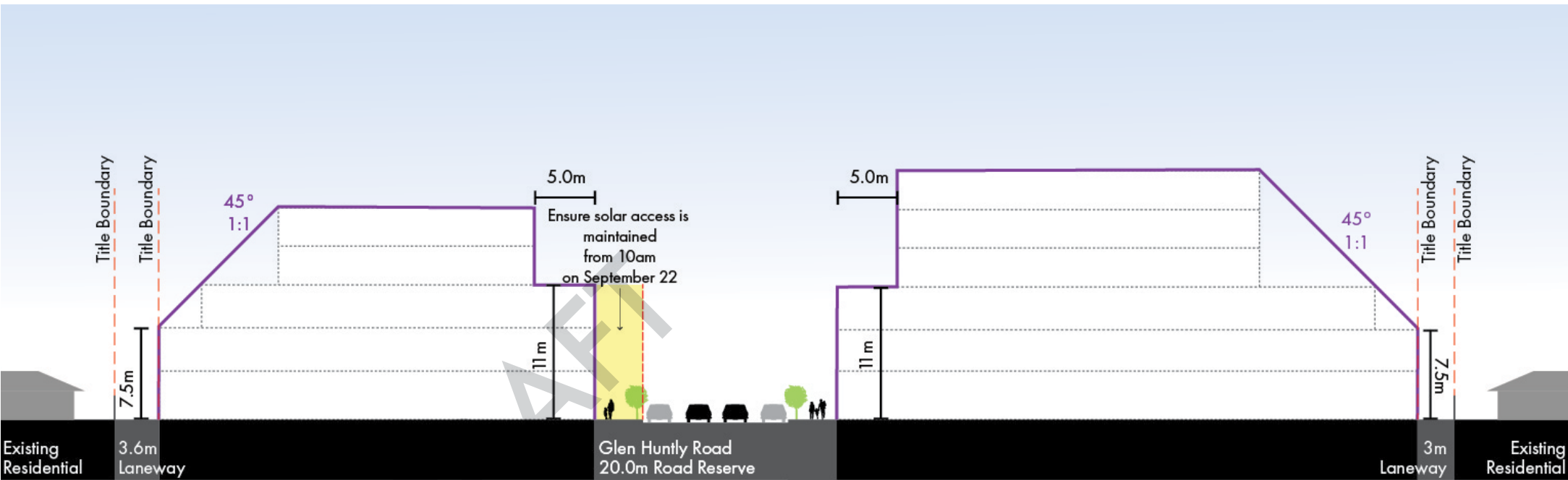


Figure 49. Precinct 4 - Cross Section A

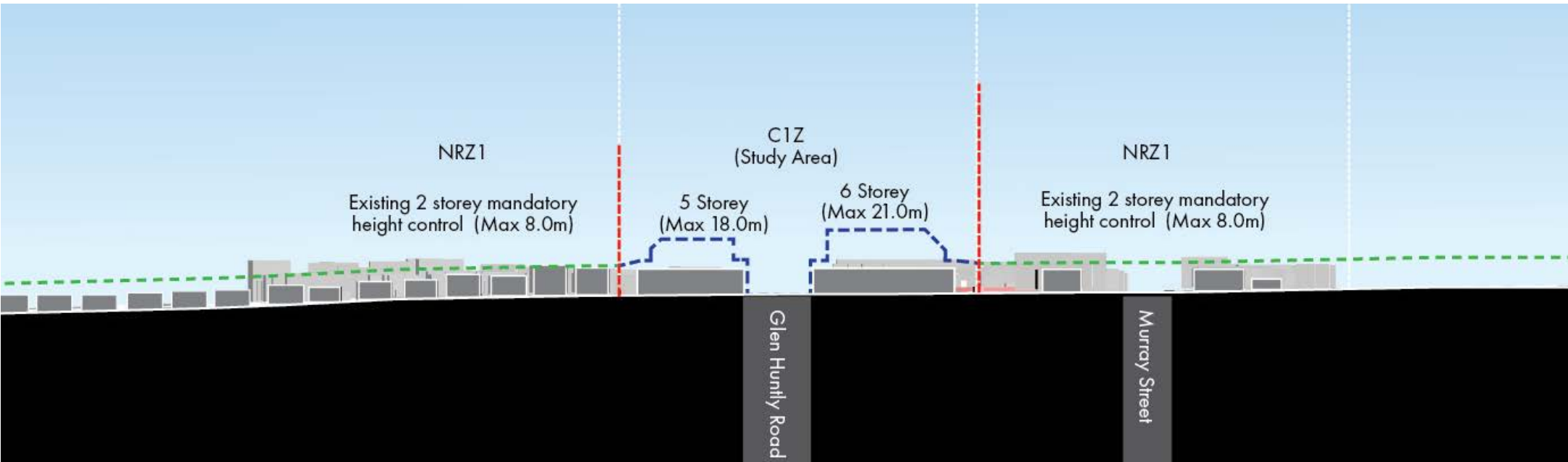


Figure 50. Precinct 4 - Cross Section B

4.6.5 SKETCH VISUALISATION



Figure 53. Viewpoint 4 - Existing Conditions



Figure 54. View 4 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

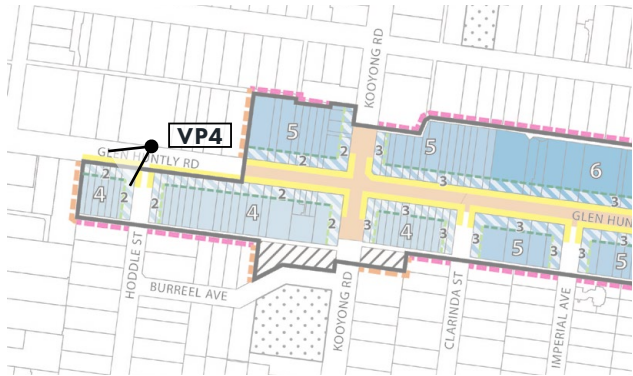


Figure 52. Viewpoint 4 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the established two storey street wall will be maintained with new developments. The upper levels for four storey development will be visible however they will not overwhelm the streetscape.

4.6.6 PRECINCT 4 - SHADOW ANALYSIS

Figure 55 demonstrates the shadows cast by existing buildings within the Precinct 4 of the Caulfield South NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



10am - Sept 22



11am - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 55. Precinct 4 - Shadow Analysis

12pm - Sept 22



3pm - Sept 22



1pm - Sept 22



2pm - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

DRAFT

BENTLEIGH EAST NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK



Prepared for Glen Eira City Council

DRAFT BUILT FORM FRAMEWORK

APRIL 2021

QUALITY ASSURANCE

Glen Eira NAC`s Built Form Frameworks
Bentleigh East NAC Draft Built Form Framework

Prepared for
Glen Eira City Council

Project Number
320.0714.U.03

Revisions

Issue	Date	Description	Prepared By	Reviewed By	Project Principal
00	04 February 2021	Response to Council Officer Comments	KXS	TS	MN
01	24 February 2021	Response to Council Officer Comments	KXS	TS	MN
02	14 April 2021	Response to Council Officer Comments	KXS	TS	MN
03	21 April 2021	Revisions from Council discussions	KXS	TS	MN

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INTRODUCTION

STUDY OVERVIEW

STUDY AREA

DRAFT





PROJECT OVERVIEW

This built form framework for the Bentleigh East Neighbourhood Activity Centre (NAC) is one of three built form frameworks being prepared towards addressing an existing policy void. The other centres forming part of the project are the Caulfield Park Neighbourhood Activity Centre and the Caulfield South Neighbourhood Activity Centre.

The Built Form Frameworks set out preferred development outcomes in each centre. This will ensure new development provides for the continual revitalisation of the shopping strips with high quality design whilst maintaining the valued character of each centre and protecting the amenity of adjoining residential areas.

The Frameworks provide guidance for building heights, setbacks and other design considerations. These frameworks are supported by rigorous analysis and guided by sound urban design principles appropriate to the context of each centre.

This Draft Bentleigh East NAC Built Form Framework consists of the following parts:

Part 1 - Context and Analysis - Provides analysis and background research to understand the NAC, its context and drivers for change.

Part 2 - The Built Form Framework - Provides the guiding principles and built form frameworks including building heights and setbacks.

Proposed planning controls have been developed to reflect the requirements of the Bentleigh East NAC Built Form Framework. The controls are provided in the form of a Design and Development Overlay.

STUDY AREA

Bentleigh East is located approximately 800m east of the Bentleigh Major Activity Centre. It extends for a length of 750m along Centre Road and includes a range of specialty retail and services, and limited hospitality uses. The study area for the Bentleigh East NAC Built Form Framework generally aligns with the existing Commercial 1 Zoning. Refer to Figure 1 - Study Area.

An IGA supermarket anchors the eastern end of the centre and supports surrounding specialty retail. There is less activity at the western end of the centre where retail uses are located on the northern side of Centre Road only. A Smart Bus route is located along Centre Road providing high frequency bus services.

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Figure 1. Study Area Plan

PART 1 - CONTEXT & ANALYSIS

1. CONTEXT

- 1.1 STRATEGIC CONTEXT
- 1.2 STRATEGIC DOCUMENTS
- 1.3 PLANNING POLICY FRAMEWORK
- 1.4 LOCAL PLANNING POLICY FRAMEWORK
- 1.5 PLANNING ZONES
- 1.6 OVERLAYS
- 1.7 RECENT DEVELOPMENT CONTEXT

2. ANALYSIS

- 2.1 TOPOGRAPHY
- 2.2 LAND USE & INTERFACES
- 2.3 BUILT FORM & CHARACTER
- 2.4 DEVELOPMENT ACTIVITY & CAPACITY



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1 CONTEXT

1.1 STRATEGIC CONTEXT

Consistent with sustainable land use and development principles, Neighbourhood Activity Centres are identified in state and local planning policy and strategy as suitable locations for accommodating increased residential density. The policy framework including Plan Melbourne, City Plan and the relevant clauses of the Glen Eira Planning Scheme (State and local) establishes this clear direction.

1.0.1 PLANNING POLICY CONTEXT

Planning policy relating to Bentleigh East Neighbourhood Activity Centre (NAC) identifies the centre as one of 11 NACs in Glen Eira. The higher order centres in Glen Eira are Major Activity Centres, of which there are six. Consistent with sustainable land use principles, NACs in the metropolitan areas are high amenity areas that should accommodate growth to meet the needs of existing and future communities. They differ from the Major Activity Centres identified in Figure 3, which offer a greater amount of transport options, retail and services. As such the level of development and change in the NACs is expected to be less than that experienced in the Major Activity Centres.

The following sections provide a brief outline of this policy context. As noted previously, the centre is largely in the Commercial 1 Zone.

In and around activity centres, a degree of change will occur, consistent with Plan Melbourne, City Plan and the Glen Eira Planning Scheme and commensurate with the level of amenity and accessibility provided by each centre and also taking into account locally-specific amenity and character considerations. Planning controls informed by the built form framework will provide Council with the necessary guidance to assess planning applications and to provide proponents and the community with clear parameters for design in centres.

Council aims to sustainably manage anticipated growth and amenity and to identify and preserve the valued features of the built environment as its activity centres evolve while also protecting the valued amenity of the areas surrounding the centres. For Council to implement built form guidance that is robust and defensible, the guidance must balance the strong

policy objectives of directing more intense development to well-located areas such as neighbourhood activity centres with the need to protect the key elements that make these centres desirable places in their own right. In the Victorian planning system, planning controls that do not achieve a reasonable balance cannot be introduced to the planning scheme.

1.0.2 BUILT FORM FRAMEWORKS IN CONTEXT

Built form frameworks, like structure plans, consider locally-specific elements of a centre such as land use patterns, built form and character, function and amenity and form a vision and built form controls for addition to the planning scheme. Figure 2 provides an overview of where built form frameworks sit in the planning framework relative to built form decisions and residential growth.

1.0.3 PURPOSE OF THE BENTLEIGH EAST NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK

Recent development activity and planning applications in Glen Eira's NACs indicate the need for more detailed and specific built form policy and controls to guide development. While there are existing policies and controls applying to the Bentleigh East NAC they generally consist of:

- High-level strategy and planning policy that supports increased densities in NACs.
- Zoning that predominantly guides land use rather than built form outcomes. The Commercial 1 Zone allows for more intense mixed use development and nearby residential zoning provides for a transition up from the residential hinterland towards activity centres.

This built form framework will provide the locally-relevant analysis of physical characteristics and a vision to inform the drafting of built form controls for the planning scheme and provide that more specific and detailed guidance.

The following issues are addressed by the built form framework:

- Preferred building heights based on built form and shadow analyses.
- Street wall and interfaces
- Building setbacks and modulation – for amenity and visual impact
- Transitions to less intense development



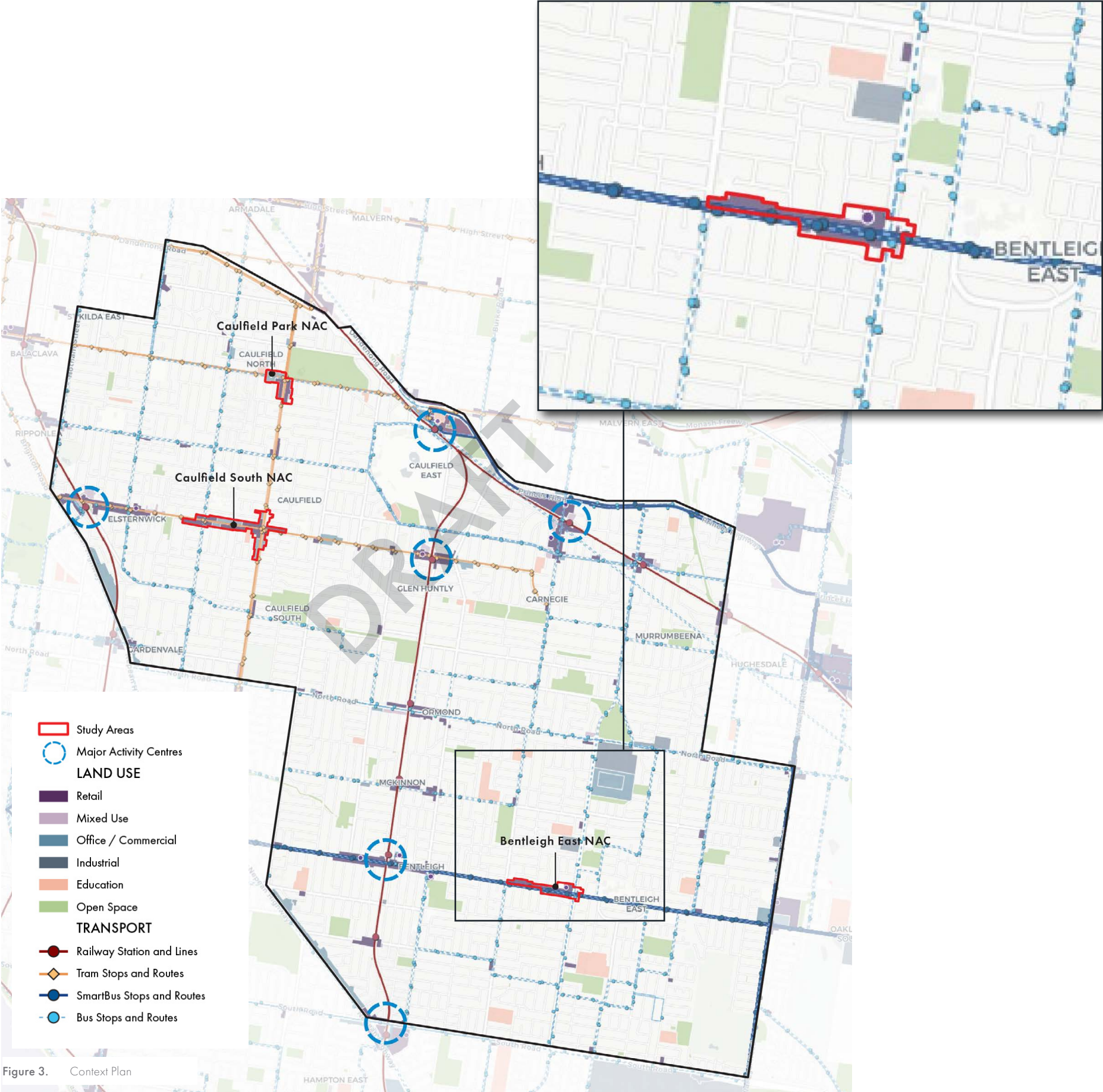
Figure 2. Built Form Framework Planning Context

1.1.1 BENTLEIGH EAST NEIGHBOURHOOD
ACTIVITY CENTRE LOCATION AND PHYSICAL
CONTEXT

The Bentleigh East Neighbourhood Activity Centre is located on Centre Road and East Boundary Road in Bentleigh East. The centre is located in the south-eastern part of the municipality in a predominantly residential context and enjoys access to convenience retail (including an IGA supermarket) and the Smart Bus route along Centre Road. The centre is located approximately 14 km south-east of the Melbourne CBD.

See the Built Form Analysis section of this report for an overview of the existing built form context. The areas immediately surrounding the centre are low-rise neighbourhoods in the General Residential Zone. The General Residential Zone allows for development of up to three storeys.

As mentioned previously, Council’s planning policies identify the centre as one of 11 NACs in Glen Eira, being second in the activity centre hierarchy in terms of suitability for increased density and height due to the level of amenity offered.



1.2 STRATEGIC DOCUMENTS

1.2.1 PLAN MELBOURNE 2017-2050

The updated Plan Melbourne document provides strategic direction and guidance for some of Melbourne’ state significant employment and activity centre clusters, along with housing and transport directions for greater Melbourne. While the document does not provide specific guidance to the Bentleigh East Neighbourhood Activity Centre some of the more relevant directions are:

- Direction 2.1: Manage the supply of new housing in the right locations to meet population growth and create a sustainable city.
- Direction 5.1: Create a city of 20-minute neighbourhoods.

These directions aim to cluster new housing in activity centres and other places that offer good access to jobs, services and public transport and includes a policy for local governments to prepare structure plans for activity centres to accommodate growth.

There are significant opportunities for medium and higher density housing options in locations close to jobs and services including located around Major Activity Centres

The strategic direction closely aligns to the future outcomes envisaged for the NAC. Particularly providing housing opportunities close to transport enabling people to work and live in close proximity.

1.2.2 GLEN EIRA CITY PLAN

The Glen Eira City Plan provides a broad framework to manage growth and change in both housing and employment. Consistent with Plan Melbourne and several existing local planning policies, City Plan aims to direct the majority of new growth into activity centres towards sustainable land use and development outcomes.

To implement the strategic framework (introducing planning controls to the planning scheme), structure planning or built form frameworks need to be prepared for key areas suitable for growth such as major and neighbourhood activity centres and urban renewal areas. These activities produce the built

form, amenity and character material to assist to refine height controls and identify specific ways of managing amenity and character through the planning scheme.

Glen Eira City Plan provides an activity centre hierarchy which identifies Major Activity Centres at the top, followed by Neighbourhood Activity Centres and local centres. The highest levels of growth are to be directed to Major Activity Centres and urban renewal areas. Neighbourhood Activity Centres are identified as suitable for ‘moderate focus for housing growth’.

City Plan provides vision statements for growth locations. The vision for the Bentleigh East NAC is:

- **Bentleigh East** will be a welcoming and connected centre that caters for all. The centre will be enhanced by a range of retail and dining options to meet the everyday needs of its local community.

The Plan also seeks to nominate appropriate heights for the identified housing change areas with the neighbourhood activity centres identified as being suitable for up to between four storeys (for commercial areas with heritage overlays) and five storeys (for commercial areas without heritage overlays).

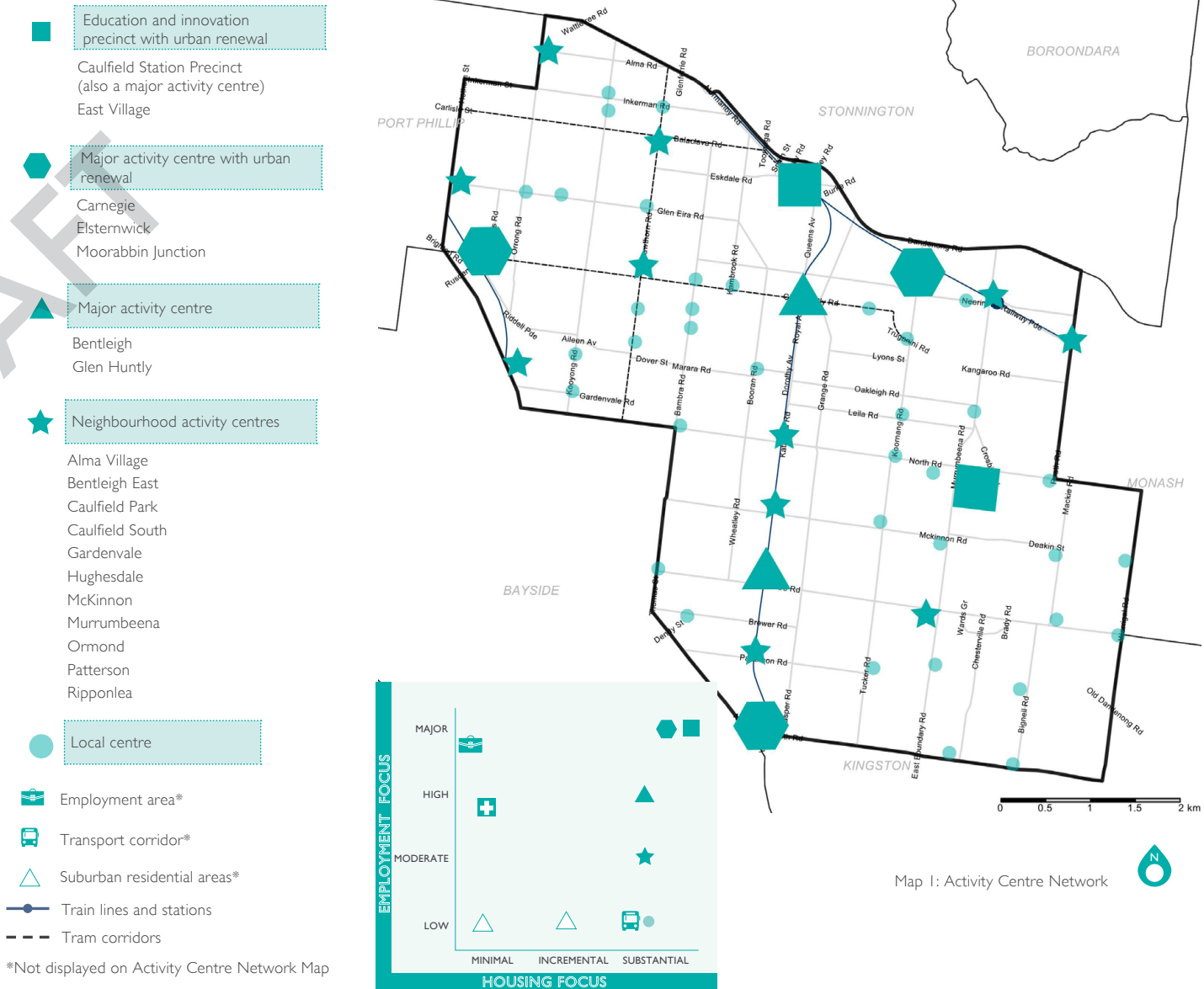


Figure 4: Activity centres — housing and employment focus

Figure 4. Glen Eira Activity Centre Network Map

1.3 PLANNING POLICY FRAMEWORK

1.3.1 PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following state policies of the Glen Eira Planning Scheme, summarised as follows:

1.3.2 CLAUSE 11 – SETTLEMENT

Seeks to anticipate and respond to the needs of existing and future communities through the provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure.

Clause 11.03-1S (Activity Centres)

Seeks to encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

Clause 11.03-1R (Activity Centres – Metropolitan Melbourne)

Aims to support the development and growth of Metropolitan Activity Centres by encouraging the location of new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.

1.3.3 CLAUSE 15 – BUILT ENVIRONMENT

Seeks to ensure that all new land uses and development appropriately responds to its landscape, valued built form and cultural context, and protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value.

Clause 15.01-1S (Urban design)

Aims to create urban environment that are safe, functional and provide good quality environments with a sense of place and cultural identity.

Clause 15.01-1R (Urban design – Metropolitan Melbourne)

Aims to create a distinctive and liveable city with quality design and amenity by supporting the creation of well-designed places that are memorable, distinctive and liveable.

Clause 15.02-1S (Energy and resource efficiency)

Aims to encourage land use and development that is energy and resource efficient, supports a cooler environment and minimises greenhouse gas emissions through strategies such as greening urban areas, buildings, transport corridors and open spaces with vegetation to reduce urban heat island effects.

Clause 15.03-1S (Heritage conservation)

Seeks to ensure the conservation of places of heritage significance by retaining elements that contribute to the importance of the heritage place.

1.3.4 CLAUSE 16 – HOUSING

Seeks to provide for housing diversity and ensure the efficient provision of supporting infrastructure. However, the quantum of development intensification remains relatively open, with appropriate controls to be defined through local planning based on local context.

Clause 16.01-1S (Housing supply)

Seeks to facilitate well-located, integrated and diverse housing that meets community needs through measures such as increasing the proportion of housing in designated locations in established urban areas (including under-utilised urban land) and reduce the share of new dwellings in greenfield, fringe and dispersed development areas and encouraging higher density housing development on sites that are well located in relation to jobs, services and public transport.

Clause 16.01-1R (Housing Supply – Metropolitan Melbourne)

Aims to manage the supply of new housing to meet population growth and create a sustainable city by developing housing and mixed use development opportunities in established areas. Those locations include neighbourhood activity centres - especially those with good public transport connections.

1.4 LOCAL PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following local policies of the Glen Eira Planning Scheme, summarised as follows:

Please note that while this content is correct at the time of writing, Council is in the process of updating its local planning policy. While there will be changes to specific policies it is not expected that the general intent of policy relating to NACs will change.

1.5.1 CLAUSE 21.04 – HOUSING AND RESIDENTIAL DEVELOPMENT

This policy provides objectives and strategies to guide housing and residential development in Glen Eira. The following policies are considered relevant to this built form framework:

Objective 4

- To stimulate and improve the vitality of Glen Eira’s commercial centres.

Strategies

- Encourage a mix of housing types, increased residential densities and mixed use developments within urban villages and neighbourhood centres.
- Encourage residential development, where considered appropriate, subject to consultation with traders and the wider community, above or below existing car parks in urban villages and neighbourhood centres, whilst maintaining or enhancing the existing level of parking.
- Ensure that developments in commercial areas that adjoin residential areas are sensitively designed to protect residential amenity.
- Ensure residential development in commercial areas does not contribute to traffic and car parking problems.

1.5.2 CLAUSE 21.06 – BUSINESS

This policy provides objectives and strategies to guide the development of identified urban villages and neighbourhood centres to cater for a wider variety of commercial use while retaining a hub of convenience shops within Glen Eira. The following policies are considered relevant to this built form framework:

Objectives (amongst others)

- To maintain a mix of commercial centres that cater for the needs of the Glen Eira community.
- To enhance and further develop urban villages and neighbourhood centres as the focus for community life.
- To maintain pleasant and safe public environments in commercial centres in partnership with business.

Strategies (amongst others)

- Maintain the hierarchy of commercial centres so that centre functions are easily definable and to ensure that retail and other requirements are suitably provided at each level in the hierarchy.
- Ensure that new or expanded land uses are able to be accommodated in existing commercial centres, including the encouragement of non-retail businesses, where appropriate.
- Ensure commercial activities in mixed use zone complement other commercial activities nearby and not undermine the commercial hierarchy.
- Ensure that, wherever possible, additional population growth can be accommodated (for example through multi-unit development) so existing facilities in commercial centres can continue to be supported.
- Ensure that commercial centres have a high standard of urban design and are attractive and appealing to potential customers, traders and investors.

1.5.3 CLAUSE 22.07 – HOUSING DIVERSITY AREA POLICY

This policy identifies the areas where housing diversity will be encouraged. It also seeks to ensure that the density, mass and scale of development is consistent with the role, capacity and constraints of each centre.

Objectives (amongst others)

- To encourage housing diversity in preferred strategic locations that have good access to public transport, commercial, community, educational and recreational facilities.
- To ensure that the density, mass and scale of residential development is appropriate to the location, role and neighbourhood character of the specific housing diversity area.
- To ensure that the siting and design of new residential development takes account of its interface with existing residential development on adjoining sites.
- To ensure that the design of new residential development is sensitive to and respectful of the scale of existing residential development on adjoining sites.

Strategies for Neighbourhood Activity Centres

- Recognise neighbourhood centres as locations which provide significant opportunities for housing diversity, but at a lesser scale and density than developments in urban villages and the Phoenix Precinct.
- Recognise that different development outcomes are sought in the commercial and residential areas of neighbourhood centres.

Strategies for commercial areas of centres (amongst others)

- Ensure that the density, mass and scale of development is appropriate to the scale, character and physical size of the neighbourhood centre.
- Ensure that any new residential development is designed and sited to minimise its adverse impacts on adjoining existing residential properties by way of building bulk, overlooking and overshadowing.

- Ensure that residential buildings are encouraged to step down at the rear to achieve a transition to residential areas.
- Ensure that building heights are compatible with their site context and the wider neighbourhood centre.
- Ensure that where the new building is greater in height than the prevailing building height or where significant changes in building height are proposed for residential buildings:
- There is a graduated transition in building height between the proposed building and adjoining buildings.
- The resulting height, mass and scale of the building does not dominate or visually intrude on the streetscape and takes account of views from the wider neighbourhood and at a distance.
- The upper storeys are recessive so that the visibility of upper storeys is reduced when viewed from the footpath opposite or residential properties to the rear.
- Provision be made for the establishment of new landmark buildings on strategic redevelopment sites that are made available through the removal of a level crossing.
- Ensure that the retail function and active frontage of centres is retained on the ground floor.

Strategies for the residential areas of centres (amongst others)

- Recognise that these areas offer opportunities for multi-unit development, but at a lower scale and density than development in the commercial and mixed use areas of neighbourhood centres.
- Ensure that the density, mass and scale of residential development is appropriate to that of the neighbourhood centre.
- Encourage a decrease in the density of residential development as the proximity to the commercial area of the neighbourhood centre decreases.
- Ensure that the siting and design of residential development responds positively to its interface with existing residential development in minimal change areas.

1.5.4 CLAUSE 21.10 – HERITAGE

This policy provides objectives and strategies to guide the development that does not threaten the heritage aspects of Glen Eira. The following policies are considered relevant to this study:

Objectives

To identify, protect, enhance and promote understanding of Glen Eira’s heritage.

Strategies

- Protect places identified as having architectural, cultural or historical significance.
- Ensure sympathetic redevelopment and renovation of areas and places identified as having architectural, cultural or historic significance in the municipality.
- Enhance knowledge and popular understanding of Glen Eira’s architectural, cultural and historic heritage.

1.5.5 CLAUSE 22.01 – HERITAGE POLICY

This policy applies to all land within the Heritage Overlay and seeks to identify, protect, enhance and promote understanding of Glen Eira’s heritage. More specifically, this Clause builds upon the basis of Clause 15.03 (Heritage) and Clause 21.10 (Heritage) and provides guidance on a number of elements:

Demolition (amongst others)

- Retain significant and contributory buildings.
- Allow the partial demolition of significant and contributory buildings where the fabric to be demolished is of no significance, or for the purpose of additions if the addition will not affect the heritage significance of the building and is sympathetic in its scale and form.
- To retain significant trees, and/or garden layouts that have been identified for their historical significance.

New buildings, Alterations and Additions (Commercial Heritage Areas)

- Encourage the conservation of setbacks that impart significance to those buildings that are surrounded by open space (including but not limited to churches and schools).
- Ensure any new upper level additions and works are respectful to the scale and form of the heritage place or contributory elements of the place and, where relevant, the heritage precinct as a whole.
- Encourage higher building additions to be well set back from the front wall of the building unless the specific context of the site recommends otherwise.
- Encourage the retention, restoration or reconstruction of original shopfronts and verandahs.
- Ensure commercial infill buildings adopt a contemporary architectural form or simplified interpretation of nearby contributory buildings.
- Discourage the introduction of architectural features, where it is known that these features were not originally present.
- Discourage signage above the verandah if it results in visual clutter in the streetscape and obscures views of the subject building and nearby contributory buildings.
- Ensure retention of signage deemed to have heritage value.
- Discourage sky signs, reflective signs, animated signs and electronic signs within heritage precincts

1.6 ZONES

1.6.1 CLAUSE 34.01 COMMERCIAL 1 ZONE (C1Z)

The NAC is predominantly included within the Commercial 1 Zone the purpose of which is (amongst others) to support vibrant mixed use commercial centres for retail, office, business, entertainment and community uses, and additionally provides for residential uses at densities complementary to the role and scale of the commercial centre.

The zone contains decision guidelines that ask decision makers to consider certain aspects of development when deciding planning applications. The most relevant decision guidelines for this zone are:

Amenity

- Consideration of the interface with adjoining zones, especially the relationship with residential areas.
- Consideration of the overlooking and overshadowing as a result of building or works affecting adjoining land in a General Residential Zone, Neighbourhood Residential Zone, Residential Growth Zone or Township Zone.
- The objectives, standards and decision guidelines of Clause 54 and Clause 55. This does not apply to an apartment development. For an apartment development, the objectives, standards and decision guidelines of Clause 58.

Built form

- Consideration of the streetscape, including the conservation of buildings, the design of verandahs, access from the street front, protecting active frontages to pedestrian areas, solar access, the treatment of the fronts and backs of buildings and their appurtenances, illumination of buildings or their immediate spaces and the landscaping of land adjoining a road.

Transport

- Considerations relating to the movement of pedestrians and cyclists, and vehicles providing for supplies, waste removal, emergency services and public transport.

1.6.2 CLAUSE 36.01 PUBLIC USE ZONE (PUZ6)

Several allotments within the north-east and south portion of the Bentleigh East Activity Centre are included within Schedule 6 to the Public Use Zone. The PUZ seeks to recognise public land use for public utility and community services and facilities, and provides for associated uses that are consistent with the intent of the public land reservation or purpose.

PUZ6 designates the associated public land use for the purpose of ‘Local Government’.

The relevant decision guidelines of the PUZ6 includes the following:

- The comments of any Minister or public land manager having responsibility for the care or management of the land or adjacent land.
- Whether the development is appropriately located and designed, including in accordance with any relevant use, design or siting guidelines.

1.6.3 CLAUSE 32.04 MIXED USE ZONE (MUZ1)

A pocket of land to the east of East Boundary Road within the Bentleigh East Activity Centre Zone is within Schedule 1 to the Mixed Use Zone the purpose of which is to provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality, housing at higher densities, and additionally encourage development which responds to the existing or preferred neighbourhood character of the area.

Schedule 1 to the Mixed Use Zone is designated ‘Glen Eira Mixed use Areas’.

The relevant decision guidelines of the MUZ1 ask decision makers to consider:

Development

- The impact of overshadowing on existing rooftop solar energy systems on dwellings on adjoining lots in a Mixed Use Zone or Residential Growth Zone.
- For two or more dwellings on a lot, dwellings on common property and residential buildings, the objectives,

standards and decision guidelines of Clause 55. This does not apply to an apartment development of five or more storeys, excluding a basement. For an apartment development of five or more storeys, excluding a basement, the objectives, standards and decisions guidelines of Clause 58.

Use

- The effect of traffic to be generated by the use.
- The interim use of those parts of the land not required for the proposed use.
- Whether the use is compatible with adjoining and nearby land uses.
- For non-residential uses, the proposed hours of operation, noise and any other likely off-site amenity impacts.

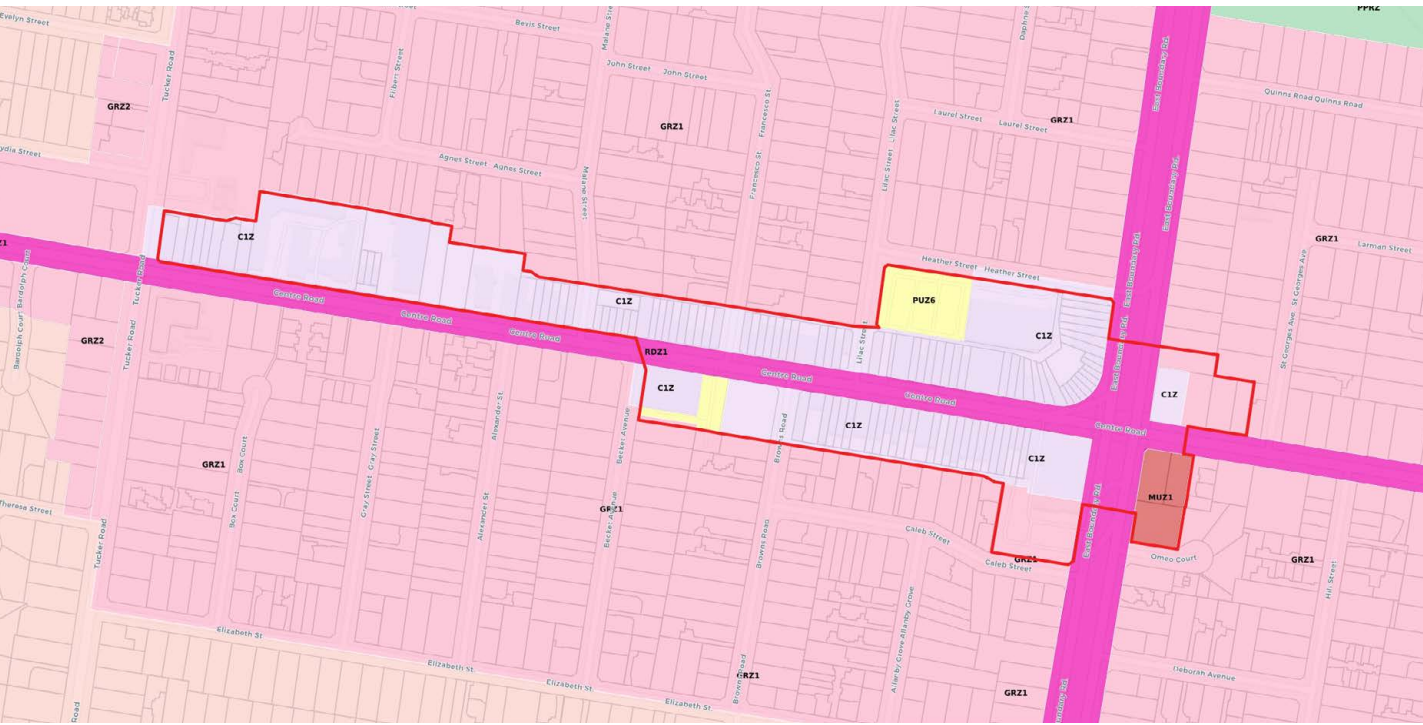


Figure 5. Planning Zones

1.7 OVERLAYS

1.6.4 CLAUSE 43.01 HERITAGE OVERLAY (HO99)

The Heritage Overlay aims to conserve and enhance heritage places of natural or cultural significance and to ensure that development does not adversely affect the significance of heritage places.

The HO (Schedule 99) applies to a single allotment within the western portion of the Study Area namely '675 Centre Road, Bentleigh East'.

The relevant decision guidelines ask decision makers to consider:

Heritage significance

- The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.
- Any applicable statement of significance (whether or not specified in the schedule to this overlay), heritage study and any applicable conservation policy.

Built form

- Whether the location, bulk, form or appearance of the proposed building will adversely affect the significance of the heritage place.
- Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.

Demolition, works and subdivision

- Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place.
- Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.
- Whether the proposed subdivision will adversely affect the significance of the heritage place.
- Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.

- Solar energy systems
- Whether the location, style, size, colour and materials of the proposed solar energy system will adversely affect the significance, character or appearance of the heritage place.

1.6.5 CLAUSE 45.09 PARKING OVERLAY (PO2)

Schedule 2 to the Parking Overlay applies to the entire NAC. Schedule 2 to the Parking Overlay is titled 'Student Housing in Specific Areas'. Where student housing is proposed in this area, the schedule aims to provide car parking at a rate commensurate with the reduced ownership pattern of students, allowing a reduced rate of on-site car parking of 0.5 spaces per student housing bed provided.



Figure 7. Heritage Overlay Plan



Figure 6. Parking Overlay Plan

2 ANALYSIS, ISSUES AND OPPORTUNITIES

The following chapter outlines analysis, issues and opportunities to be addressed through the development of the Built Form Framework for the Bentleigh East NAC.

2.1 TOPOGRAPHY

Figure 8 reveals the elevation of landform within the NAC and surrounds, and identifies key topographic features.

The mapping shows that there is a subtle rise to the east with a ridgeline located outside of the study area. There is a fall of 5m from the eastern edge to the western edge of the centre.

The flat topography makes it easy to walk within the centre and will not present any constraints to future development.

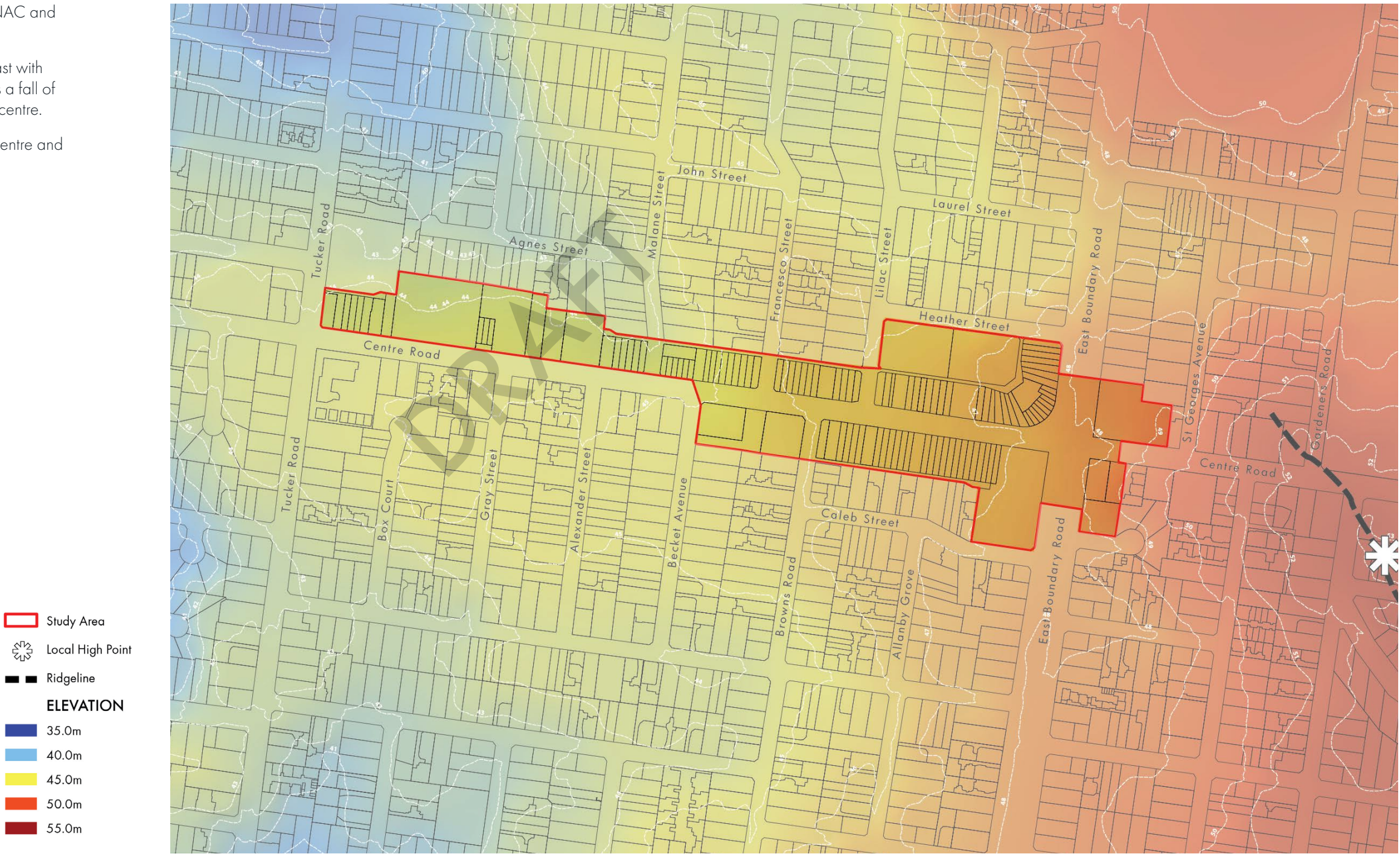


Figure 8. Elevation Plan

Figure 9 provides an assessment of land slope within the NAC.

The slope on the majority of sites is in the 0-2.5% range, which is generally flat. This presents minimal constraints to development opportunities.

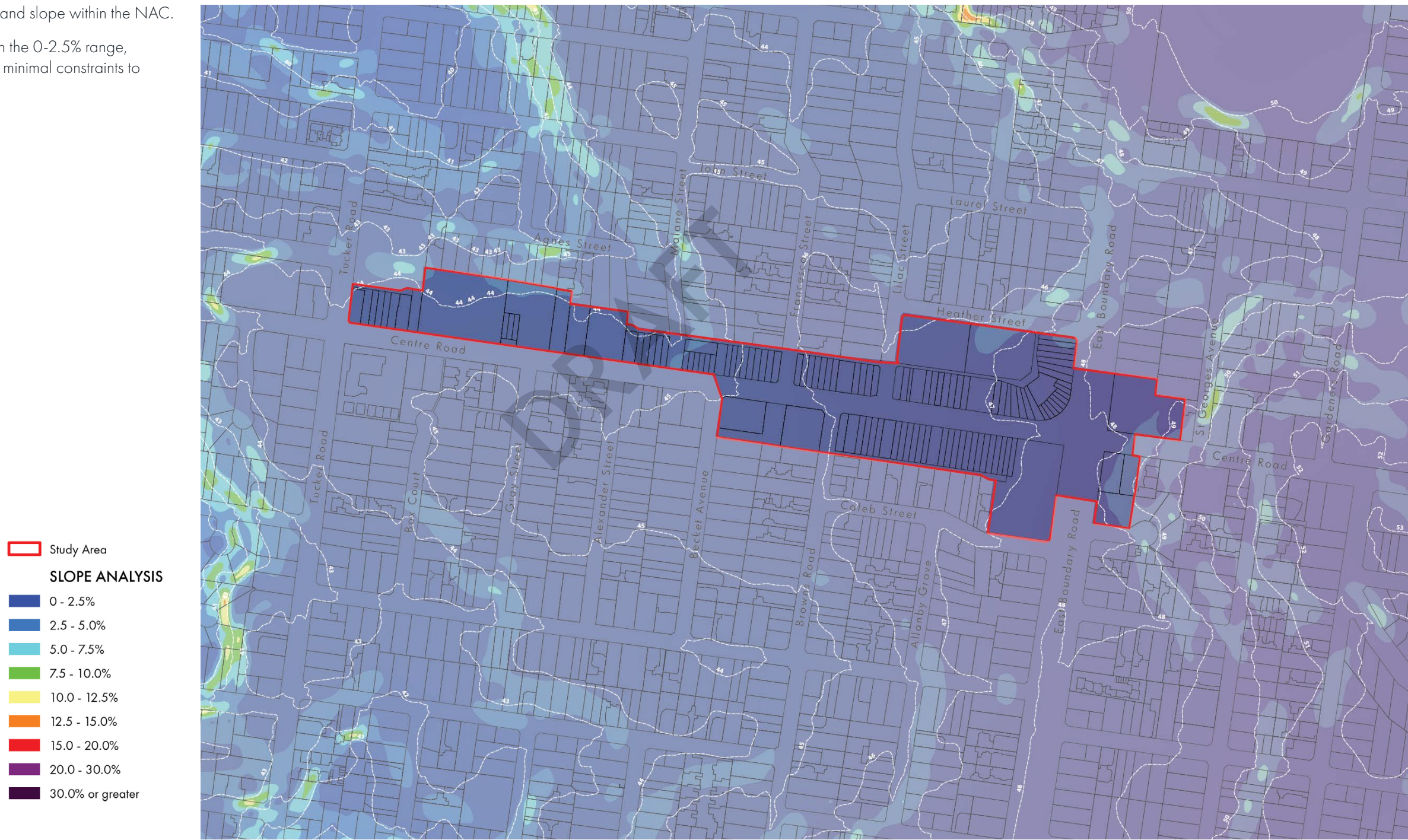


Figure 9. Slope Analysis

2.2 LAND USE & INTERFACES

Figure 10 identifies land use precincts and a range of interface conditions across the NAC.

The Bentleigh East NAC is a relatively large centre extending along Centre Road for a length of 770m. It provides a local retail, hospitality and service role for the community (see Image 1-2). Compared to other NAC's its public transport offering is limited, with bus routes located along Centre Road and East Boundary Road. However it does include a higher frequency SmartBus route on Centre Road.

The Bentleigh East NAC is broadly defined by four areas of activity. A core retail area is located at the eastern end of Centre Road. This area is the most vibrant within the NAC and includes a range of hospitality uses. It is anchored by the IGA which fronts onto a car park at the rear of the shops and is accessible from Centre Road via a walkway. There are a number of double fronted shops in this location taking advantage of the customer activity generated by the IGA.

A small secondary retail area is located at the western end of the NAC focused around the Tucker Road intersection. This area is separated from the balance of the NAC by larger format uses including a service station and factoryettes, and an Australia Post warehouse. These uses have limited engagement with the footpath and generate minimal pedestrian activity.

East of East Boundary Road are two properties which have been recently developed providing a mix of retail and residential uses. The expansive road reserve separates these properties from the majority of the NAC.

The NAC has a predominant interface with residential uses. There is a mix of laneways separating residential and commercial uses, direct abuttal between the uses, and a small area where a road separates the uses (see Image 3). There is a substantial section of the NAC with residential uses located to the south of commercial properties. Potential shadow impacts from new development will be the greatest in this location.

2.2.1 KEY ISSUES AND OPPORTUNITIES

- There are a number of new mixed use redevelopments within the activity centre providing a narrow shop-front character of the street (see Image 4). There is an opportunity to continue this character across the NAC.
- Large at-grade car parks and building loading areas near IGA front onto a number of residential streets in the centre. There are opportunities to improve this interface.
- There are several poor street address related to at-grade car park and car-related land use along Centre Road, as well as blank wall interface on side streets (see Image 5). There are opportunities to improve these areas.
- The residential interface to the south provide constraints for future building heights and setbacks. Development will require a sensitive design response to transition to adjoining residential areas (see Image 6).



Image 1. Hospitality uses at the eastern end of Centre Road take advantage of the widened footpaths created by the pedestrian crossing



Image 2. A mix of hospitality and personal services are located at the eastern end of Centre Road



Image 3. An existing commercial, laneway and residential interface



Image 4. Narrow shopfronts in a recent development wrap around the corner to activate the secondary frontage



Image 5. Recent developments on Centre road with large basement car park entries and blank walls diminish continuous activity along the street



Image 6. A mixed use development of five storeys scales effectively to the adjoining residential area

- LAND USES**
- Primary Retail
 - Office / Commercial / Secondary Retail
 - Mixed Use - Redevelopment Sites
 - Open Space
- INTERFACES**
- Residential Interface - Laneway Separation
 - Residential Interface - Direct Abuttal
 - Open Space Interface
 - School/Community Interface
 - Poor Street Address / Interface

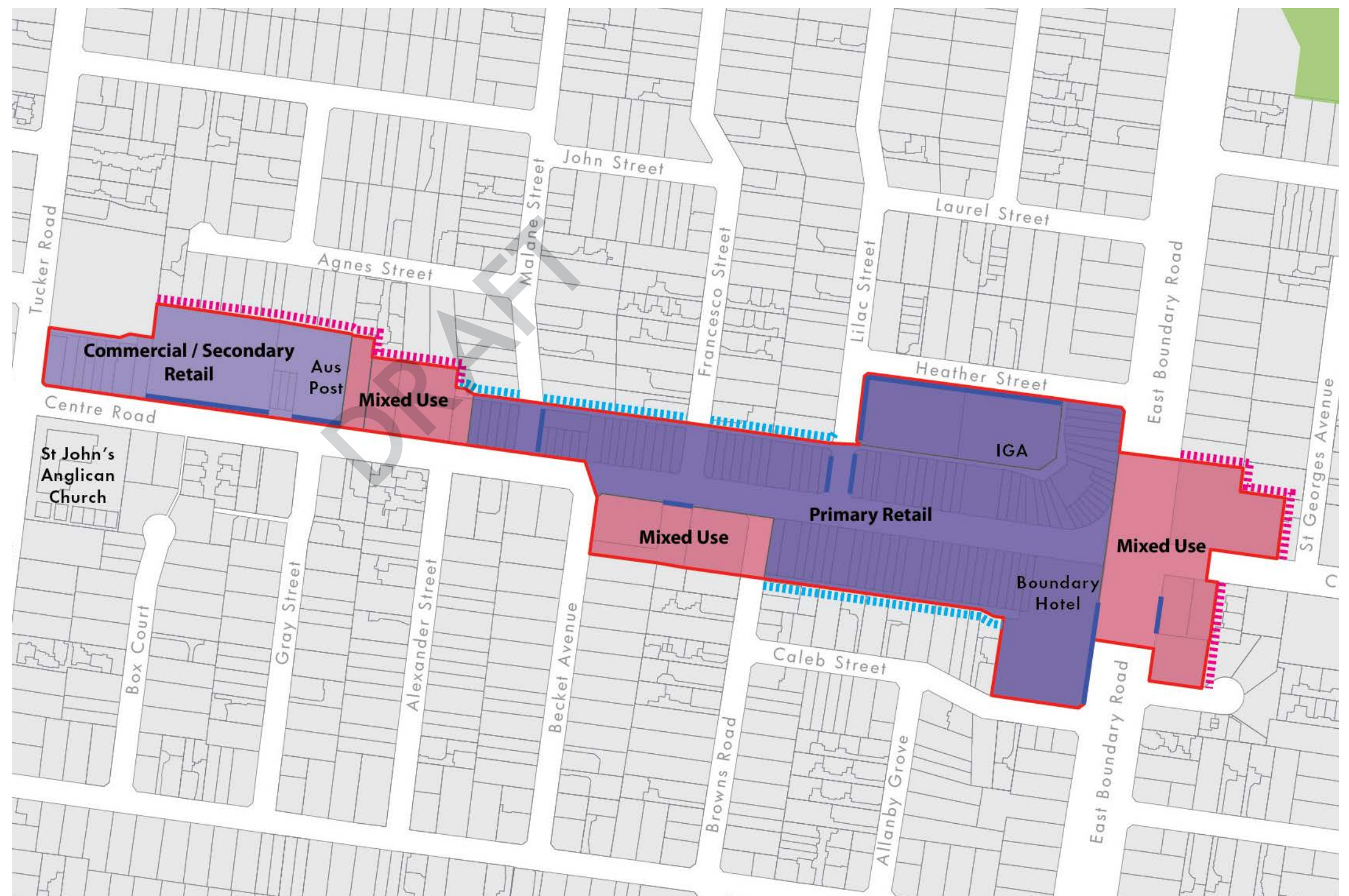


Figure 10. Land Use and Interfaces Analysis

2.3 BUILT FORM CHARACTER & PUBLIC REALM

Figure 11 maps a range of built form character and public realm elements across the Bentleigh East NAC.

Understanding the existing and emerging built form and character of the Bentleigh East NAC is important in order to identify character elements that should be protected, how built form could be improved and assess the impact of new, taller development on the streetscapes.

The Bentleigh East NAC has low-scale built form character with a strong presence of single storey buildings. Shopfronts are narrow providing a fine-grain character however they have limited architectural detailing and generally low parapets (see Image 7).

There are a number of large scale buildings of 3-5 storeys located on the southern side of Centre Road and in the western sections of the NAC (see Image 8-9). These buildings provide a stronger presence of built form to the street with a three storey street wall. Similarly two new developments on the east side of East Boundary Road of 5-6 storeys and provide a strong built form bookend to the centre (see Image 11).

The service station and Australia Post sites in the western sections of the NAC occupy a substantial frontage to Centre Road. Buildings on these sites are set back and provide limited activation to the footpath (see Image 12). There is a significant opportunity to improve this frontage to create continuous activity along Centre Road.

Weather protection through building awnings is well provided across the NAC. With the exception of the service station and Australia post site, weather protection is provided to the retail footpaths of Centre Road and the small frontage along East Boundary Road.

The footpaths along Centre Road are key public spaces. The southern footpath has good access to sunlight providing opportunities for on street dining and gathering.

2.3.1 KEY ISSUES AND OPPORTUNITIES

- The existing one storey shops provide limited built form presence in the streetscape. There are opportunities to replace these buildings with higher scale development and create a more contemporary built form character.
- The narrow or 'fine-grain' shop-fronts, particular on the east end of Centre Road, are an important part of the streetscape character and provide for a diversity of uses and experiences. This should be retained and reflected in new development across the NAC.
- It will be important to maintain solar access to key pedestrian streets in the activity centre, particularly the southern footpath of Centre Road.
- There is generally a lack of "green" public spaces in the activity centre. There are opportunities to provide additional greenery within the Centre Road Streetscape, and potential footpath widening through kerb outstands in strategic locations (see Image 13). There may be an opportunity to provide new public spaces on larger sites when they are redeveloped .
- Signalised pedestrian crossings are located at Tucker Road junction, East Boundary Road junction, and between East Boundary Road and Lilac St on Centre Road. Given the significant length of the strip there may be opportunities to provide additional crossings along the strip Centre Road.
- There is an internal connection between Centre Road and IGA entrance in one retail shop (Bakers Delight). This type of mid-block internal links should be retained and encouraged in future developments.



Image 7. Existing shopfronts on Centre Road are generally single storey with low parapets and limited architectural detailing



Image 9. Recent mixed use developments of 4 and 5 storeys on Centre Road create a new urban character with a 2-3 storey street wall



Image 11. Recently developed buildings of a substantial scale mark the intersection of Centre Road and East Boundary Road



Image 8. Recent development of 6 storeys is of a significant scale compared to the heritage protected East Bentleigh Hall



Image 10. The building scale and character at the western end of the centre is mixed with a number of redeveloped properties interspersed with original shopfronts



Image 12. The Australia Post and Service Station diminish activity along Centre Road



Image 13. Centre Road Footpaths support limited outdoor dining because of width constraints. Opportunities for kerb extensions in strategic locations to increase street activity

- BUILT FORM**
- 'Fine grain' Retail
 - Existing Awnings
 - Poor Street Address / Interface
 - Inconsistent Setbacks
 - Existing Heritage Overlays
 - Prominent Corners
 - Existing 1-2 Storey Building Height
 - Existing 3+ Storey Building Height
 - Approved 3+ Storey Building Height
 - Existing At-grade Car Park
- PUBLIC REALM**
- Primary Public Realm Areas
 - Secondary Public Realm Areas
 - Laneway Access



Figure 11. Built Form Character and Public Realm Analysis

2.3.2 EXISTING SHADOW ANALYSIS

Figure 12 demonstrates the shadows cast by existing buildings within the Bentleigh East NAC. Development above 3 storeys and recent approvals are shown separately in green.

The date selected for the shadow analysis is September 22 - the equinox. This represents the mid-point between the summer solstice and winter solstice. It is the date commonly used in planning assessments to assess the impact of shadows.

The analysis demonstrates that more recently developed buildings on Centre Road cast more shadow than the original low scale shopfronts however the shadow does not impact on the southern footpath.

The 3D model for the shadow analysis has been developed using PSMA Geoscape building footprint data which provides an outline of the building footprint and overall roof height. The building footprints have been extruded up to the overall roof height and integrated into a topographic model.

9am - Sept 22



10am - Sept 22



- NAC Boundary
- Approved / Constructed Development of 3 storeys or greater
- Shadow from existing and approved buildings

Figure 12. Existing Shadow Analysis



1pm - Sept 22



2pm - Sept 22





2.4 DEVELOPMENT ACTIVITY AND CAPACITY

Figure 14 maps recent development activity and an indication of constrained and opportunity sites within the centre based on their size

Development activity and capacity is an important consideration in preparing the Built Form Framework. Recent development can indicate the type of development that the centre will attract in the future. Analysing the potential capacity of the centre is also important to understand the scale and type of development that could potentially be accommodated.

Development activity has been spread across the Bentleigh East NAC. Building heights for new development range between 3 and 6 storeys. The lower scale developments have tended to be located on smaller sites in the western sections of the NAC.

2.4.1 KEY ISSUES AND OPPORTUNITIES

- The mapping opposite identifies areas at the eastern sections of Centre Road where future development may be constrained, due to lot size, width and depth. Lot consolidation may be required to support development (see Image 14-15)
- There are a number of sites that present good opportunities for development based on lot size.
- The NAC has an interface with the General Residential Zone which allows development of up to three storeys. It will be important for future building heights to transition sensitively to the surrounding residential areas.



Image 14. Smaller sites on Centre Road that may be constrained for development due to their width unless they are consolidated



Image 16. A recent development of a wider lot on Centre road



Image 15. Although properties are fairly narrow at the western end of Centre Road, some sites have been consolidated and redeveloped



Figure 14. Development Activity, Opportunities and Constraints

2.5 LOT WIDTH ANALYSIS & TYPOLOGIES

Figure 15 provides analysis of lot widths across the Bentleigh East NAC and Figure 16 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot width.

The typology analysis has been undertaken on the basis of a residential development outcome. Commercial development would be less constrained with less of a need to provide daylight into habitable rooms.

Front and rear facing apartments provide the simplest development outcome for narrow lots, as they can facilitate 1-2 apartments facing the street, and one facing the rear of the property, on each floor (subject to width). This type of development occupies the entire width of the lot, and does not provide any articulation or activation to either side of the development (which is assumed will develop in a similar manner).

Wider properties can accommodate a different model of development, podium and tower, which can have an outlook to other developments either side. This type of development can provide for a greater range of apartment types with varying aspects. It does however create issues of apartment separation for privacy and daylight that need to be considered.

The analysis indicates that there are a number of narrow lots at the eastern end of the centre, which would not support apartment development in their current configuration. Sites west of Becket Avenue are wider and will support a range of building configurations.

While this analysis identifies potential development outcomes for individual properties, it does not take into consideration the potential for multiple properties being consolidated, which often occurs in Activity Centres.



Figure 15. Lot Width Analysis

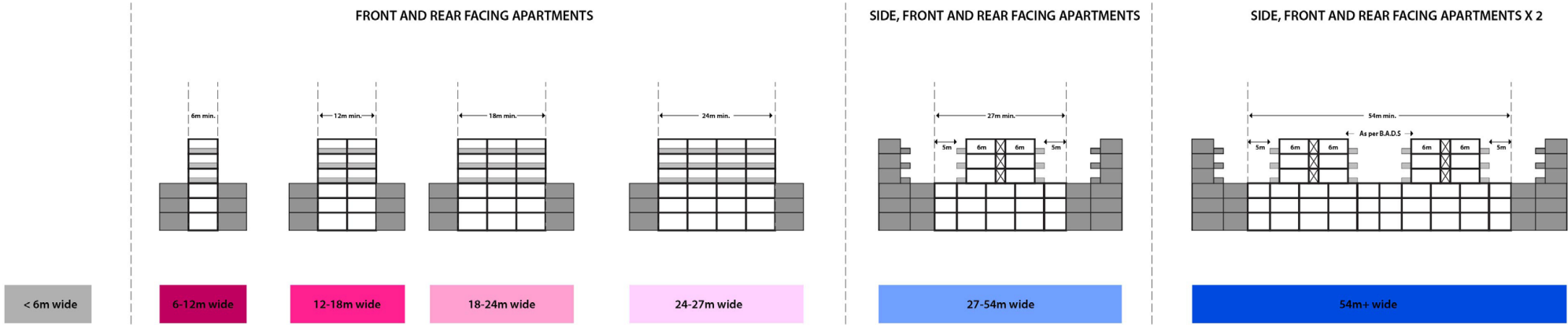


Figure 16. Lot Width Typologies

2.6 LOT DEPTH ANALYSIS & TYPOLOGIES

Figure 17 provides an analysis of lot depths across the NAC and Figure 18 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot depth.

Similar to lot width, the depth of a property has an impact on the type of development that can be provided as shown on the typologies opposite.

The typologies indicate that shallow lots provide a limited opportunity for development, and allow for only single aspect apartments. This takes into account a typical setback to a residential interface, and a suitable upper level setback to the primary street frontage.

Lots which are deeper provide opportunities for dual aspect apartments, addressing the primary street, and the rear of the lot.

Lots greater than 55m (approx.) could allow for two buildings with both front and rear facing apartments. This outcome does create apartment separation issues that need to be considered.

The analysis indicates that the majority of sites are deep enough to support front and rear facing apartment developments. Deeper lots are located west of Alexander Street, along East Boundary Road and along Heather Street and could support alternative configurations.



Figure 17. Lot Depth Analysis

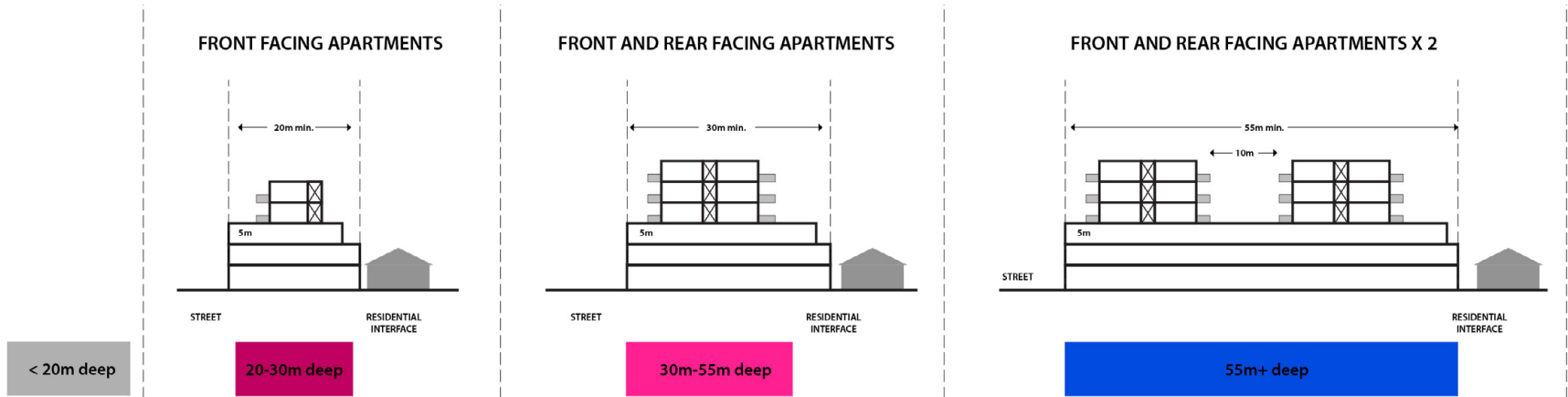


Figure 18. Lot Depth Typologies

PART 2 - THE BUILT FORM FRAMEWORK

3 DESIGN PRINCIPLES

3.1 THE PRINCIPLES

4 BUILT FORM FRAMEWORK

4.1 BUILT FORM FRAMEWORK

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

4.3 PRECINCT 1 - EASTERN JUNCTION

4.4 PRECINCT 2 - CENTRE ROAD RETAIL

4.5 PRECINCT 3 - CENTRE ROAD WEST



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3 DESIGN PRINCIPLES

The following over-arching principles have been prepared to guide the drafting of design objectives and requirements for the Bentleigh East NAC. The principles aim to respond to the issues and opportunities identified in Part 1 relating to amenity and character, and an understanding of the centre’s strategic role in meeting future housing and employment needs.

3.1 THE PRINCIPLES

PRINCIPLE 1 - A VIBRANT NEIGHBOURHOOD CENTRE WITH A MID-RISE CHARACTER

MID-RISE CHARACTER

Glen Eira includes a mix of both Major Activity Centres (MACs) and Neighbourhood Activity Centres. The MACs play a significant role for the community providing a number of public transport options, and access to retail, services and employment opportunities. The NACs play an important, though less significant role in meeting future needs for housing and employment as they are smaller in size with less retail and employment opportunities. The Bentleigh East NAC also has comparatively less public transport options than some others with fixed rail.

The Glen Eira City Plan outlines the preferred focus for the NACs and notes that they will have a ‘medium’ focus for employment growth and a ‘moderate’ focus for housing growth. Bentleigh East is identified as a Neighbourhood Activity Centre and as such is intended to experience moderate growth.

The Bentleigh East NAC is somewhat constrained for development. It includes areas of smaller lots, a residential interface along most of its edges and key footpaths which need to be protected from overshadowing. Combined together, these constraints will limit development opportunities. However there are a number of larger sites in the NAC where higher scale development can be accommodated within minimal visual and amenity impact.

The recommended approach for the Bentleigh East NAC is to establish a mid-rise character providing for building heights in the 5-6 storey range. These heights will support further growth of the centre whilst sensitively integrating with the surrounding residential neighbourhoods.

PRINCIPLE 2 - AN ENJOYABLE AND SAFE PLACE TO SHOP, WORK, GATHER, DINE AND EXERCISE

REINFORCE THE HUMAN SCALE TO KEY STREETS

Existing buildings within the Bentleigh East NAC are generally low scale, between one and two storeys in height. The low scale character of the centre will change over time and there has been some recent development in the centre of five to six storeys.

It is important that new, taller buildings are designed in a way that integrates with the existing low scale of the centre and do not dominate the streetscape. This is possible by providing a lower scale building at the street edge and setting the taller elements further behind.

A three storey building height at the street is recommended for the Bentleigh East NAC, which will maintain a scale that is both compatible with existing one and two storey shopfronts, and does not overwhelm the streetscape. This has been evidenced with recent developments in the NAC that provide a three storey street wall.

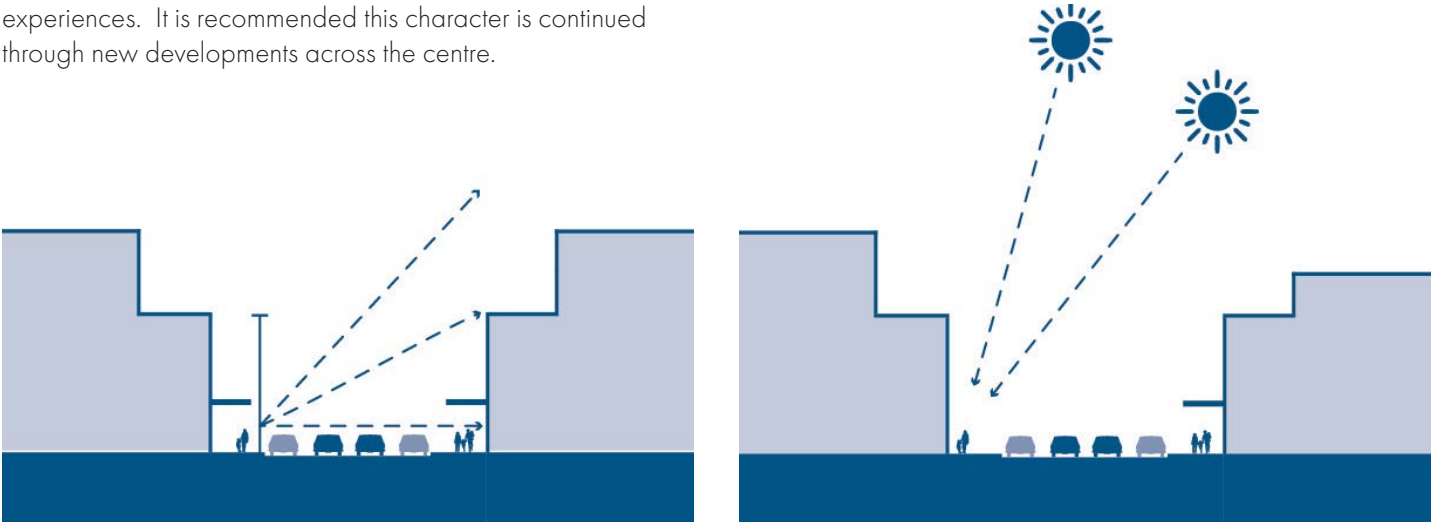
Another aspect to the human scale in the Bentleigh East NAC is the ‘fine-grain’ narrow shopfronts, which provide visual interest and provide for a greater diversity of uses and experiences. It is recommended this character is continued through new developments across the centre.

MAINTAIN SOLAR ACCESS TO KEY FOOTPATHS

The Bentleigh East NAC is limited in terms of its public gathering spaces such as squares and parks. Therefore the footpaths play an important role as spaces for people to gather and interact. Maintaining sunlight to key footpaths is important to encourage outdoor dining and street based retail, and support the vitality of the centre.

The Built Form Framework Plan for the Bentleigh East NAC identifies the key footpaths where access to sunlight is considered to be important. This includes the southern footpath of Centre Road, where retail and hospitality activity is currently focused, and the eastern and western footpath of East Boundary Road.

Sunlight access is often measured at the equinox (22 September) in Planning Schemes across Victoria. This date sits mid-point between the winter solstice (June 22) where shadows are at their longest, and the summer solstice (December 22) where shadows are at their shortest. For the Bentleigh East NAC Built Form Framework, the Equinox was selected as the date to measure solar access.



The following measures for solar access have been adopted for the Built Form Framework. These time periods will ensure sunlight is provided to the footpaths at the most active times of the day, which will help to support hospitality and retail uses.

These measures were tested and considered to provide a balance between providing good solar access whilst not unreasonably limiting development opportunities in the centre.

Maintain sunlight to key footpaths as follows:

- Southern footpaths of Centre Road - Solar access from 10am on September 22
- Western footpaths of East Boundary Road - Solar access from 10am - 12pm on September 22
- Eastern footpaths of East Boundary Road - Solar access from 12pm - 2pm on September 22

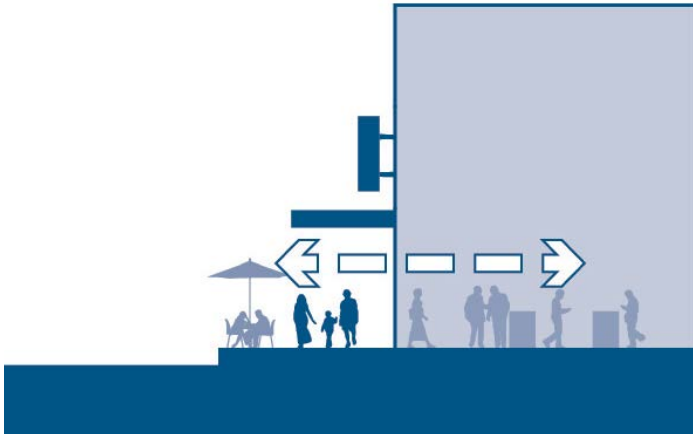
PROVIDE FOR A CONTINUOUS NETWORK OF ACTIVE FRONTAGES WITH WEATHER PROTECTION

Continuous retail and business activity across the Bentleigh East NAC is key to providing a positive pedestrian experience. Locations where there are blank walls, car park or loading areas to the street, disrupt the flow of retail activity and provide limited passive surveillance of the footpaths.

All streets across the Bentleigh East NAC are treated as active frontages, with windows at ground level, and uses at the front of the building that provide for customer engagement. Buildings on corners will provide active frontages to both streets. In addition, uses above the ground level are encouraged to address the street with windows and balconies.

All buildings should be constructed with floor to ceiling heights that would support retail, commercial and hospitality uses at ground level. This will allow for buildings to be easily adapted for such uses into the future and further strengthen street based activity.

Existing awnings provide weather protection along the majority of properties on Centre Road. New developments should continue this element with awnings placed at lowered heights to reinforce an intimate pedestrian environment.



PRINCIPLE 3 - A CENTRE THAT EMERGES SENSITIVELY FROM THE SURROUNDING NEIGHBOURHOOD AND CONSIDERS RESIDENTIAL INTERFACES BOTH WITHIN AND OUTSIDE THE CENTRE

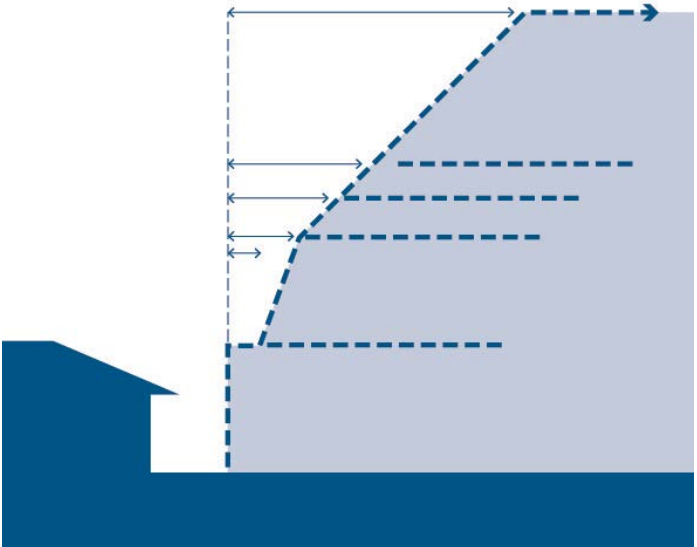
ENHANCE THE RESIDENTIAL INTERFACE

The Bentleigh East NAC abuts residential uses along all of its edges. The interface varies between direct abuttal to a residential property or a laneway separating the uses.

It is important that development in the NAC transitions appropriately to the residential edge to protect the amenity of these areas.

For locations where there is a direct abuttal to residential properties, a single storey built form edge is proposed, which is then recessed to minimise visual impact and reduce the impacts of overlooking and overshadowing. Where a laneway exists, a two storey built form edge is proposed to the laneway which then recesses considerably.

In addition to the upper level setbacks, development will need satisfy relevant requirements from Clause 54, 55 and 58 of the Glen Eira Planning Scheme to further protect residential amenity.

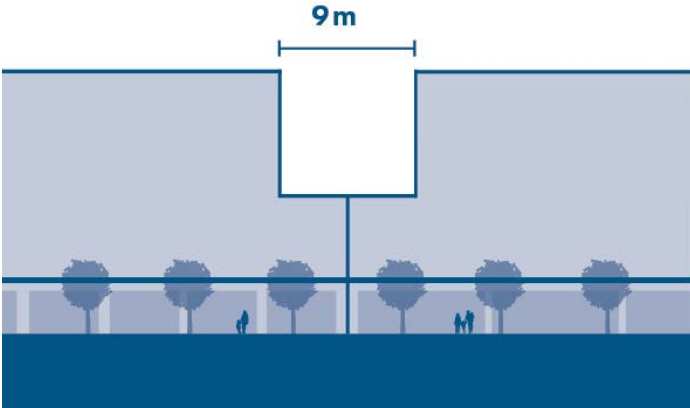


PROVIDE FOR EQUITABLE ACCESS TO AMENITY

Development across the Bentleigh East NAC is likely to be sporadic with potential for new, taller buildings to be located adjacent to existing, low scale buildings for a substantial period of time. It is important to have measures in place to ensure the future development potential of adjoining sites is not significantly compromised by the first development.

A key consideration in equitable access is ensuring adjoining buildings have sufficient separation, to limit overshadowing and ensure adequate privacy for apartments and access to daylight. The centre-wide frameworks provide for a 9 metre separation distance between apartment developments where they have balconies or windows of habitable rooms facing each other.

Zero side and rear setbacks are encouraged from ground level, up to 3 storeys. Land uses on these levels will typically be retail or office uses where access to sunlight and privacy issues are less critical.



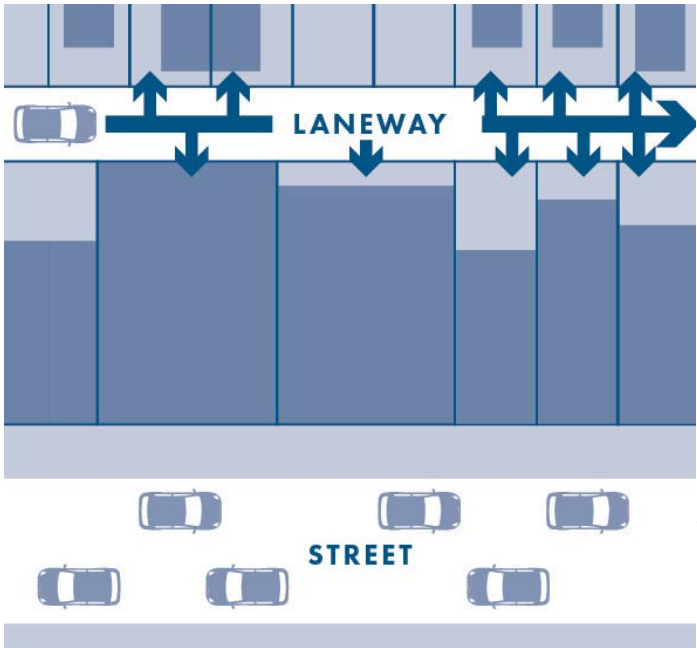
PRINCIPLE 4 - A WELL SERVICED AND ACCESSIBLE CENTRE

ENSURE ADEQUATE SERVICING OF EXISTING AND NEW DEVELOPMENTS

As the Bentleigh East NAC grows and intensifies with additional development, so do its servicing and access requirements for buildings. This includes access to car parking for residents, workers and customers, access for service vehicles, and pedestrian and cycle access.

It is important that new development takes advantage of existing service access arrangements through laneways in order to avoid vehicle disruption to footpaths. Where no laneway exists, driveway crossovers should be located on secondary frontages and minimised in width.

Another important consideration is ensuring new development can be accessed adequately by pedestrians and cyclists.



4 BUILT FORM FRAMEWORK

This section outlines Centre-Wide Design Objectives and Requirements that will apply to all development within the NAC, and more specific building height, setback and design requirements that apply to three Built Form form precincts identified in Figure 19.

4.1 BUILT FORM FRAMEWORK

Figure 19 - Built Form Framework Plan, identifies the preferred heights, setbacks and other built form requirements across the Bentleigh East NAC.

The proposed building heights have been developed through application of the design principles outlined above, responding to key issues and opportunities identified in Part 1 of this report, and testing of the built form outcomes through 3D modelling.

The recommended heights have taken into account a number of considerations including responding to sensitive interfaces, ensuring solar access to key footpaths and public spaces, and understanding the development potential of properties based on size and other constraints.

This plan identifies the key public realm areas, where good access to sunlight is required. These areas include existing footpaths where there is a high amount of pedestrian activity. Upper level setbacks are recommended at these locations to maintain good access to sunlight.

The strategic justification and rationale for the application of building heights and setbacks in each precinct is discussed in detail in Sections 4.3 to 4.5.

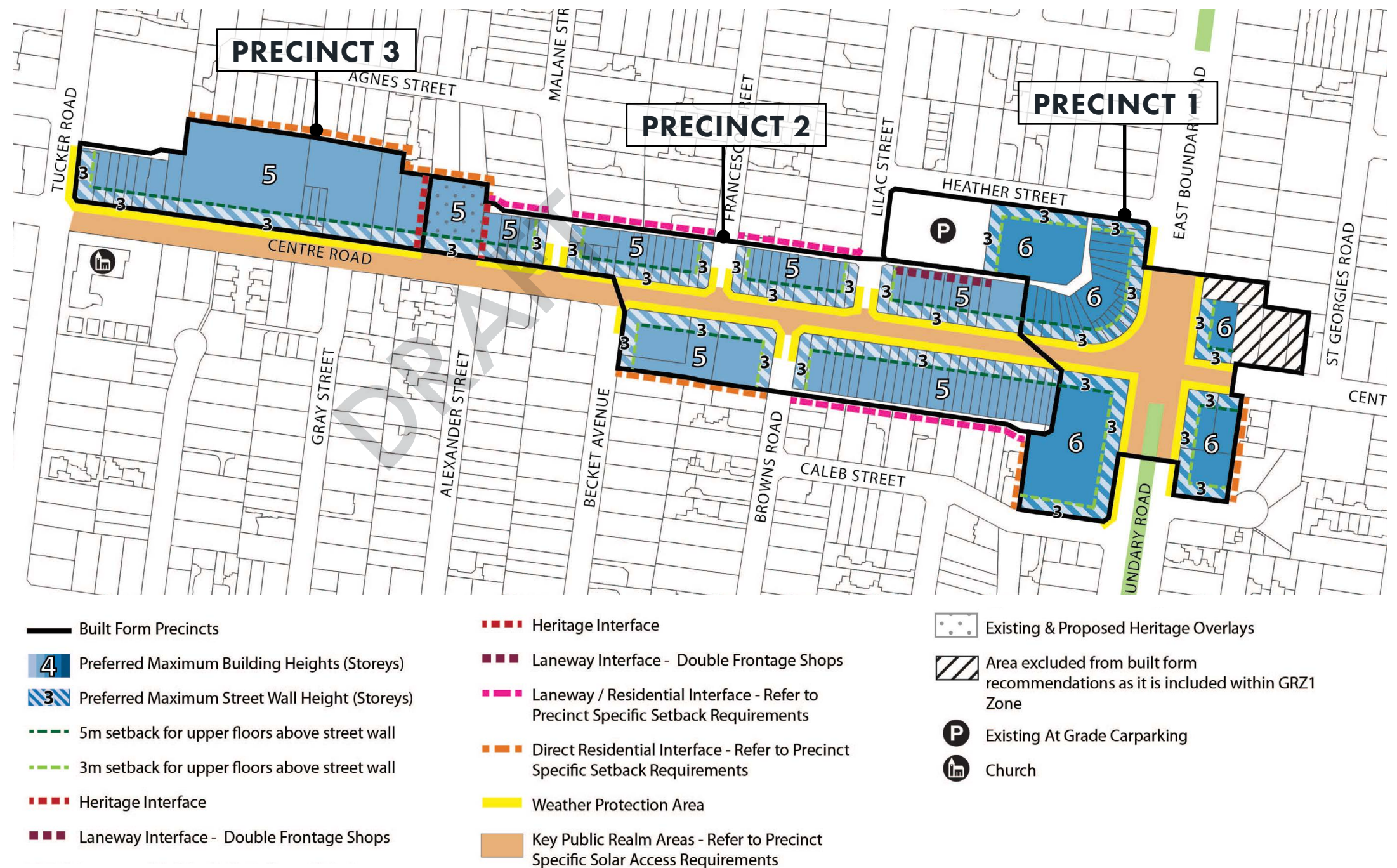


Figure 19. Built Form Framework Plan

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

The following design objectives and design requirement apply to development within all areas of the Bentleigh East NAC.

4.1.1 DESIGN OBJECTIVES

- To support a new mid-rise scale built form character for the centre with lower built form at the street interface and to adjoining residential areas.
- To enhance the intersection of Centre Road and East Boundary Road with high quality and prominent built form.
- To provide a more cohesive built form character and improved street interface along the western end of Centre Road.
- To ensure development enhances the pedestrian experience through improved activation at ground floor and maintaining sunlight to the Centre Road and East Boundary Road footpaths.
- To ensure development protects existing residential amenity and does not overwhelm adjoining residential properties.

4.1.2 DESIGN REQUIREMENTS

Building heights

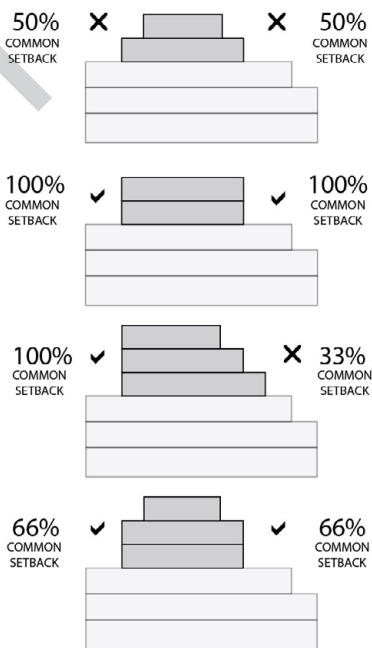
- The preferred maximum building height excludes rooftop services which should be hidden from view from any adjoining public space or designed as architectural roof top features. Roof top services includes but is not limited to plant rooms, air conditioning, lift overruns and roof mounted equipment.
- Buildings should be designed with a floor-to-floor dimension of a minimum of 4.0 metres at ground floor to enable adaptation for other uses in the future.

Building separation

- Where development shares a common boundary, upper level development should:
 - Be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
 - Be setback a minimum of 3.0m from the common boundary where a commercial or non-habitable window is proposed.

Building form and design

- Building design should minimise the visual bulk of large buildings through significant breaks and recesses in building massing.
- Development should avoid repetitive stepped building forms by providing a common street and rear setback for a minimum of 65% of the upper levels above street wall. Refer to diagram below:



- Buildings should reflect the existing fine grain pattern (narrow shopfronts) within the shopping strip by incorporating separate ground floor tenancies and vertically and horizontally modulated forms that integrate with the streetscape context.
- Roof forms should be integrated with the overall building façade design.
- Buildings should utilise materials that do not generate glare, and can withstand the effects of weathering.
- All new buildings are to incorporate best practice Environmentally Sustainable Development (ESD) principles. Refer to the Sustainable Design Assessment in the Planning Process (SDAPP) Framework.

Heritage Design

- These requirements apply to properties within an existing or proposed Heritage Overlay or abutting an existing or proposed Heritage Overlay.
- Facade design and details for infill buildings and new buildings should:
 - Be simple and not compete with the elaborate detailing of the adjoining heritage buildings.
 - Respect the vertical proportions of the heritage streetscape and adjoining heritage buildings.
 - Avoid large expanses of glazing except for ground floor shopfronts.
 - Adaptation and reuse of heritage buildings should:
 - Maintain existing openings and avoid highly reflective glazing in historic openings.
 - Encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings.
 - Maintain the inter-floor height of the existing building and avoid new floor plates and walls cutting through historic openings.
 - Development above the street wall on land within a Heritage Overlay or adjoining a Heritage Overlay should:
 - Be visually recessive and not dominate the heritage building and streetscape.
 - Utilise materials and finishes that are recessive in texture and colour.
 - Incorporate simple architectural detailing that does not detract from the heritage buildings and streetscape.
 - Reflect the fine-grain pattern of subdivision of the streetscape.

Street interface

- Provide a minimum of 80% of the building façade at ground floor level with a visually permeable façade incorporating windows and door openings with clear glazing to ‘Key Public Realm Areas’.
- Buildings on corner sites should be designed to actively address both frontages at street level.
- Upper levels of buildings should be designed to provide habitable rooms with windows or balconies that overlook the public realm.
- Buildings fronting laneways should be designed for passive surveillance with a permeable façade, including windows and door openings.
- Ensure car parking is not visible from the street by incorporating design measures such as sleeving, or alternatively providing as basement parking.

Residential Interface

- Development should provide upper level setbacks in accordance with the precinct specific setbacks (refer to sections 4.3 to 4.5).
- Development must satisfy the objectives and standards of Clauses, 54,55 or 58, which aim to protect the amenity of adjoining residential areas.

Weather protection

- Provide verandahs on all buildings located in the ‘Weather Protection Areas’.
- Verandahs should be at an appropriate height above the footpath to avoid damage whilst still providing effective weather protection, generally between 3.0 and 4.0m and consistent with adjoining sites.
- Verandahs should be designed to mitigate the potential for visual clutter effects from light fittings, service cables and under awning signage.

Access and services

- Pedestrian entries to buildings should be clearly visible and easily identifiable from the street and accessible for all abilities.
- Residential entries should be distinguished from retail and commercial entries.
- Loading, service access and car park access should be provided from laneways and secondary streets. Where this not possible, vehicle crossovers should be minimised to reduce disruption to the footpaths and active frontages and located to avoid street trees if present.
- Provide appropriate setbacks at the rear of the building to laneways ensure adequate space for car park access and servicing. Further details at Clause 52.06 of the Glen Eira Planning Scheme.
- Screen air conditioning services, antennas and other utilities from public view using balcony treatments / roof structures / architectural elements. Avoid using walls to screen services.
- Avoid and minimise building services and utilities at ground floor street frontages to prioritise active frontages at these locations. Integrate services and utilities with the building design.
- Waste storage, loading and recycling facilities should be screened from public view. They should be easily accessed by residents and well ventilated.

Landscaping

- Communal garden spaces should be provided at podium and rooftop levels where appropriate to create amenity for residents, workers and visitors. The gardens should take into consideration, aspect, materials and solar orientation.
- Large development sites are encouraged to contribute to improved pedestrian amenity of the centre by providing for new private/public pedestrian/cycling links through the centre, where sites present an appropriate and useful opportunity to do so.

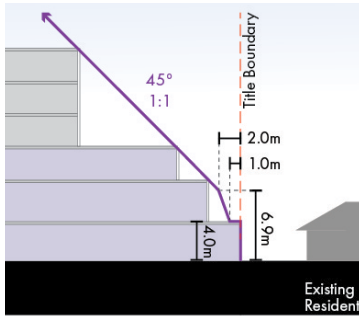
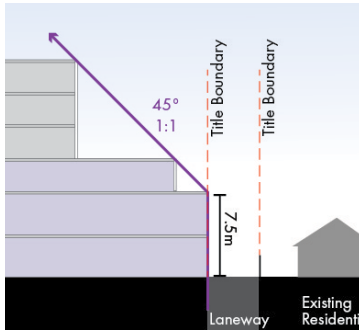
4.3 PRECINCT 1 - EASTERN JUNCTION

4.3.1 PRECINCT OVERVIEW

The Eastern Junction will provide for a high quality entry into the Bentleigh East NAC with development of up to six storeys marking the key corners.

Larger sites will support the transformation of this precinct providing substantial redevelopment opportunities.

4.3.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 6 storeys (21.0m)	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m)	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire eastern footpath of East Boundary Road from 12pm - 2pm on September 22Ensure solar access is maintained to the entire western footpath of East Boundary Road from 10am - 12pm on September 22Ensure solar access is maintained to the entire southern footpath of Centre Road from 10am on September 22 Activation <ul style="list-style-type: none">Development at 2-8 and 10-16 Heather Street should provide ground level activation to the existing laneway
<div><div></div><div></div></div>			

4.3.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 1 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.1:

- The southern footpath along Centre Road and the Eastern and Western Footpaths of East Boundary Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to footpaths during the specified periods. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired mid-rise scale of the centre will be achieved with a maximum building height of 6 storeys. The proposed heights in this precinct will help to emphasise this important junction at the eastern entry into the NAC. The sites are generally large and have limited residential interfaces, which would allow the heights to be achieved with minimal amenity impacts. The proposed height also reflects the heights of recent development on the eastern side of East Boundary Road. The long cross section shown in Figure 22 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

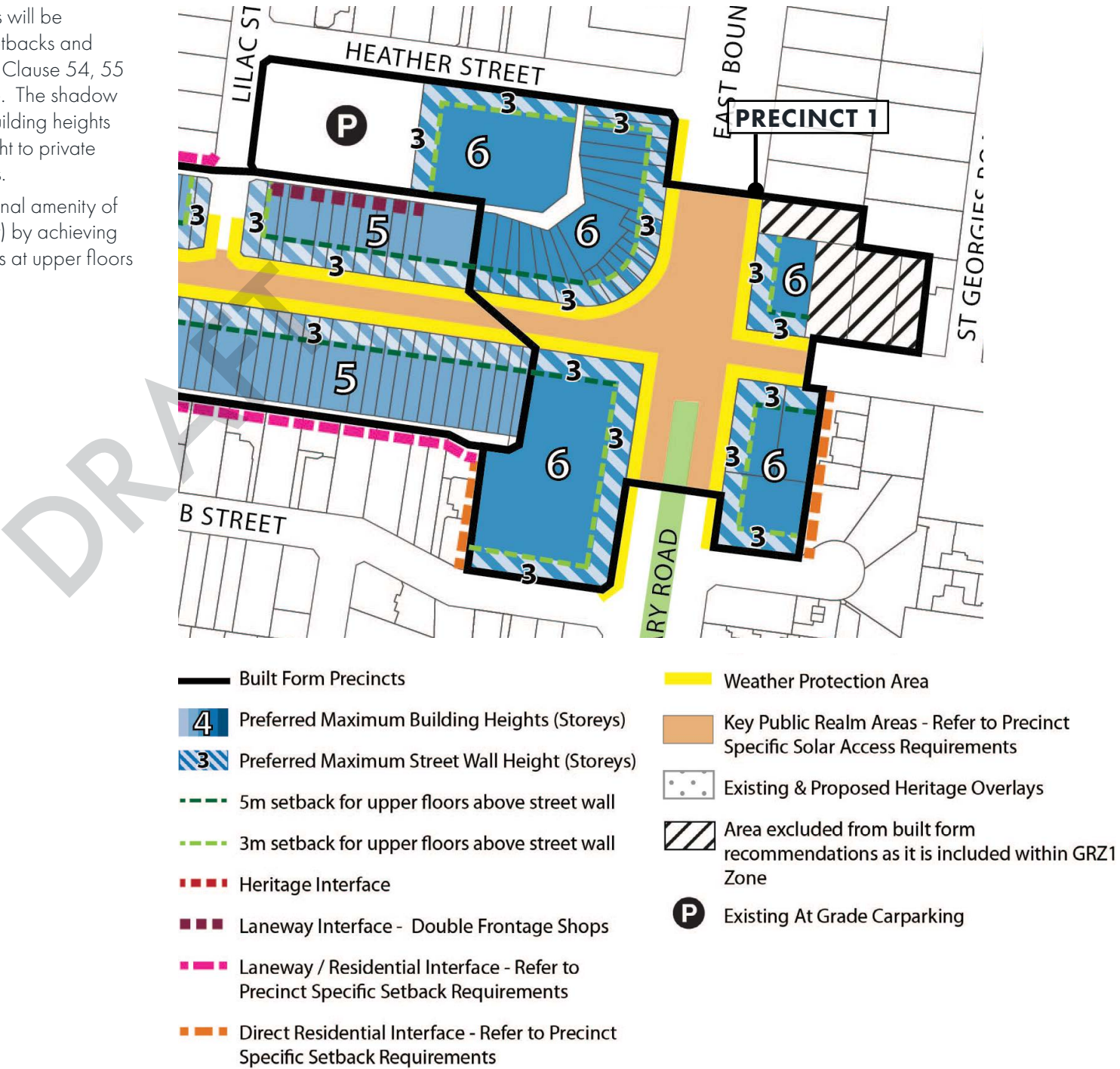


Figure 20. Built Form Framework - Precinct 1

4.3.4 CROSS SECTIONS

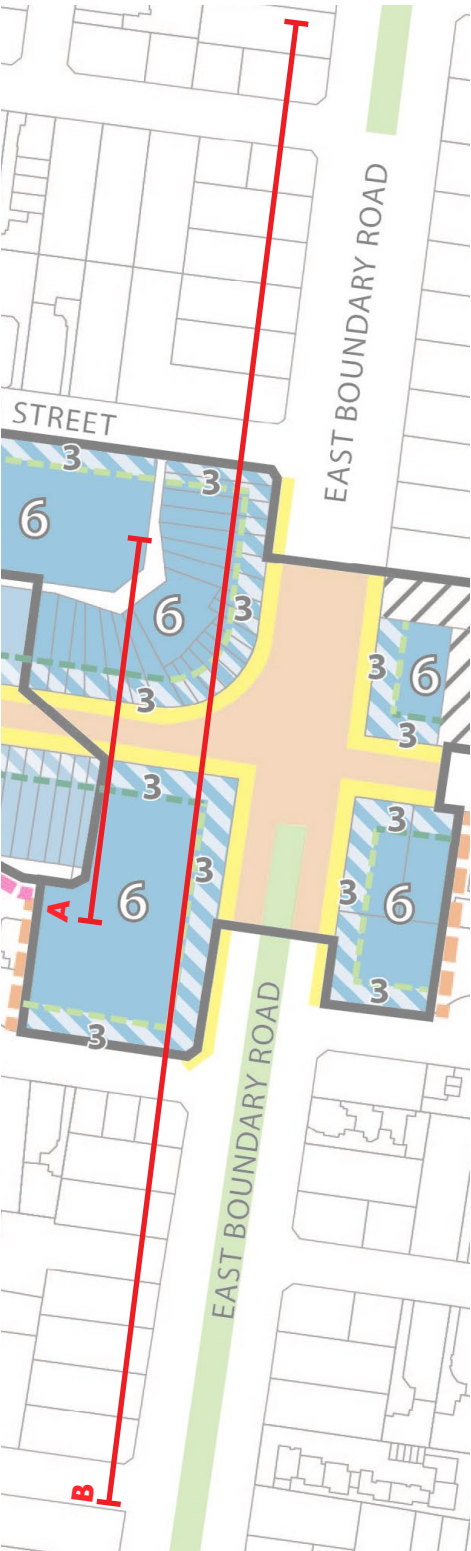


Figure 23. Precinct 1 - Cross Section Key Plan

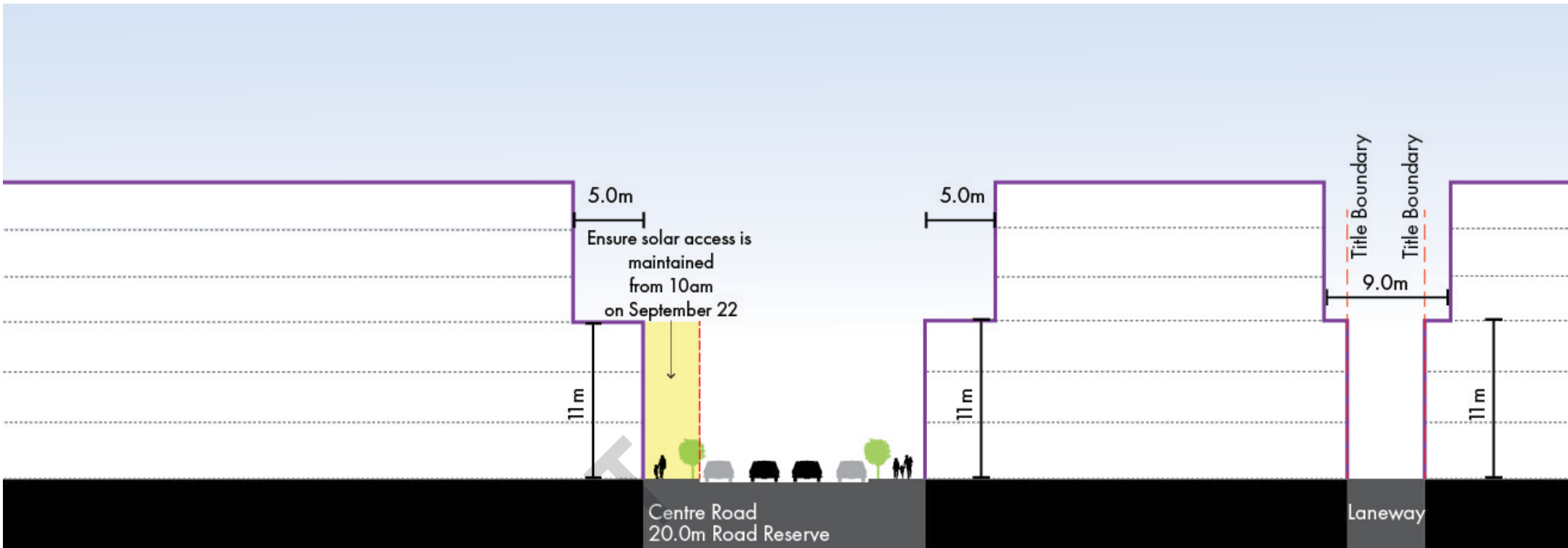


Figure 21. Precinct 1 - Cross Section A

- Maximum Building Envelope
- Potential building envelope which could be achieved under the Built Form Framework - Not illustrative of a proposed development outcome or design
- Maintain Solar Access to footpath

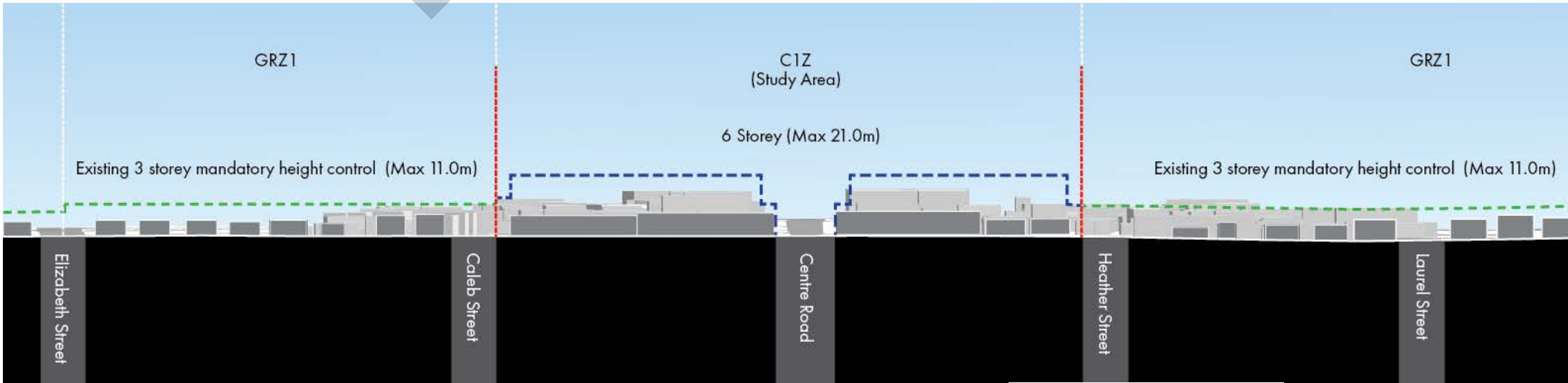


Figure 22. Precinct 1 - Cross Section B

- Study Area Boundary
- Existing Maximum Built Form Height
- Planning Scheme Zoning Boundary
- Proposed Maximum Built Form Height
- Existing Built Form

4.3.5 SKETCH VISUALISATION



Figure 24. Viewpoint 1 - Existing Conditions

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

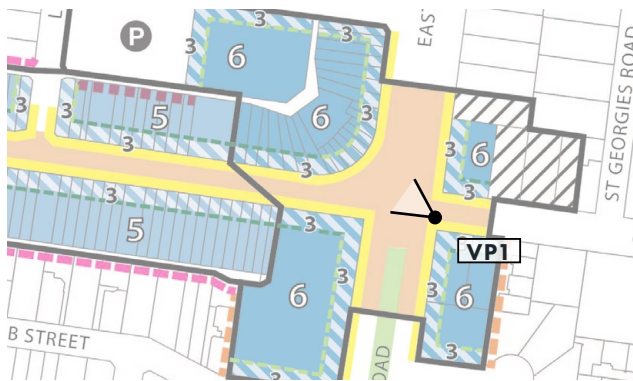


Figure 25. Viewpoint 1 Location Plan



Figure 26. Viewpoint 1 - With examples of supported street wall and building height shown

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows prominent built form to the corner of Centre Road and East Boundary Road. The three storey street wall wraps the curved corner and extends along Centre Road integrating with a number of two storey shopfronts.

Development of five storeys in Precinct 2 is also visible from this view.

4.3.6 PRECINCT 1 - SHADOW ANALYSIS

Figure 27 demonstrates the shadows cast by existing buildings within Precinct 1 of the Bentleigh East NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



10am - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 27. Precinct 1 - Shadow Analysis

11am - Sept 22



12pm - Sept 22



1pm - Sept 22



2pm - Sept 22





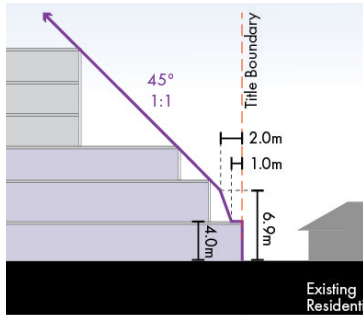
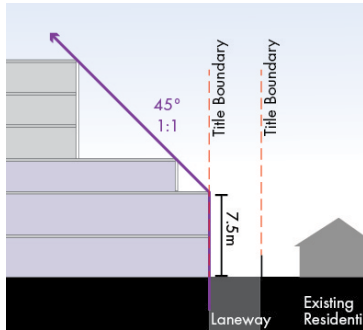
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4.4 PRECINCT 2 - CENTRE ROAD RETAIL

4.4.1 PRECINCT OVERVIEW

The Centre Road Retail precinct will continue to transform with development of up to five storeys. This scale allows for a sensitive transition to adjoining residential areas in the south.

4.4.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 5 storeys (18.0m)	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m)	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Centre Road from 10am on September 22 Activation <ul style="list-style-type: none">Development at 2-8 and 10-16 Heather Street should provide ground level activation to the existing laneway Heritage <ul style="list-style-type: none">Development on land at 675-677 Centre Road should respect the heritage protected East Bentleigh Hall
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below: 	

4.4.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 2 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.1:

- The southern footpath along Centre Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to footpath from 10am on September 22. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired mid rise scale of the centre will be achieved with a maximum building height of 5 storeys. This height supports additional intensification and development in the centre which is consistent with the planning policy context outlined in Part 1 of this report. It also reflects the height of recent development within the Precinct. The proposed height is slightly lower than the proposed heights for Precinct 1, which is largely due to the smaller sites. Setbacks of 5m for development above 3 storeys will ensure the street wall is dominant in the streetscape when viewed from the opposite footpath. The long cross section shown in Figure 32 and 33 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

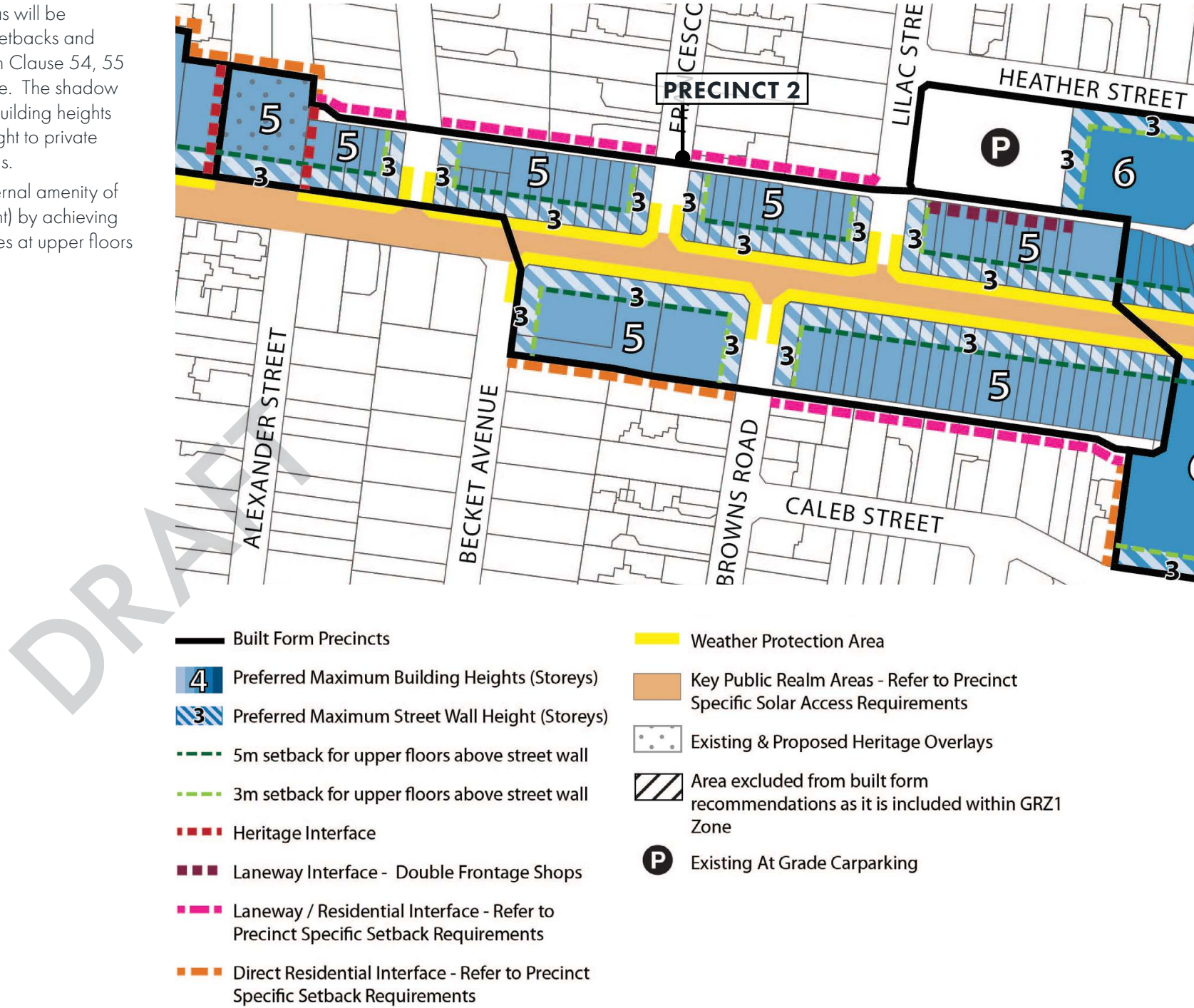


Figure 28. Built Form Framework - Precinct 2

4.4.4 CROSS SECTIONS

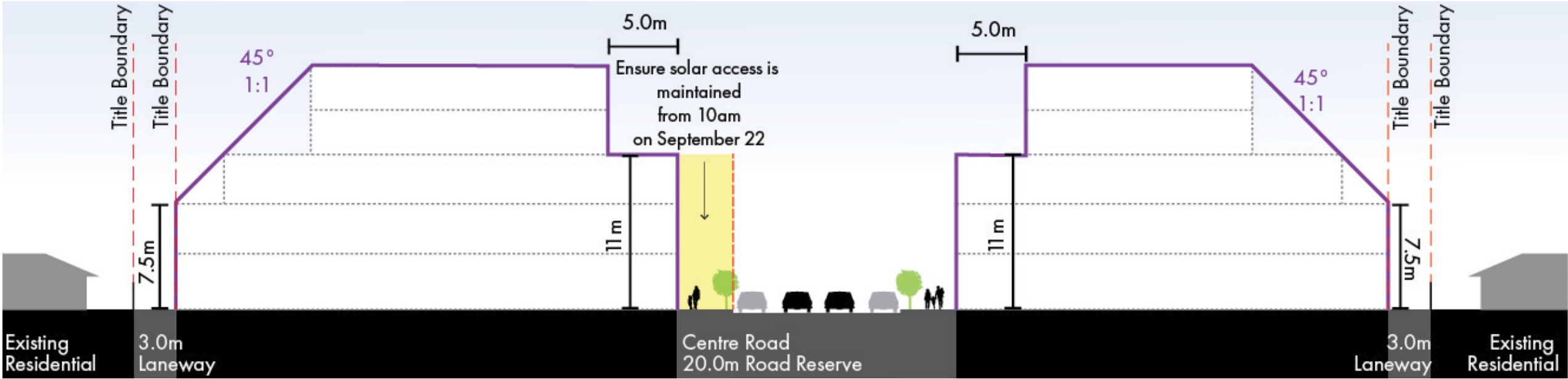


Figure 29. Precinct 2 - Cross Section A

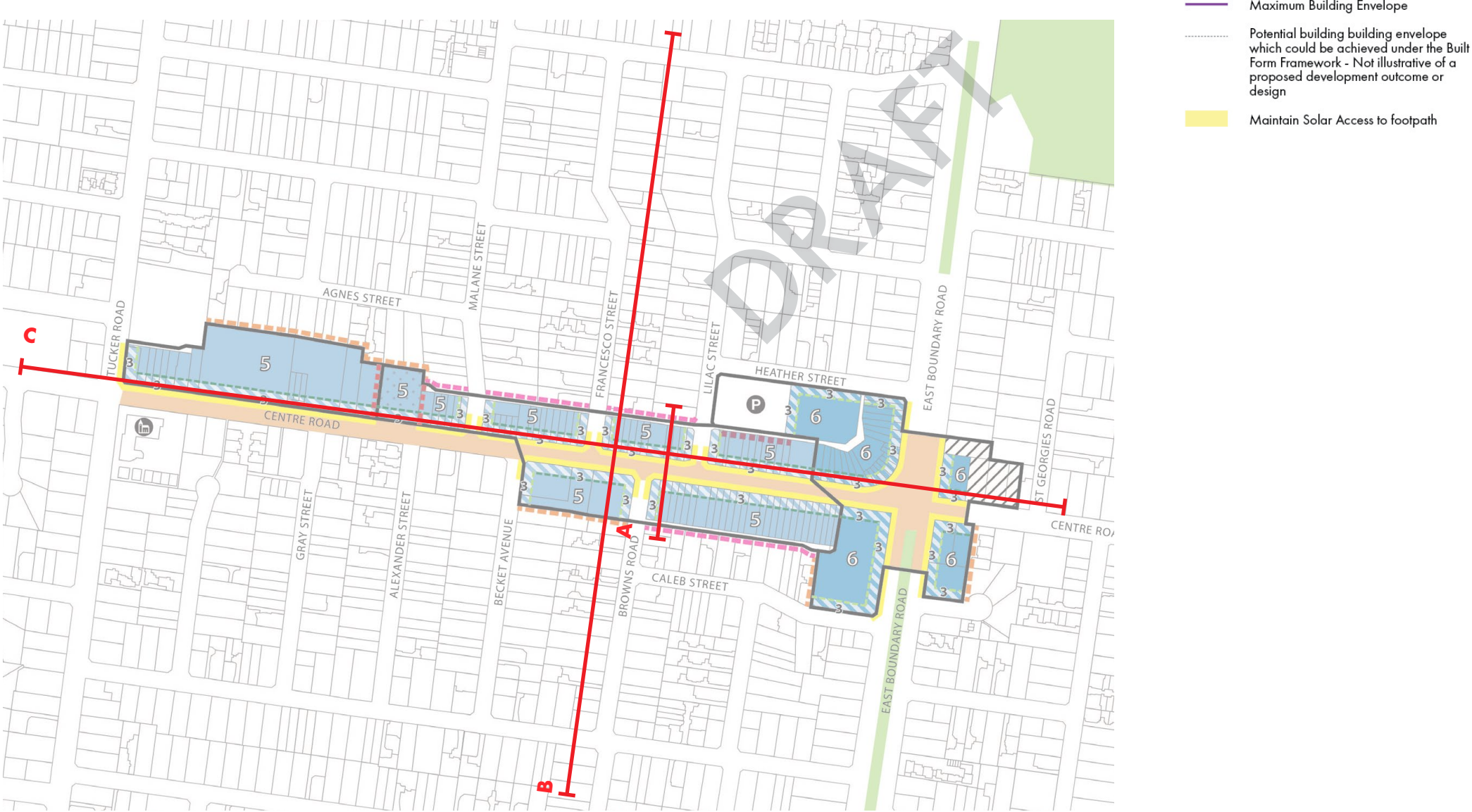


Figure 30. Precinct 1 - Cross Section Key Plan

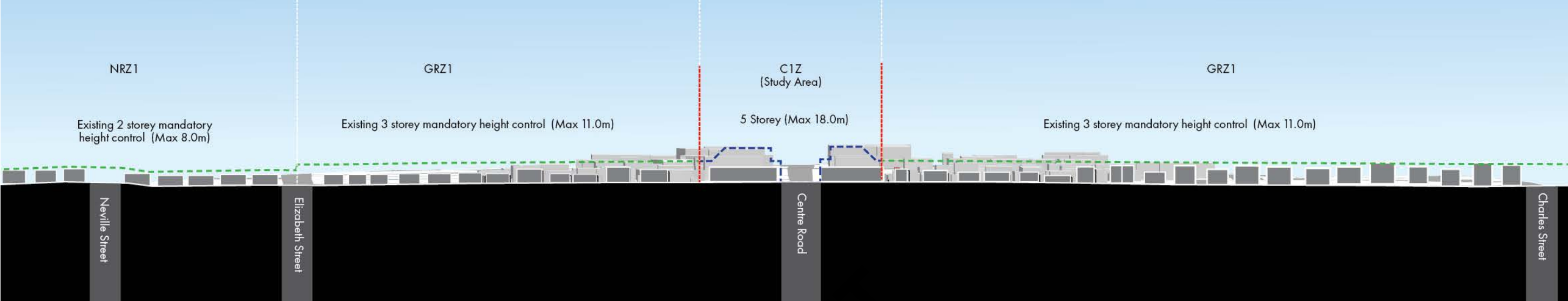


Figure 32. Precinct 2 - Cross Section B

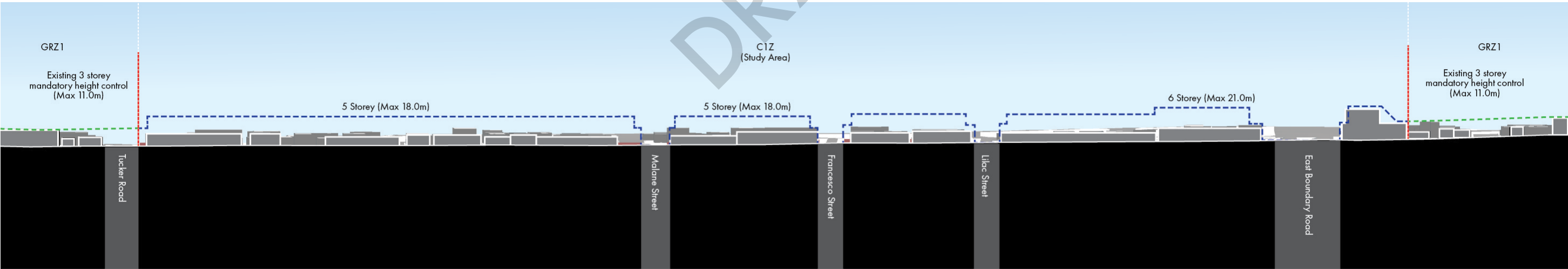


Figure 33. Precinct 2 - Cross Section C

- Study Area Boundary
- Existing Maximum Built Form Height
- Planning Scheme Zoning Boundary
- Proposed Maximum Built Form Height
- Existing Built Form

4.4.5 SKETCH VISUALISATION



Figure 34. Viewpoint 2 - Existing Conditions



Figure 35. Viewpoint 2 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

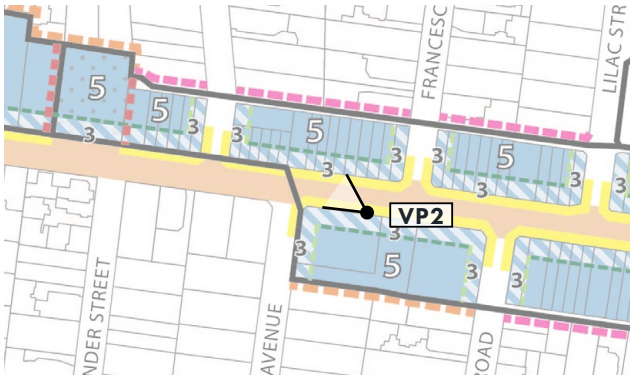


Figure 36. Viewpoint 2 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the single storey street wall will change significantly however the proposed three storey scale will not overwhelm the streetscape. The levels four and five are recessed allowing the street wall to be the dominant element.

4.4.6 PRECINCT 2 - SHADOW ANALYSIS

Figure 37 demonstrates the shadows cast by existing buildings within the Precinct 2 of the Bentleigh East NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



10am - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 37. Precinct 2 - Shadow Analysis

11am - Sept 22



12pm - Sept 22



1pm - Sept 22



2pm - Sept 22





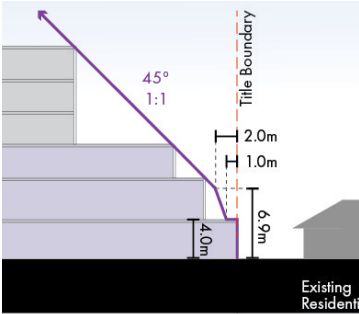
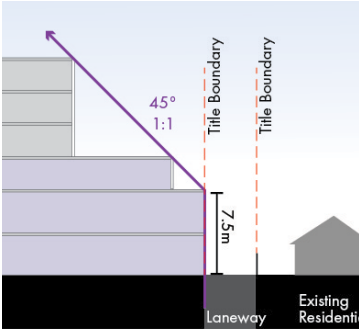
4.5 PRECINCT 3 - CENTRE ROAD WEST

4.5.1 RECOMMENDATIONS

The large sites within the peripheral retail precinct will support development of up to six storeys and provide an opportunity to create a continuous retail experience along Centre Road.

Properties at the western end of the strip will support development of up to five storeys and mark the entry to the NAC with high quality buildings.

4.5.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 5 storeys (18.0m)	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m)	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Centre Road from 10am on September 22 Heritage <ul style="list-style-type: none">Development on land at 669-673 Centre Road should respect the heritage protected East Bentleigh Hall
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below: 	

4.5.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 3 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.1:

- The southern footpath along Centre Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to footpath from 10am on September 22. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired mid rise scale of the centre will be achieved with maximum building heights of 5 storeys. These height supports additional intensification and development in the centre which is consistent with the planning policy context outlined in Part 1 of this report. Residential areas are also located to the north which means there will be no shadow impacts from new development. Setbacks of 5m for development above 3 storeys will ensure the street wall is dominant in the streetscape when viewed from the opposite footpath. The long cross section shown in Figure 33 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.
- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55

- and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

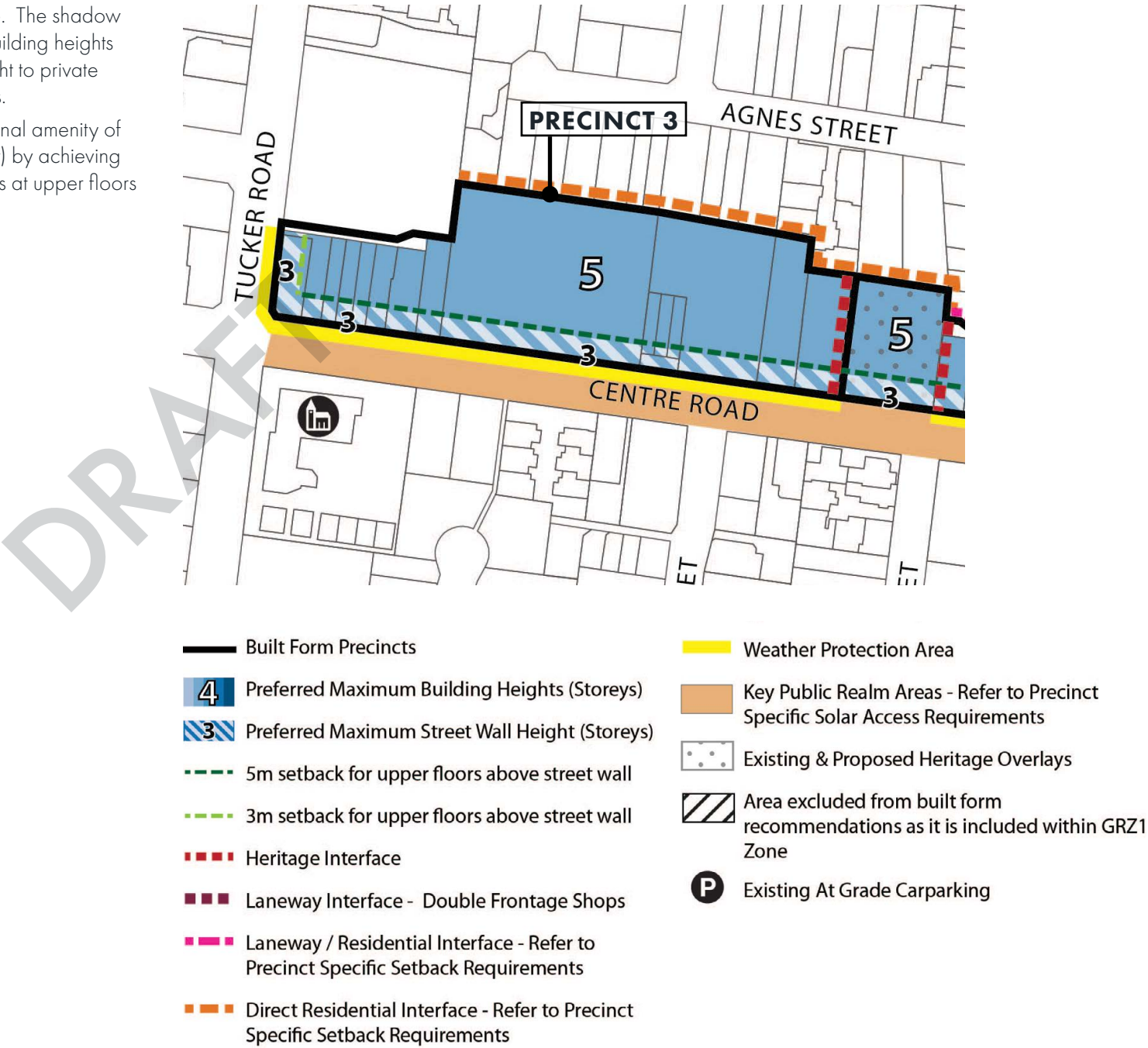


Figure 39. Built Form Framework - Precinct 3

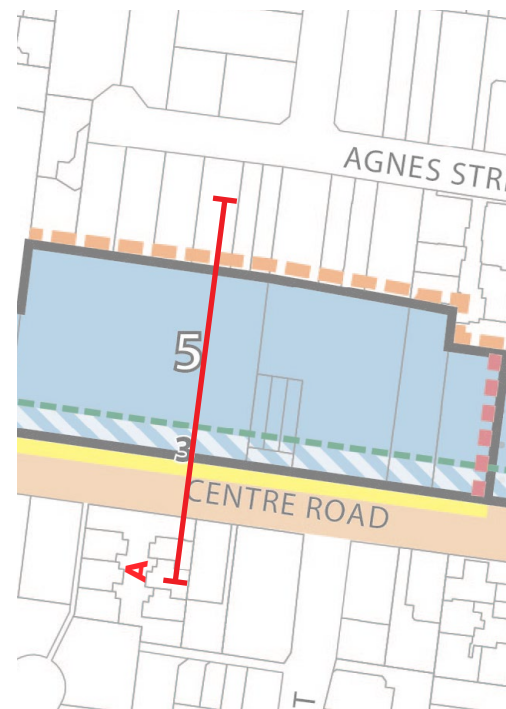


Figure 40. Precinct 3 - Cross Section Key Plan

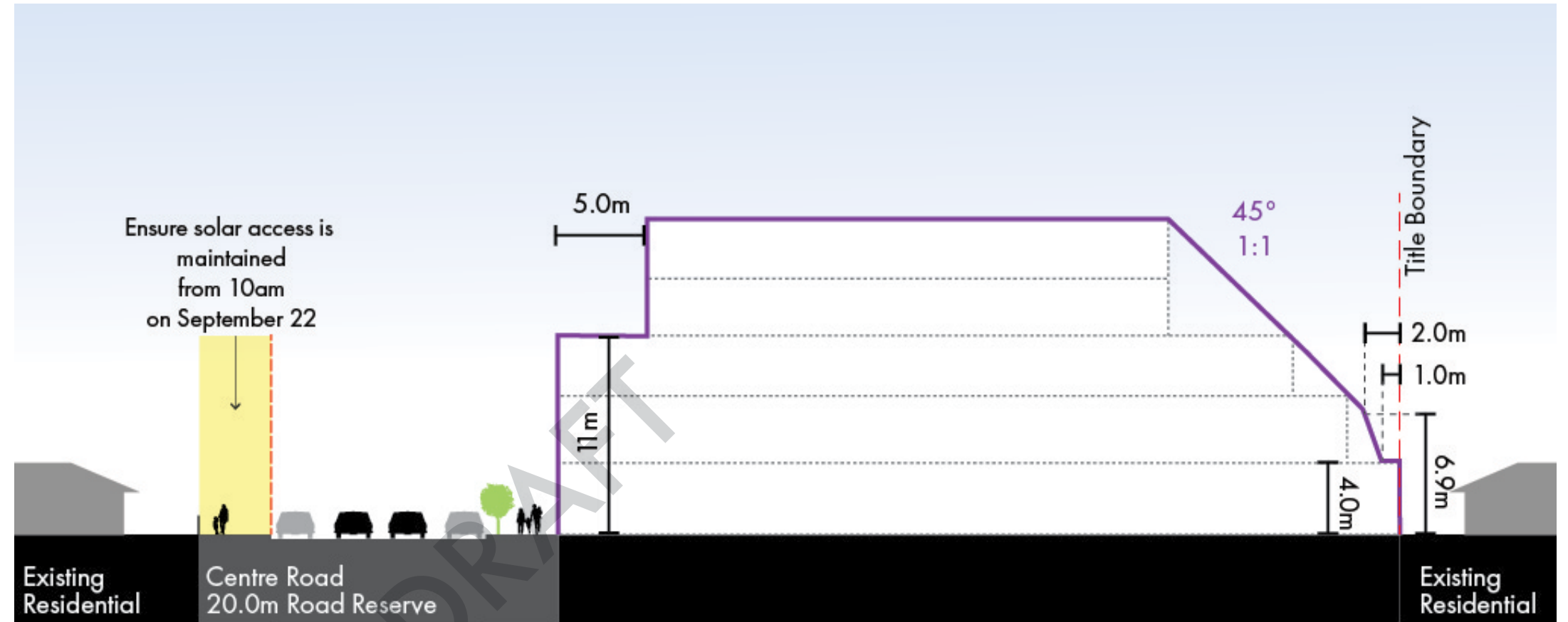


Figure 41. Precinct 3 - Cross Section A

- Maximum Building Envelope
- Potential building building envelope which could be achieved under the Built Form Framework - Not illustrative of a proposed development outcome or design
- Maintain Solar Access to footpath

4.5.5 SKETCH VISUALISATION



Figure 42. Viewpoint 3 - Existing Conditions



Figure 43. Viewpoint 3 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

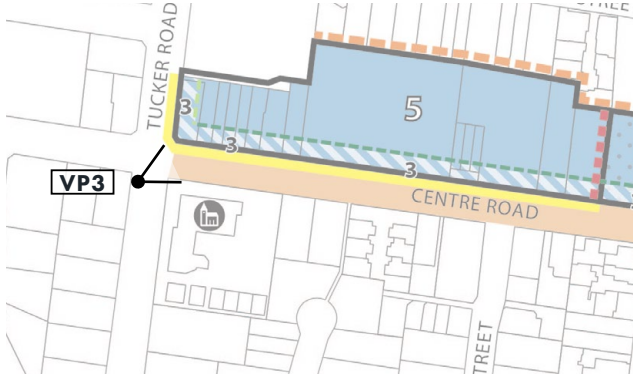


Figure 44. Viewpoint 3 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the proposed three storey street wall creates a more cohesive streetscape. The additional upper levels are recessed and allow the street wall to be the dominant element.

4.5.6 PRECINCT 3 - SHADOW ANALYSIS

Figure 45 demonstrates the shadows cast by existing buildings within Precinct 3 of the Bentleigh East NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



11am - Sept 22



10am - Sept 22



12pm - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 45. Precinct 3 - Shadow Analysis

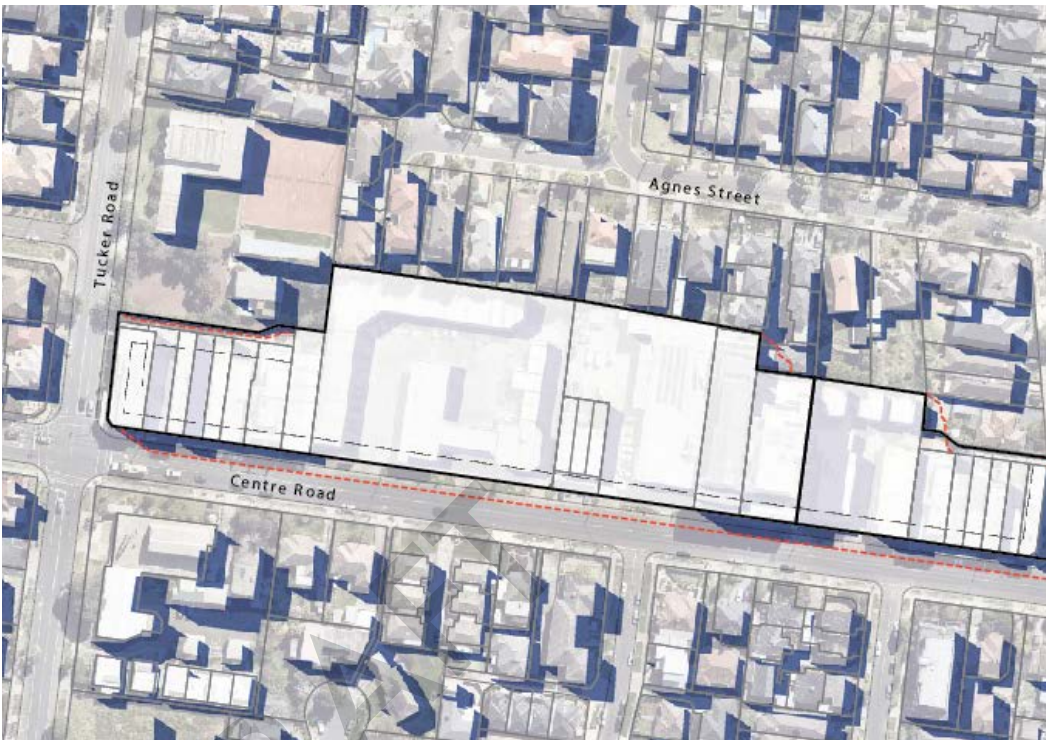
1pm - Sept 22



2pm - Sept 22



3pm - Sept 22



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BUILT FORM FRAMEWORK

CAULFIELD PARK NEIGHBOURHOOD ACTIVITY CENTRE



GLEN EIRA
CITY COUNCIL

This information sheet aims to explain more about:

- what built form frameworks are;
- why Council is preparing them for three of our commercial areas (Neighbourhood Activity Centres);
- the steps involved; and
- the opportunities you have to provide input.

What are built form frameworks?

A built form framework establishes recommendations to manage building heights and setbacks in an area. It analyses the existing buildings and takes into account their size and position, the topography of the land, how buildings are used and their relationship with other buildings. It also identifies the special elements that the community values such as public spaces and historic buildings. This analysis establishes the context for the recommendations.

Why do we need built form frameworks?

Currently Glen Eira City Council does not have specific guidelines for building design in these three areas that are under development pressure. Our community has told us that it is important that we set clear development expectations. Built form frameworks provide the analysis required to substantiate new planning controls for an area.

Through the preparation of frameworks, we have an opportunity to work towards new planning controls for three of our popular and growing Neighbourhood Activity Centres:

- Caulfield Park (Hawthorn and Balaclava Roads);
- Caulfield South (Hawthorn and Glenhuntly Roads); and
- Bentleigh East (Centre and East Boundary Roads).

This work will help to:

- provide clarity to landowners, businesses and the community about the acceptable height and setbacks of future buildings;
- identify and retain the local sense of identity in these centres;
- strengthen the vitality and economic viability of the centres; and
- ensure these Centres continue to be enjoyable places to shop, work and gather.

How are built form frameworks different from structure plans?

There are many different types of analysis that can be used to support the development of planning controls. Structure Plans are undertaken in large activity centres. They cover issues relating to height and setbacks, but also extend to more complex matters such as traffic, access, car parking, open space and community infrastructure.

Built form frameworks are more appropriate for our smaller centres, where the priority is to respond to development pressure by establishing robust planning controls to guide matters relating to height and setbacks.

What are the steps involved?

There are two stages in the process. Stage one is the development of the controls. Stage two is embedding those controls into the *Glen Eira Planning Scheme*.

Each stage and the keys steps involved in this project is set out below. We are just starting stage one:

Stage one

1. Prepare draft built form frameworks
- 2. Seek feedback from the community on the draft built form frameworks**
3. Council reviews the feedback and makes a decision on the frameworks

If Council adopt the frameworks, the second stage involves translating the recommendations into planning controls for the *Glen Eira Planning Scheme*. This follows a legal process, including the following steps:

Stage two

1. Prepare draft planning controls and policies based on the recommendation in the frameworks
2. Request the Minister for Planning to authorise Council to prepare and exhibit a planning scheme amendment
- 3. Seek feedback from the community in a process known as public exhibition**
4. Hold a planning conference, where Council can hear from anyone how has provided feedback
5. Council to consider all submissions and whether changes can be made to address all submissions
- 6. An independent panel hearing will be held if changes cannot be made to address all submissions. Anyone who has provided feedback is invited to present at the panel hearing**
7. Council to consider the panel recommendations and decides on the outcome of the amendment

Moving from framework recommendations to planning controls

For recommendations made by the frameworks to be successfully translated into planning controls and included in the *Scheme*, we have to ensure that the recommendations reflect local and state policy.

This means that the frameworks must:

- plan for current and future communities;
- meet State Government guidance for the introduction of planning controls in activity centres (this includes guidance about where it is and is not appropriate to use mandatory controls); and
- communicate a vision for future built form outcomes that is informed by:
 - o a thorough built form and character review of the centres; and
 - o views from the community about what people value about these centres.

How can you provide input?

The opportunities to provide your feedback on this project are highlighted in bold in the steps above. The release of draft built form frameworks is expected in the next few weeks. You can register for updates on this project, or find more information at **haveyoursaygleneira.com.au**

It is recorded that Cr Zyngier declared a General Conflict of Interest given he has had one of the objectors to his home recently.

It is recorded that Cr Zyngier vacated the Chamber at 8:37pm prior to any discussion on this matter.

8.6 POST-WAR AND HIDDEN GEMS HERITAGE REVIEW AND THEMATIC ENVIRONMENTAL HISTORY (AMENDMENT C214) - CONSIDER SUBMISSIONS

Moved: Cr Zmood

Seconded: Cr Parasol

That Council:

1. considers the submissions received, including late submissions received up until 10 March 2021;
2. requests the Minister for Planning to appoint a Planning Panel to consider submissions received during the exhibition period for Amendment C214 and the late submissions received until 10 March 2021 for Amendment C214 — in accordance with section 23 of the Planning and Environment Act 1987; and
3. for the purpose of advocacy before a planning panel, note Council's intention of abandoning the application of the heritage overlay to the following places and precincts:
 - a) 195-197 Hawthorn Road, Caulfield North
 - b) 58 Norwood Road, Caulfield North
 - c) 53 Balaclava Road, Caulfield North
 - d) 124 Balaclava Road, Caulfield North
 - e) 61-63 Gordon Street, Elsternwick
 - f) 40 Lumeah Road, Caulfield North
 - g) 44 Murrumbeena Road, Murrumbeena
 - h) Wimbledon Estate Precinct, St Kilda East
 - i) Grimwade Court Precinct, Caulfield North
 - j) 335 Alma Road, Caulfield North
 - k) 30 Aroona Road, Caulfield North
 - l) 64 Balaclava Road, Caulfield North
 - m) 624 Centre Road, Bentleigh East
 - n) 30 Griffiths Street, Caulfield South
 - o) 82 Lumeah Road, Caulfield North
 - p) 49 Rosemont Avenue, Caulfield North
 - q) Findon Avenue Precinct, Caulfield North

FOR: Crs Esakoff, Cade, Parasol and Zmood (4)

AGAINST: Crs Athanasopoulos, Magee, Pilling and Zhang (4)

The Mayor had the casting vote and voted in favour of the Motion before Council.

CARRIED

It is recorded that Cr Zyngier vacated the Chamber prior to Item 8.6 and was not present when this item was considered. It is recorded that Cr Zyngier entered the Chamber at 9:03pm.

8.7 SAFE CYCLING CORRIDOR - INKERMANN ROAD

Moved: Cr Athanasopoulos**Seconded: Cr Zyngier**

That Council:

1. proceeds with the detailed design for the Inkermann Road Safe Cycling Corridor project as outlined in the 17 December 2019 Council resolution;
2. requests officers consult with the Community Reference Group and Technical Reference Group to assist in developing a preferred corridor design;
3. requests Officers explore how best to address community concerns regarding parking, safety, accessibility and amenity where possible through the detailed design process;
4. requests the tabling of a further report on the preferred corridor design for Council consideration, including indicative costings and how community concerns were able to be addressed through the design process. No construction will occur without a further resolution of the Council.

Procedural Motion**Moved: Cr Magee****Seconded: Cr Zyngier**

That Council grants Cr Athanasopoulos a 2 minute extension of speaking time.

CARRIED UNANIMOUSLY**FOR:** Crs Athanasopoulos, Magee, Pilling, Zhang and Zyngier (5)**AGAINST:** Crs Esakoff, Cade, Parasol and Zmood (4)**CARRIED**

8.8 PARKLET PROGRAM UPDATE

Moved: Cr Pilling**Seconded: Cr Athanasopoulos**

That Council:

1. trials leaving the parklets throughout Winter 2021 in Bentleigh only;
2. reinstates the parklet program across more activity centres from Friday 1 October 2021 through to Thursday 31 March 2022 through a new application process; and
3. subject to support for items 1 and 2, receives a report at a Council meeting after March 2022 on whether or not to continue the parklet program.

*It is recorded that Cr Esakoff vacated the Chamber at 9:42pm.**It is further recorded that the Deputy Mayor, Cr Magee took the Chair for consideration of this item.***CARRIED UNANIMOUSLY****Procedural Motion****Moved: Cr Athanasopoulos****Seconded: Cr Pilling**

That Council extends the meeting to conclude at 11pm.

CARRIED UNANIMOUSLY*It is recorded that Cr Esakoff entered the Chamber at 9:45pm and resumed as Chair of the meeting.*

8.9 REINTRODUCTION OF SPECIAL RATE SCHEME - BENTLEIGH SHOPPING CENTRE

Moved: Cr Zhang**Seconded: Cr Magee***It is recorded that Cr Zmood vacated the Chamber at 9:46pm.**It is recorded that Cr Pilling vacated the Chamber at 9:46pm.*

That Council:

1. gives notice of its intention to declare a new Special Rate on properties in the Bentleigh Shopping Centre for a period of seven years from 1 July 2021 to 30 June 2028 (in the form of the declaration contained in Attachment 1);
2. authorises the Chief Executive Officer (CEO) to give public notice in relevant publications and Council's website of its intention to declare a new Special Rate;
3. authorises the CEO to send a copy of the public notice to each person who will be liable to pay the Special Rate;
4. specifies the following for the purpose of sections 163(2), 163(2A) and 163(2B) of the Act:
 - (a) The total amount of the Special Rate proposed to be levied in accordance with section 163(2) of the Act is:
 - (i) for the first year of the Scheme \$185,700; and
 - (ii) for each subsequent year the Scheme remains in force — the previous year's amount to be levied increased by the Consumer Price Index (CPI) rate provided by the Australian Bureau of Statistics.
 - (b) The total amount of the Special Rate which may be levied is not to exceed the following which is calculated in accordance with section 163(2A) of the Act:
 - (i) for the first year of the Scheme \$185,700; and
 - (ii) for each subsequent year the Scheme remains in force — the previous year's amount to be levied increased by the Consumer Price Index (CPI) rate provided by the Australian Bureau of Statistics.
 - (c) For the purposes of section 163(2B) above:
 - (i) the 'benefit ratio' (R) to be levied on liable persons is 100 per cent;
 - (ii) there are no commercial properties receiving a special benefit from the Special Rate which are not to be levied the rate. (Properties that are deemed to be non-rateable under the Act will not be levied the rate); and
 - (iii) the 'community benefit' from the Special Rate is zero.
 - (d) The criteria to be used in accordance with section 163(2) as the basis for levying the Special Rate would be:
 - (i) for each year of the Special Rate, each rateable property included in the Special Rate is to pay the applicable rate of cents in the dollar (as determined by Council on 1 July in every year) of the respective property's Net Annual Value.

5. notes that if the Special Rate is to proceed, an agreement between Council and the Traders Association will be established, with a particular focus on aligning efforts with the community vision for Bently — set out in the Glen Eira City Plan and Bently Structure Plan.

It is recorded that Cr Zmood entered the Chamber at 9:48pm.

It is recorded that Cr Pilling entered the Chamber at 9:49pm.

CARRIED UNANIMOUSLY

**8.10 CAULFIELD PARK TENNIS CENTRE - OPERATION, LEASE AND
MANAGEMENT EXPRESSION OF INTEREST OUTCOME**

Moved: Cr Zmood

Seconded: Cr Parasol

That Council:

1. notes the outcome and recommendation of the Expression of Interest process to appoint VicTen21 Pty Ltd (Victorian Tennis Academy) for the ongoing lease, operation and management of Caulfield Park Tennis Centre;
2. approves officers to progress the leasing process as detailed, including authorisation to go to public notice to advertise the lease and invite submissions;
3. notes attachments 2 and 3 to the report remain confidential in accordance with section 3(1)(g) and section 66(5) of the Local Government Act 2020 or until Council resolves otherwise; and
4. authorises Council officers to disclose any confidential information in relation to this item, but only to the extent necessary to give effect to the resolution.

CARRIED UNANIMOUSLY

8.11 ELSTERNWICK CULTURAL PRECINCT - PRECINCT DESIGN

Moved: Cr Athanasopoulos**Seconded: Cr Zyngier**

That Council:

1. endorses the Elsternwick Cultural Precinct Functional Arrangement & Precinct Design (Attachment 2 to this report);
2. support the commencement of further design work to develop a detailed design of the endorsed plan; and
3. communicates Council's decision to residents and businesses in the area.

CARRIED UNANIMOUSLY

8.12 MUNICIPAL ASSOCIATION OF VICTORIA - STATE COUNCIL MOTION MAY 2021

Moved: Cr Pilling**Seconded: Cr Zmood**

That Council notes the following motion and submits it to the 21 May 2021 Municipal Association of Victoria State Council Meeting:

'That the Municipal Association of Victoria advocates to the State Government to reform the regulation of Rooming Houses to improve affordability and arrangements for control of Rooming House standards.'

CARRIED UNANIMOUSLY

8.13 CITIZEN OF THE YEAR AWARDS

Moved: Cr Magee**Seconded: Cr Zmood**

That Council:

1. adopts the minutes of the Citizen of the Year Advisory Committee of 31 March 2021 and adopt the recommendations of this Committee;
2. notes the attachments to the report remain confidential in accordance with section 3(1)(f) and section 66(5) of the Local Government Act 2020 or until Council resolves otherwise; and
3. authorises Council officers to disclose any confidential information in relation to this item, but only to the extent necessary to give effect to the resolution.

CARRIED UNANIMOUSLY

8.14 COMMUNITY SAFETY COMMITTEE TERMS OF REFERENCE

Moved: Cr Magee**Seconded: Cr Cade**

That Council endorses the revised Glen Eira Community Safety Committee Terms of Reference (Attachment 1 to this report).

CARRIED UNANIMOUSLY

9. URGENT BUSINESS - NIL**10. ORDINARY BUSINESS****10.1 Requests for reports from a member of Council staff****10.1.1 Changes to Dwelling Numbers and Locations in Glen Eira****Moved: Cr Zmood****Seconded: Cr Magee**

Requests that officers prepare a report for the Ordinary Council Meeting of 8 June 2021 that outlines the changes in dwelling numbers and types by location throughout Glen Eira from 2017 to 2021.

CARRIED UNANIMOUSLY

10.2 Right of reply - Nil**10.3 Notice of Motion - Nil****10.4 Councillor questions**

Cr Magee asked the following question of Mark Saunders, Director Community Wellbeing.

The community are discussing the COVID-19 vaccination and rollout asking when will we see our residential aged care residents and staff getting the vaccine? Can you describe to us what Council has done in the past to make that happen as quickly as possible?

If not already, when could we see our residential aged care residents, our most vulnerable residents and the staff caring for them getting their COVID-19 vaccination?

Mark Saunders, Director Community Wellbeing responded:

On 22 April, the South East Melbourne Primary Health Network who are organising the vaccinations advised us that they will be visiting Warrawee Nursing Home and Rosstown Community from the week commencing 17 May to administer the first dose of the COVID-19 vaccine.

It is expected that the aged care residents will receive their seasonal flu vaccines by staff on Thursday 29 April before they get the COVID-19 vaccination. There will be a 14 day period in between both vaccinations.

Advice received today, 27 April, is that our residential aged care staff can attend one of the 3 vaccination hubs, Kingston Centre, Moorabbin Hospital or Casey vaccination hub to receive their vaccination. Those over 50 years of age will receive the AstraZeneca vaccine and those under 50 will be able to attend those centres to get the Pfizer vaccine when it becomes available. They will advise us of when those bookings can be made.

Council has advocated to both State and Federal Members of Parliament in terms of getting our residential aged care residents vaccinated as soon as possible and we have been ready with our consents for both residents and staff since the end of February.

10.5 Written public questions to Council

This item was dealt with at an earlier stage of the meeting.

11. CONFIDENTIAL ITEMS**Moved: Cr Athanasopoulos****Seconded: Cr Zmood**

That pursuant to Section 66(1) and 66(2)(a) of the Local Government Act 2020, the Council resolves that so much of this meeting be closed to members of the public, as is required for Council to consider the following matters that are confidential in accordance with Section 3(1) of the Act:

11.1 Consultant Engagement - Open Space

This agenda item is confidential information for the purposes of section 3(1) of the Local Government Act 2020:

- because it is Council business information, being information that would prejudice the Council's position in commercial negotiations if prematurely released (section 3(1)(a)); and
- this ground applies because the agenda item, if prematurely released would diminish the strength of Council's position in commercial negotiations.

CARRIED UNANIMOUSLY

The Mayor advised that after consideration of the confidential item, the meeting will be closed.

11.1 CONSULTANT ENGAGEMENT - OPEN SPACE

Confidential Item

12. CLOSURE OF MEETING

The meeting closed at 10.38pm.

Confirmed this 19 day of May 2021

Chairperson.....