

BENTLEIGH EAST NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK



Prepared for Glen Eira City Council

DRAFT BUILT FORM FRAMEWORK

FEBRUARY 2021

QUALITY ASSURANCE

Glen Eira NAC`s Built Form Frameworks
Bentleigh East NAC Draft Built Form Framework

Prepared for
Glen Eira City Council

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INTRODUCTION

STUDY OVERVIEW

STUDY AREA

DRAFT





PROJECT OVERVIEW

This built form framework for the Bentleigh East Neighbourhood Activity Centre (NAC) is one of three built form frameworks being prepared towards addressing an existing policy void. The other centres forming part of the project are the Caulfield Park Neighbourhood Activity Centre and the Caulfield South Neighbourhood Activity Centre.

The Built Form Frameworks set out preferred development outcomes in each centre. This will ensure new development provides for the continual revitalisation of the shopping strips with high quality design whilst maintaining the valued character of each centre and protecting the amenity of adjoining residential areas.

The Frameworks provide guidance for building heights, setbacks and other design considerations. These frameworks are supported by rigorous analysis and guided by sound urban design principles appropriate to the context of each centre.

This Draft Bentleigh East NAC Built Form Framework consists of the following parts:

Part 1 - Context and Analysis - Provides analysis and background research to understand the NAC, its context and drivers for change.

Part 2 - The Built Form Framework - Provides the guiding principles and built form frameworks including building heights and setbacks.

Proposed planning controls have been developed to reflect the requirements of the Bentleigh East NAC Built Form Framework. The controls are provided in the form of a Design and Development Overlay.

STUDY AREA

Bentleigh East is located approximately 800m east of the Bentleigh Major Activity Centre. It extends for a length of 750m along Centre Road and includes a range of specialty retail and services, and limited hospitality uses. The study area for the Bentleigh East NAC Built Form Framework generally aligns with the existing Commercial 1 Zoning. Refer to Figure 1 - Study Area.

An IGA supermarket anchors the eastern end of the centre and supports surrounding specialty retail. There is less activity at the western end of the centre where retail uses are located on the northern side of Centre Road only. A Smart Bus route is located along Centre Road providing high frequency bus services.

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Figure 1. Study Area Plan

PART 1 - CONTEXT & ANALYSIS

1. CONTEXT

- 1.1 STRATEGIC CONTEXT
- 1.2 STRATEGIC DOCUMENTS
- 1.3 PLANNING POLICY FRAMEWORK
- 1.4 LOCAL PLANNING POLICY FRAMEWORK
- 1.5 PLANNING ZONES
- 1.6 OVERLAYS
- 1.7 RECENT DEVELOPMENT CONTEXT

2. ANALYSIS

- 2.1 TOPOGRAPHY
- 2.2 LAND USE & INTERFACES
- 2.3 BUILT FORM & CHARACTER
- 2.4 DEVELOPMENT ACTIVITY & CAPACITY



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1 CONTEXT

1.1 STRATEGIC CONTEXT

Consistent with sustainable land use and development principles, Neighbourhood Activity Centres are identified in state and local planning policy and strategy as suitable locations for accommodating increased residential density. The policy framework including Plan Melbourne, City Plan and the relevant clauses of the Glen Eira Planning Scheme (State and local) establishes this clear direction.

1.0.1 PLANNING POLICY CONTEXT

Planning policy relating to Bentleigh East Neighbourhood Activity Centre (NAC) identifies the centre as one of 11 NACs in Glen Eira. The higher order centres in Glen Eira are Major Activity Centres, of which there are six. Consistent with sustainable land use principles, NACs in the metropolitan areas are high amenity areas that should accommodate growth to meet the needs of existing and future communities. They differ from the Major Activity Centres identified in Figure 3, which offer a greater amount of transport options, retail and services. As such the level of development and change in the NACs is expected to be less than that experienced in the Major Activity Centres.

The following sections provide a brief outline of this policy context. As noted previously, the centre is largely in the Commercial 1 Zone.

In and around activity centres, a degree of change will occur, consistent with Plan Melbourne, City Plan and the Glen Eira Planning Scheme and commensurate with the level of amenity and accessibility provided by each centre and also taking into account locally-specific amenity and character considerations. Planning controls informed by the built form framework will provide Council with the necessary guidance to assess planning applications and to provide proponents and the community with clear parameters for design in centres.

Council aims to sustainably manage anticipated growth and amenity and to identify and preserve the valued features of the built environment as its activity centres evolve while also protecting the valued amenity of the areas surrounding the centres. For Council to implement built form guidance that is robust and defensible, the guidance must balance the strong

policy objectives of directing more intense development to well-located areas such as neighbourhood activity centres with the need to protect the key elements that make these centres desirable places in their own right. In the Victorian planning system, planning controls that do not achieve a reasonable balance cannot be introduced to the planning scheme.

1.0.2 BUILT FORM FRAMEWORKS IN CONTEXT

Built form frameworks, like structure plans, consider locally-specific elements of a centre such as land use patterns, built form and character, function and amenity and form a vision and built form controls for addition to the planning scheme. Figure 2 provides an overview of where built form frameworks sit in the planning framework relative to built form decisions and residential growth.

1.0.3 PURPOSE OF THE BENTLEIGH EAST NEIGHBOURHOOD ACTIVITY CENTRE BUILT FORM FRAMEWORK

Recent development activity and planning applications in Glen Eira's NACs indicate the need for more detailed and specific built form policy and controls to guide development. While there are existing policies and controls applying to the Bentleigh East NAC they generally consist of:

- High-level strategy and planning policy that supports increased densities in NACs.
- Zoning that predominantly guides land use rather than built form outcomes. The Commercial 1 Zone allows for more intense mixed use development and nearby residential zoning provides for a transition up from the residential hinterland towards activity centres.

This built form framework will provide the locally-relevant analysis of physical characteristics and a vision to inform the drafting of built form controls for the planning scheme and provide that more specific and detailed guidance.

The following issues are addressed by the built form framework:

- Preferred building heights based on built form and shadow analyses.
- Street wall and interfaces
- Building setbacks and modulation – for amenity and visual impact
- Transitions to less intense development

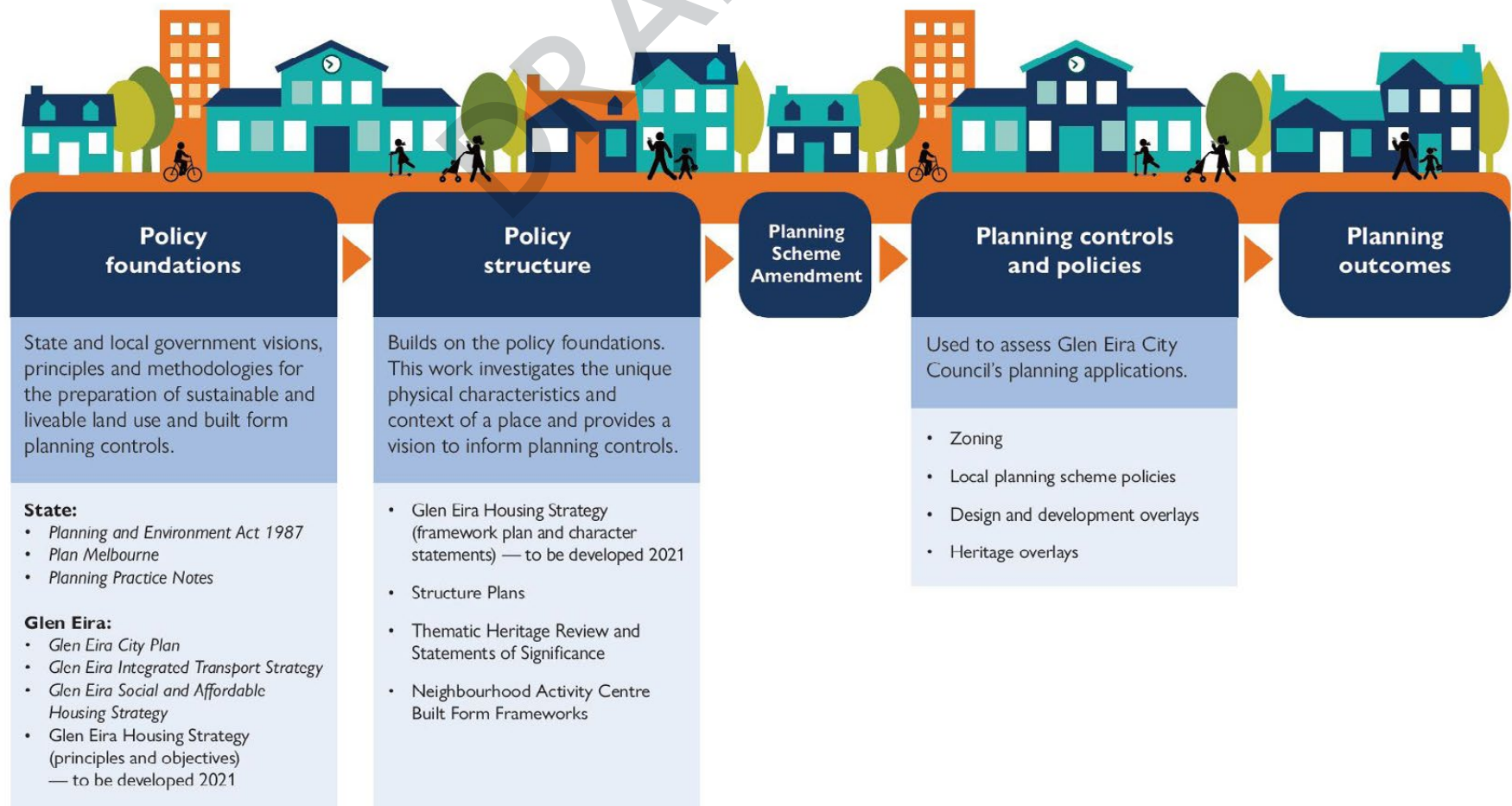


Figure 2. Built Form Framework Planning Context

1.1.1 BENTLEIGH EAST NEIGHBOURHOOD
ACTIVITY CENTRE LOCATION AND PHYSICAL
CONTEXT

The Bentleigh East Neighbourhood Activity Centre is located on Centre Road and East Boundary Road in Bentleigh East. The centre is located in the south-eastern part of the municipality in a predominantly residential context and enjoys access to convenience retail (including an IGA supermarket) and the Smart Bus route along Centre Road. The centre is located approximately 14 km south-east of the Melbourne CBD.

See the Built Form Analysis section of this report for an overview of the existing built form context. The areas immediately surrounding the centre are low-rise neighbourhoods in the General Residential Zone. The General Residential Zone allows for development of up to three storeys.

As mentioned previously, Council’s planning policies identify the centre as one of 11 NACs in Glen Eira, being second in the activity centre hierarchy in terms of suitability for increased density and height due to the level of amenity offered.



Figure 3. Context Plan

1.2 STRATEGIC DOCUMENTS

1.2.1 PLAN MELBOURNE 2017-2050

The updated Plan Melbourne document provides strategic direction and guidance for some of Melbourne’ state significant employment and activity centre clusters, along with housing and transport directions for greater Melbourne. While the document does not provide specific guidance to the Bentleigh East Neighbourhood Activity Centre some of the more relevant directions are:

- Direction 2.1: Manage the supply of new housing in the right locations to meet population growth and create a sustainable city.
- Direction 5.1: Create a city of 20-minute neighbourhoods.

These directions aim to cluster new housing in activity centres and other places that offer good access to jobs, services and public transport and includes a policy for local governments to prepare structure plans for activity centres to accommodate growth.

There are significant opportunities for medium and higher density housing options in locations close to jobs and services including located around Major Activity Centres

The strategic direction closely aligns to the future outcomes envisaged for the NAC. Particularly providing housing opportunities close to transport enabling people to work and live in close proximity.

1.2.2 GLEN EIRA CITY PLAN

The Glen Eira City Plan provides a broad framework to manage growth and change in both housing and employment. Consistent with Plan Melbourne and several existing local planning policies, City Plan aims to direct the majority of new growth into activity centres towards sustainable land use and development outcomes.

To implement the strategic framework (introducing planning controls to the planning scheme), structure planning or built form frameworks need to be prepared for key areas suitable for growth such as major and neighbourhood activity centres and urban renewal areas. These activities produce the built

form, amenity and character material to assist to refine height controls and identify specific ways of managing amenity and character through the planning scheme.

Glen Eira City Plan provides an activity centre hierarchy which identifies Major Activity Centres at the top, followed by Neighbourhood Activity Centres and local centres. The highest levels of growth are to be directed to Major Activity Centres and urban renewal areas. Neighbourhood Activity Centres are identified as suitable for ‘moderate focus for housing growth’.

City Plan provides vision statements for growth locations. The vision for the Bentleigh East NAC is:

- **Bentleigh East** will be a welcoming and connected centre that caters for all. The centre will be enhanced by a range of retail and dining options to meet the everyday needs of its local community.

The Plan also seeks to nominate appropriate heights for the identified housing change areas with the neighbourhood activity centres identified as being suitable for up to between four storeys (for commercial areas with heritage overlays) and five storeys (for commercial areas without heritage overlays).

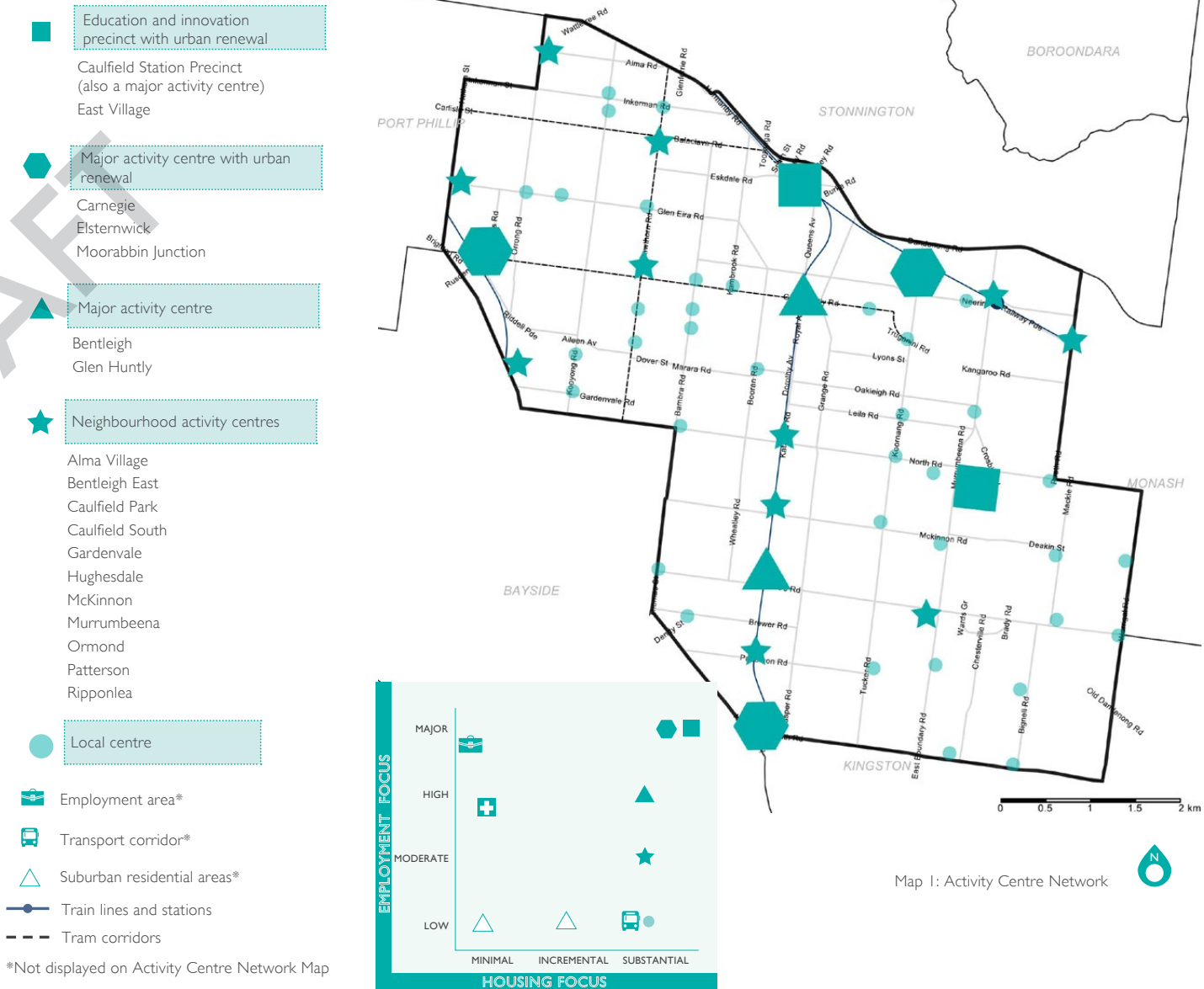


Figure 4. Glen Eira Activity Centre Network Map

1.3 PLANNING POLICY FRAMEWORK

1.3.1 PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following state policies of the Glen Eira Planning Scheme, summarised as follows:

1.3.2 CLAUSE 11 – SETTLEMENT

Seeks to anticipate and respond to the needs of existing and future communities through the provision of zoned and serviced land for housing, employment, recreation and open space, commercial and community facilities and infrastructure.

Clause 11.03-1S (Activity Centres)

Seeks to encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

Clause 11.03-1R (Activity Centres – Metropolitan Melbourne)

Aims to support the development and growth of Metropolitan Activity Centres by encouraging the location of new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.

1.3.3 CLAUSE 15 – BUILT ENVIRONMENT

Seeks to ensure that all new land uses and development appropriately responds to its landscape, valued built form and cultural context, and protect places and sites with significant heritage, architectural, aesthetic, scientific and cultural value.

Clause 15.01-1S (Urban design)

Aims to create urban environment that are safe, functional and provide good quality environments with a sense of place and cultural identity.

Clause 15.01-1R (Urban design – Metropolitan Melbourne)

Aims to create a distinctive and liveable city with quality design and amenity by supporting the creation of well-designed places that are memorable, distinctive and liveable.

Clause 15.02-1S (Energy and resource efficiency)

Aims to encourage land use and development that is energy and resource efficient, supports a cooler environment and minimises greenhouse gas emissions through strategies such as greening urban areas, buildings, transport corridors and open spaces with vegetation to reduce urban heat island effects.

Clause 15.03-1S (Heritage conservation)

Seeks to ensure the conservation of places of heritage significance by retaining elements that contribute to the importance of the heritage place.

1.3.4 CLAUSE 16 – HOUSING

Seeks to provide for housing diversity and ensure the efficient provision of supporting infrastructure. However, the quantum of development intensification remains relatively open, with appropriate controls to be defined through local planning based on local context.

Clause 16.01-1S (Housing supply)

Seeks to facilitate well-located, integrated and diverse housing that meets community needs through measures such as increasing the proportion of housing in designated locations in established urban areas (including under-utilised urban land) and reduce the share of new dwellings in greenfield, fringe and dispersed development areas and encouraging higher density housing development on sites that are well located in relation to jobs, services and public transport.

Clause 16.01-1R (Housing Supply – Metropolitan Melbourne)

Aims to manage the supply of new housing to meet population growth and create a sustainable city by developing housing and mixed use development opportunities in established areas. Those locations include neighbourhood activity centres - especially those with good public transport connections.

1.4 LOCAL PLANNING POLICY FRAMEWORK

This built form framework has been prepared to contribute towards achieving the objectives and strategies of the following local policies of the Glen Eira Planning Scheme, summarised as follows:

Please note that while this content is correct at the time of writing, Council is in the process of updating its local planning policy. While there will be changes to specific policies it is not expected that the general intent of policy relating to NACs will change.

1.5.1 CLAUSE 21.04 – HOUSING AND RESIDENTIAL DEVELOPMENT

This policy provides objectives and strategies to guide housing and residential development in Glen Eira. The following policies are considered relevant to this built form framework:

Objective 4

- To stimulate and improve the vitality of Glen Eira’s commercial centres.

Strategies

- Encourage a mix of housing types, increased residential densities and mixed use developments within urban villages and neighbourhood centres.
- Encourage residential development, where considered appropriate, subject to consultation with traders and the wider community, above or below existing car parks in urban villages and neighbourhood centres, whilst maintaining or enhancing the existing level of parking.
- Ensure that developments in commercial areas that adjoin residential areas are sensitively designed to protect residential amenity.
- Ensure residential development in commercial areas does not contribute to traffic and car parking problems.

1.5.2 CLAUSE 21.06 – BUSINESS

This policy provides objectives and strategies to guide the development of identified urban villages and neighbourhood centres to cater for a wider variety of commercial use while retaining a hub of convenience shops within Glen Eira. The following policies are considered relevant to this built form framework:

Objectives (amongst others)

- To maintain a mix of commercial centres that cater for the needs of the Glen Eira community.
- To enhance and further develop urban villages and neighbourhood centres as the focus for community life.
- To maintain pleasant and safe public environments in commercial centres in partnership with business.

Strategies (amongst others)

- Maintain the hierarchy of commercial centres so that centre functions are easily definable and to ensure that retail and other requirements are suitably provided at each level in the hierarchy.
- Ensure that new or expanded land uses are able to be accommodated in existing commercial centres, including the encouragement of non-retail businesses, where appropriate.
- Ensure commercial activities in mixed use zone complement other commercial activities nearby and not undermine the commercial hierarchy.
- Ensure that, wherever possible, additional population growth can be accommodated (for example through multi-unit development) so existing facilities in commercial centres can continue to be supported.
- Ensure that commercial centres have a high standard of urban design and are attractive and appealing to potential customers, traders and investors.

1.5.3 CLAUSE 22.07 – HOUSING DIVERSITY AREA POLICY

This policy identifies the areas where housing diversity will be encouraged. It also seeks to ensure that the density, mass and scale of development is consistent with the role, capacity and constraints of each centre.

Objectives (amongst others)

- To encourage housing diversity in preferred strategic locations that have good access to public transport, commercial, community, educational and recreational facilities.
- To ensure that the density, mass and scale of residential development is appropriate to the location, role and neighbourhood character of the specific housing diversity area.
- To ensure that the siting and design of new residential development takes account of its interface with existing residential development on adjoining sites.
- To ensure that the design of new residential development is sensitive to and respectful of the scale of existing residential development on adjoining sites.

Strategies for Neighbourhood Activity Centres

- Recognise neighbourhood centres as locations which provide significant opportunities for housing diversity, but at a lesser scale and density than developments in urban villages and the Phoenix Precinct.
- Recognise that different development outcomes are sought in the commercial and residential areas of neighbourhood centres.

Strategies for commercial areas of centres (amongst others)

- Ensure that the density, mass and scale of development is appropriate to the scale, character and physical size of the neighbourhood centre.
- Ensure that any new residential development is designed and sited to minimise its adverse impacts on adjoining existing residential properties by way of building bulk, overlooking and overshadowing.

- Ensure that residential buildings are encouraged to step down at the rear to achieve a transition to residential areas.
- Ensure that building heights are compatible with their site context and the wider neighbourhood centre.
- Ensure that where the new building is greater in height than the prevailing building height or where significant changes in building height are proposed for residential buildings:
- There is a graduated transition in building height between the proposed building and adjoining buildings.
- The resulting height, mass and scale of the building does not dominate or visually intrude on the streetscape and takes account of views from the wider neighbourhood and at a distance.
- The upper storeys are recessive so that the visibility of upper storeys is reduced when viewed from the footpath opposite or residential properties to the rear.
- Provision be made for the establishment of new landmark buildings on strategic redevelopment sites that are made available through the removal of a level crossing.
- Ensure that the retail function and active frontage of centres is retained on the ground floor.

Strategies for the residential areas of centres (amongst others)

- Recognise that these areas offer opportunities for multi-unit development, but at a lower scale and density than development in the commercial and mixed use areas of neighbourhood centres.
- Ensure that the density, mass and scale of residential development is appropriate to that of the neighbourhood centre.
- Encourage a decrease in the density of residential development as the proximity to the commercial area of the neighbourhood centre decreases.
- Ensure that the siting and design of residential development responds positively to its interface with existing residential development in minimal change areas.

1.5.4 CLAUSE 21.10 – HERITAGE

This policy provides objectives and strategies to guide the development that does not threaten the heritage aspects of Glen Eira. The following policies are considered relevant to this study:

Objectives

To identify, protect, enhance and promote understanding of Glen Eira’s heritage.

Strategies

- Protect places identified as having architectural, cultural or historical significance.
- Ensure sympathetic redevelopment and renovation of areas and places identified as having architectural, cultural or historic significance in the municipality.
- Enhance knowledge and popular understanding of Glen Eira’s architectural, cultural and historic heritage.

1.5.5 CLAUSE 22.01 – HERITAGE POLICY

This policy applies to all land within the Heritage Overlay and seeks to identify, protect, enhance and promote understanding of Glen Eira’s heritage. More specifically, this Clause builds upon the basis of Clause 15.03 (Heritage) and Clause 21.10 (Heritage) and provides guidance on a number of elements:

Demolition (amongst others)

- Retain significant and contributory buildings.
- Allow the partial demolition of significant and contributory buildings where the fabric to be demolished is of no significance, or for the purpose of additions if the addition will not affect the heritage significance of the building and is sympathetic in its scale and form.
- To retain significant trees, and/or garden layouts that have been identified for their historical significance.

New buildings, Alterations and Additions (Commercial Heritage Areas)

- Encourage the conservation of setbacks that impart significance to those buildings that are surrounded by open space (including but not limited to churches and schools).
- Ensure any new upper level additions and works are respectful to the scale and form of the heritage place or contributory elements of the place and, where relevant, the heritage precinct as a whole.
- Encourage higher building additions to be well set back from the front wall of the building unless the specific context of the site recommends otherwise.
- Encourage the retention, restoration or reconstruction of original shopfronts and verandahs.
- Ensure commercial infill buildings adopt a contemporary architectural form or simplified interpretation of nearby contributory buildings.
- Discourage the introduction of architectural features, where it is known that these features were not originally present.
- Discourage signage above the verandah if it results in visual clutter in the streetscape and obscures views of the subject building and nearby contributory buildings.
- Ensure retention of signage deemed to have heritage value.
- Discourage sky signs, reflective signs, animated signs and electronic signs within heritage precincts

1.6 ZONES

1.6.1 CLAUSE 34.01 COMMERCIAL 1 ZONE (C1Z)

The NAC is predominantly included within the Commercial 1 Zone the purpose of which is (amongst others) to support vibrant mixed use commercial centres for retail, office, business, entertainment and community uses, and additionally provides for residential uses at densities complementary to the role and scale of the commercial centre.

The zone contains decision guidelines that ask decision makers to consider certain aspects of development when deciding planning applications. The most relevant decision guidelines for this zone are:

Amenity

- Consideration of the interface with adjoining zones, especially the relationship with residential areas.
- Consideration of the overlooking and overshadowing as a result of building or works affecting adjoining land in a General Residential Zone, Neighbourhood Residential Zone, Residential Growth Zone or Township Zone.
- The objectives, standards and decision guidelines of Clause 54 and Clause 55. This does not apply to an apartment development. For an apartment development, the objectives, standards and decision guidelines of Clause 58.

Built form

- Consideration of the streetscape, including the conservation of buildings, the design of verandahs, access from the street front, protecting active frontages to pedestrian areas, solar access, the treatment of the fronts and backs of buildings and their appurtenances, illumination of buildings or their immediate spaces and the landscaping of land adjoining a road.

Transport

- Considerations relating to the movement of pedestrians and cyclists, and vehicles providing for supplies, waste removal, emergency services and public transport.

1.6.2 CLAUSE 36.01 PUBLIC USE ZONE (PUZ6)

Several allotments within the north-east and south portion of the Bentleigh East Activity Centre are included within Schedule 6 to the Public Use Zone. The PUZ seeks to recognise public land use for public utility and community services and facilities, and provides for associated uses that are consistent with the intent of the public land reservation or purpose.

PUZ6 designates the associated public land use for the purpose of ‘Local Government’.

The relevant decision guidelines of the PUZ6 includes the following:

- The comments of any Minister or public land manager having responsibility for the care or management of the land or adjacent land.
- Whether the development is appropriately located and designed, including in accordance with any relevant use, design or siting guidelines.

1.6.3 CLAUSE 32.04 MIXED USE ZONE (MUZ1)

A pocket of land to the east of East Boundary Road within the Bentleigh East Activity Centre Zone is within Schedule 1 to the Mixed Use Zone the purpose of which is to provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality, housing at higher densities, and additionally encourage development which responds to the existing or preferred neighbourhood character of the area.

Schedule 1 to the Mixed Use Zone is designated ‘Glen Eira Mixed use Areas’.

The relevant decision guidelines of the MUZ1 ask decision makers to consider:

Development

- The impact of overshadowing on existing rooftop solar energy systems on dwellings on adjoining lots in a Mixed Use Zone or Residential Growth Zone.
- For two or more dwellings on a lot, dwellings on common property and residential buildings, the objectives,

standards and decision guidelines of Clause 55. This does not apply to an apartment development of five or more storeys, excluding a basement. For an apartment development of five or more storeys, excluding a basement, the objectives, standards and decisions guidelines of Clause 58.

Use

- The effect of traffic to be generated by the use.
- The interim use of those parts of the land not required for the proposed use.
- Whether the use is compatible with adjoining and nearby land uses.
- For non-residential uses, the proposed hours of operation, noise and any other likely off-site amenity impacts.

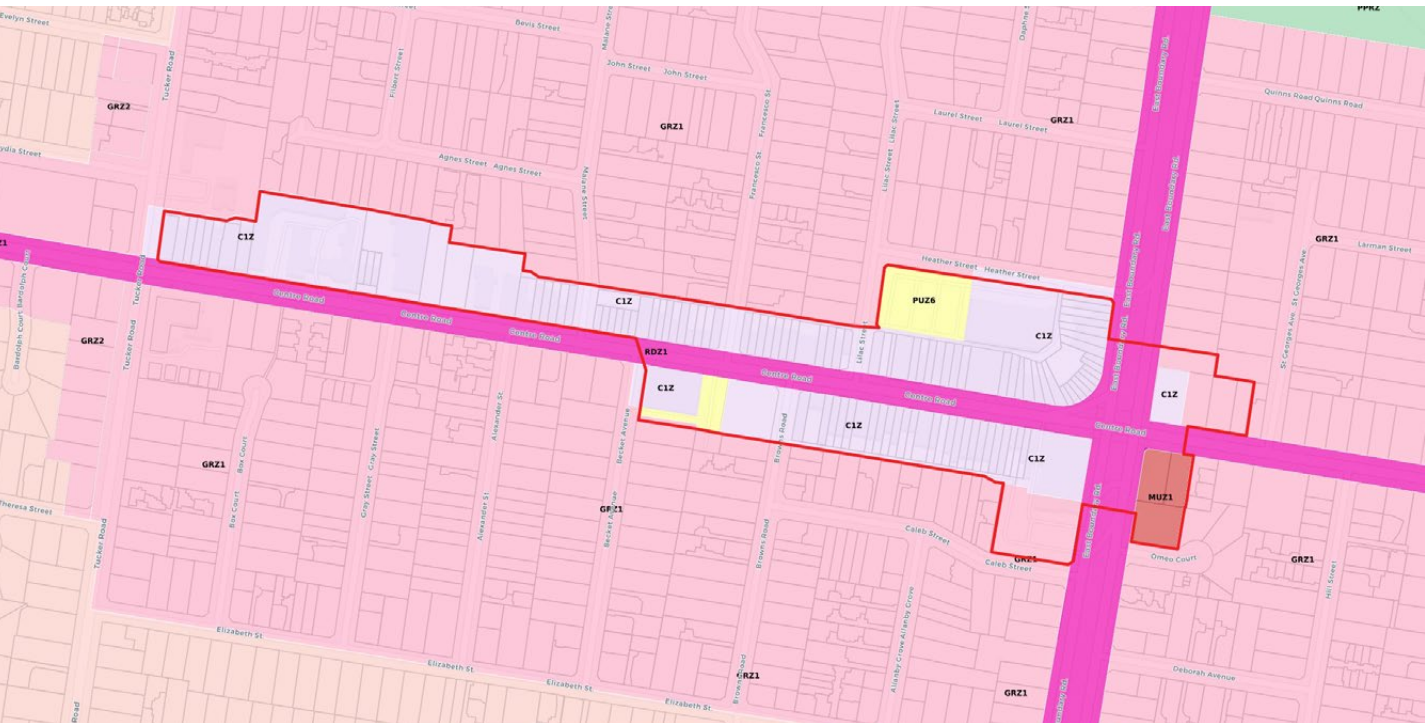


Figure 5. Planning Zones

1.7 OVERLAYS

1.6.4 CLAUSE 43.01 HERITAGE OVERLAY (HO99)

The Heritage Overlay aims to conserve and enhance heritage places of natural or cultural significance and to ensure that development does not adversely affect the significance of heritage places.

The HO (Schedule 99) applies to a single allotment within the western portion of the Study Area namely '675 Centre Road, Bentleigh East'.

The relevant decision guidelines ask decision makers to consider:

Heritage significance

- The significance of the heritage place and whether the proposal will adversely affect the natural or cultural significance of the place.
- Any applicable statement of significance (whether or not specified in the schedule to this overlay), heritage study and any applicable conservation policy.

Built form

- Whether the location, bulk, form or appearance of the proposed building will adversely affect the significance of the heritage place.
- Whether the location, bulk, form and appearance of the proposed building is in keeping with the character and appearance of adjacent buildings and the heritage place.

Demolition, works and subdivision

- Whether the demolition, removal or external alteration will adversely affect the significance of the heritage place.
- Whether the proposed works will adversely affect the significance, character or appearance of the heritage place.
- Whether the proposed subdivision will adversely affect the significance of the heritage place.
- Whether the proposed subdivision may result in development which will adversely affect the significance, character or appearance of the heritage place.

- Solar energy systems
- Whether the location, style, size, colour and materials of the proposed solar energy system will adversely affect the significance, character or appearance of the heritage place.

1.6.5 CLAUSE 45.09 PARKING OVERLAY (PO2)

Schedule 2 to the Parking Overlay applies to the entire NAC. Schedule 2 to the Parking Overlay is titled 'Student Housing in Specific Areas'. Where student housing is proposed in this area, the schedule aims to provide car parking at a rate commensurate with the reduced ownership pattern of students, allowing a reduced rate of on-site car parking of 0.5 spaces per student housing bed provided.



Figure 7. Heritage Overlay Plan

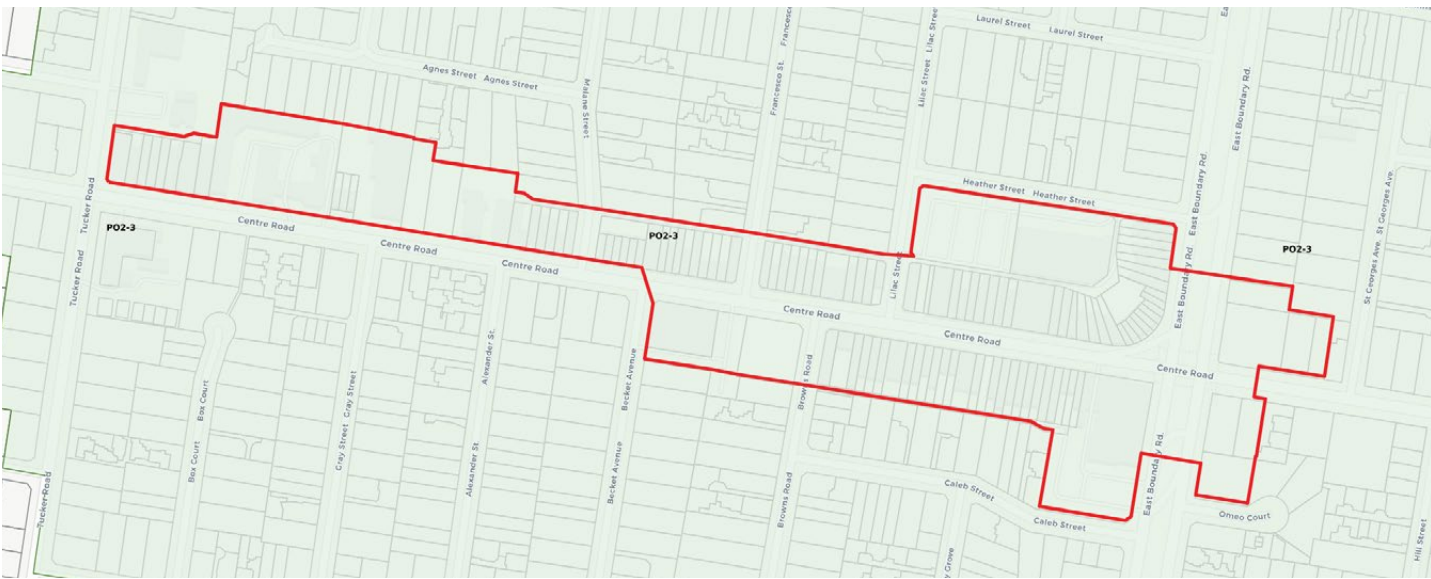


Figure 6. Parking Overlay Plan

2 ANALYSIS, ISSUES AND OPPORTUNITIES

The following chapter outlines analysis, issues and opportunities to be addressed through the development of the Built Form Framework for the Bentleigh East NAC.

2.1 TOPOGRAPHY

Figure 8 reveals the elevation of landform within the NAC and surrounds, and identifies key topographic features.

The mapping shows that there is a subtle rise to the east with a ridgeline located outside of the study area. There is a fall of 5m from the eastern edge to the western edge of the centre.

The flat topography makes it easy to walk within the centre and will not present any constraints to future development.

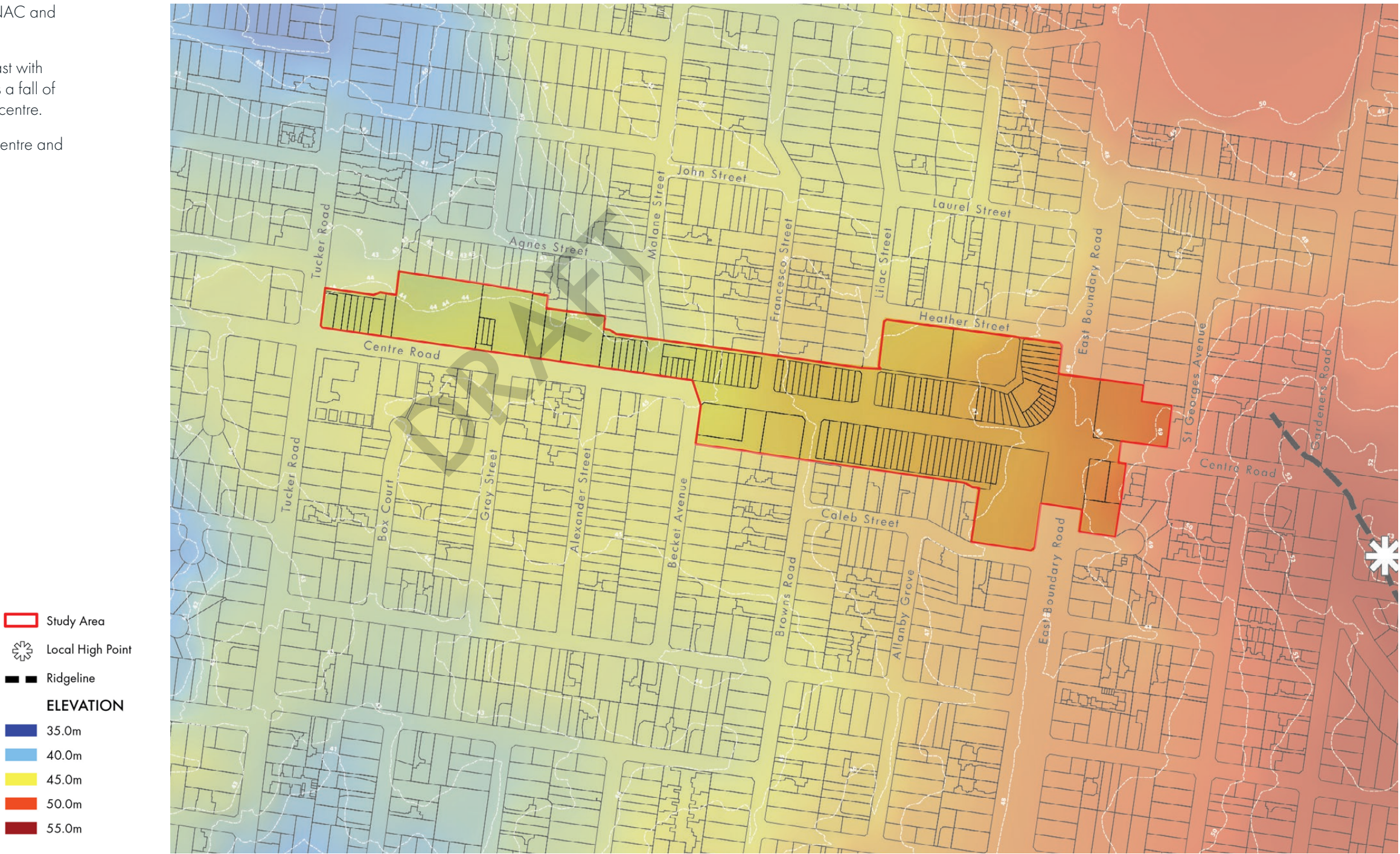


Figure 8. Elevation Plan

Figure 9 provides an assessment of land slope within the NAC.

The slope on the majority of sites is in the 0-2.5% range, which is generally flat. This presents minimal constraints to development opportunities.

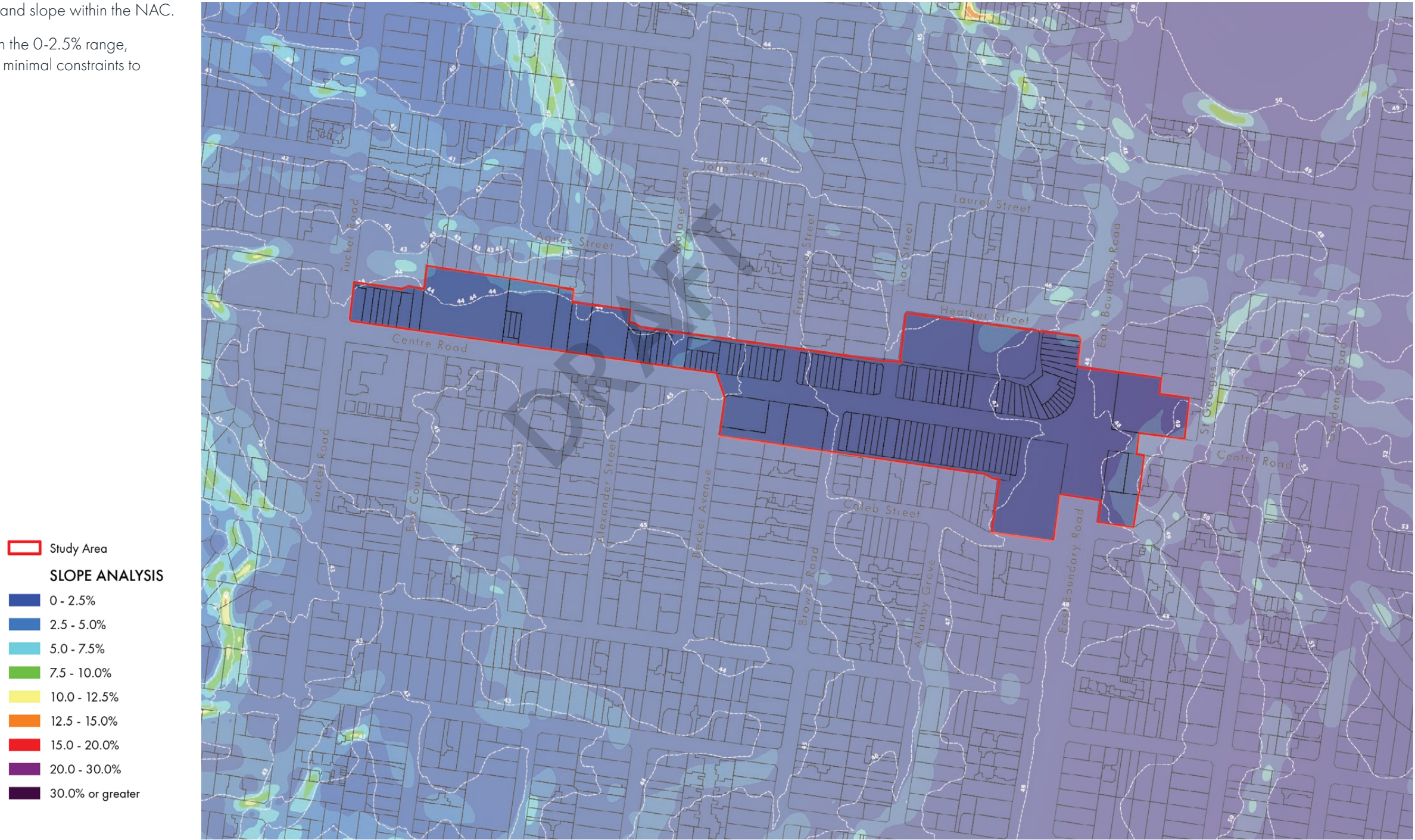


Figure 9. Slope Analysis

2.2 LAND USE & INTERFACES

Figure 10 identifies land use precincts and a range of interface conditions across the NAC.

The Bentleigh East NAC is a relatively large centre extending along Centre Road for a length of 770m. It provides a local retail, hospitality and service role for the community (see Image 1-2). Compared to other NAC's its public transport offering is limited, with bus routes located along Centre Road and East Boundary Road. However it does include a higher frequency SmartBus route on Centre Road.

The Bentleigh East NAC is broadly defined by four areas of activity. A core retail area is located at the eastern end of Centre Road. This area is the most vibrant within the NAC and includes a range of hospitality uses. It is anchored by the IGA which fronts onto a car park at the rear of the shops and is accessible from Centre Road via a walkway. There are a number of double fronted shops in this location taking advantage of the customer activity generated by the IGA.

A small secondary retail area is located at the western end of the NAC focused around the Tucker Road intersection. This area is separated from the balance of the NAC by larger format uses including a service station and factoryettes, and an Australia Post warehouse. These uses have limited engagement with the footpath and generate minimal pedestrian activity.

East of East Boundary Road are two properties which have been recently developed providing a mix of retail and residential uses. The expansive road reserve separates these properties from the majority of the NAC.

The NAC has a predominant interface with residential uses. There is a mix of laneways separating residential and commercial uses, direct abuttal between the uses, and a small area where a road separates the uses (see Image 3). There is a substantial section of the NAC with residential uses located to the south of commercial properties. Potential shadow impacts from new development will be the greatest in this location.

2.2.1 KEY ISSUES AND OPPORTUNITIES

- There are a number of new mixed use redevelopments within the activity centre providing a narrow shop-front character of the street (see Image 4). There is an opportunity to continue this character across the NAC.
- Large at-grade car parks and building loading areas near IGA front onto a number of residential streets in the centre. There are opportunities to improve this interface.
- There are several poor street address related to at-grade car park and car-related land use along Centre Road, as well as blank wall interface on side streets (see Image 5). There are opportunities to improve these areas.
- The residential interface to the south provide constraints for future building heights and setbacks. Development will require a sensitive design response to transition to adjoining residential areas (see Image 6).



Image 1. Hospitality uses at the eastern end of Centre Road take advantage of the widened footpaths created by the pedestrian crossing



Image 2. A mix of hospitality and personal services are located at the eastern end of Centre Road



Image 3. An existing commercial, laneway and residential interface



Image 4. Narrow shopfronts in a recent development wrap around the corner to activate the secondary frontage



Image 5. Recent developments on Centre road with large basement car park entries and blank walls diminish continuous activity along the street



Image 6. A mixed use development of five storeys scales effectively to the adjoining residential area

- LAND USES**
- Primary Retail
 - Office / Commercial / Secondary Retail
 - Mixed Use - Redevelopment Sites
 - Open Space
- INTERFACES**
- Residential Interface - Laneway Separation
 - Residential Interface - Direct Abuttal
 - Open Space Interface
 - School/Community Interface
 - Poor Street Address / Interface

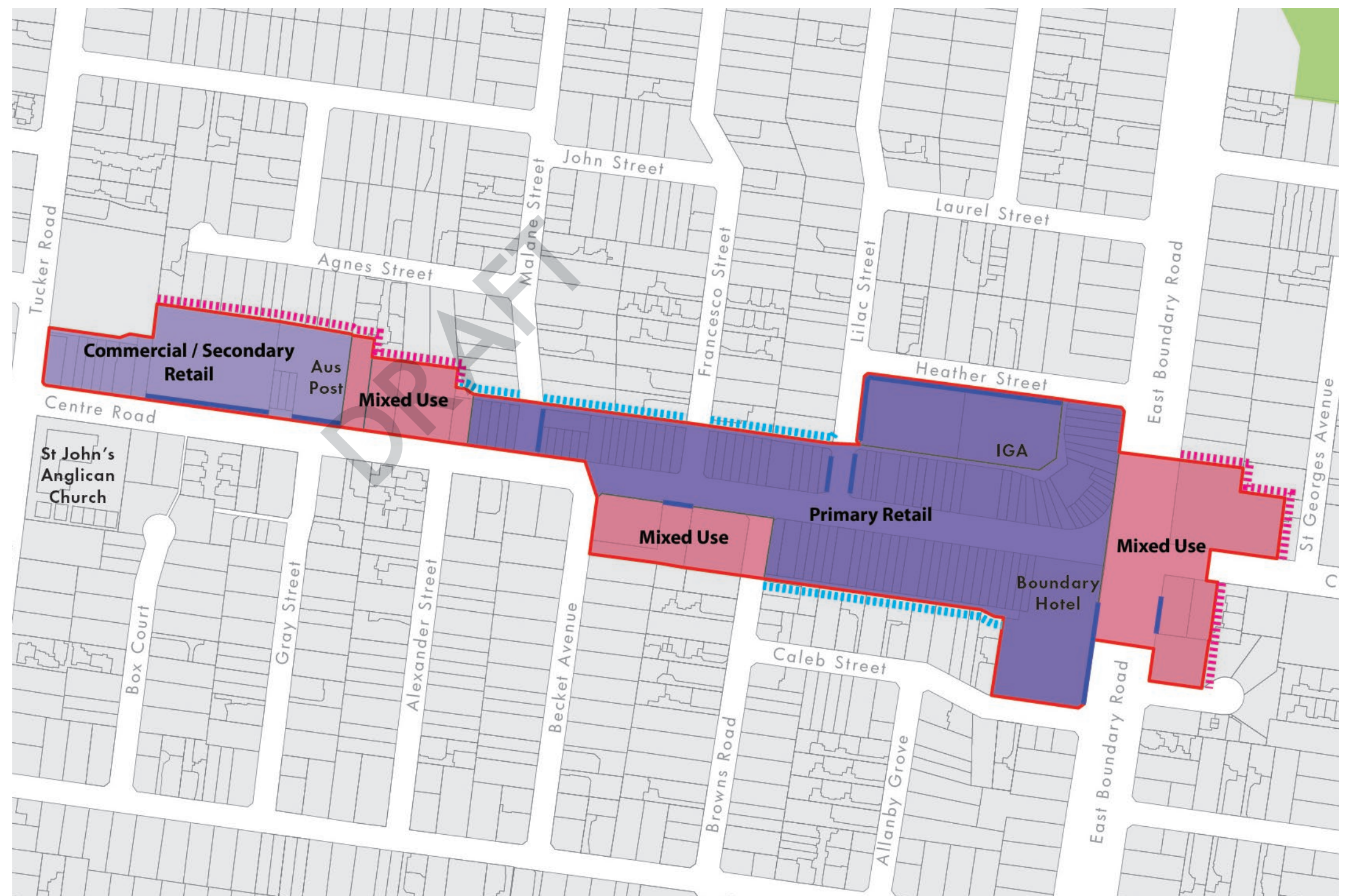


Figure 10. Land Use and Interfaces Analysis

2.3 BUILT FORM CHARACTER & PUBLIC REALM

Figure 11 maps a range of built form character and public realm elements across the Bentleigh East NAC.

Understanding the existing and emerging built form and character of the Bentleigh East NAC is important in order to identify character elements that should be protected, how built form could be improved and assess the impact of new, taller development on the streetscapes.

The Bentleigh East NAC has low-scale built form character with a strong presence of single storey buildings. Shopfronts are narrow providing a fine-grain character however they have limited architectural detailing and generally low parapets (see Image 7).

There are a number of large scale buildings of 3-5 storeys located on the southern side of Centre Road and in the western sections of the NAC (see Image 8-9). These buildings provide a stronger presence of built form to the street with a three storey street wall. Similarly two new developments on the east side of East Boundary Road of 5-6 storeys and provide a strong built form bookend to the centre (see Image 11).

The service station and Australia Post sites in the western sections of the NAC occupy a substantial frontage to Centre Road. Buildings on these sites are set back and provide limited activation to the footpath (see Image 12). There is a significant opportunity to improve this frontage to create continuous activity along Centre Road.

Weather protection through building awnings is well provided across the NAC. With the exception of the service station and Australia post site, weather protection is provided to the retail footpaths of Centre Road and the small frontage along East Boundary Road.

The footpaths along Centre Road are key public spaces. The southern footpath has good access to sunlight providing opportunities for on street dining and gathering.

2.3.1 KEY ISSUES AND OPPORTUNITIES

- The existing one storey shops provide limited built form presence in the streetscape. There are opportunities to replace these buildings with higher scale development and create a more contemporary built form character.
- The narrow or 'fine-grain' shop-fronts, particular on the east end of Centre Road, are an important part of the streetscape character and provide for a diversity of uses and experiences. This should be retained and reflected in new development across the NAC.
- It will be important to maintain solar access to key pedestrian streets in the activity centre, particularly the southern footpath of Centre Road.
- There is generally a lack of "green" public spaces in the activity centre. There are opportunities to provide additional greenery within the Centre Road Streetscape, and potential footpath widening through kerb outstands in strategic locations (see Image 13). There may be an opportunity to provide new public spaces on larger sites when they are redeveloped .
- Signalised pedestrian crossings are located at Tucker Road junction, East Boundary Road junction, and between East Boundary Road and Lilac St on Centre Road. Given the significant length of the strip there may be opportunities to provide additional crossings along the strip Centre Road.
- There is an internal connection between Centre Road and IGA entrance in one retail shop (Bakers Delight). This type of mid-block internal links should be retained and encouraged in future developments.



Image 7. Existing shopfronts on Centre Road are generally single storey with low parapets and limited architectural detailing



Image 9. Recent mixed use developments of 4 and 5 storeys on Centre Road create a new urban character with a 2-3 storey street wall



Image 11. Recently developed buildings of a substantial scale mark the intersection of Centre Road and East Boundary Road



Image 8. Recent development of 6 storeys is of a significant scale compared to the heritage protected East Bentleigh Hall



Image 10. The building scale and character at the western end of the centre is mixed with a number of redeveloped properties interspersed with original shopfronts



Image 12. The Australia Post and Service Station diminish activity along Centre Road



Image 13. Centre Road Footpaths support limited outdoor dining because of width constraints. Opportunities for kerb extensions in strategic locations to increase street activity

- BUILT FORM**
- 'Fine grain' Retail
 - Existing Awnings
 - Poor Street Address / Interface
 - Inconsistent Setbacks
 - Existing Heritage Overlays
 - Prominent Corners
 - Existing 1-2 Storey Building Height
 - Existing 3+ Storey Building Height
 - Approved 3+ Storey Building Height
 - Existing At-grade Car Park
- PUBLIC REALM**
- Primary Public Realm Areas
 - Secondary Public Realm Areas
 - Laneway Access

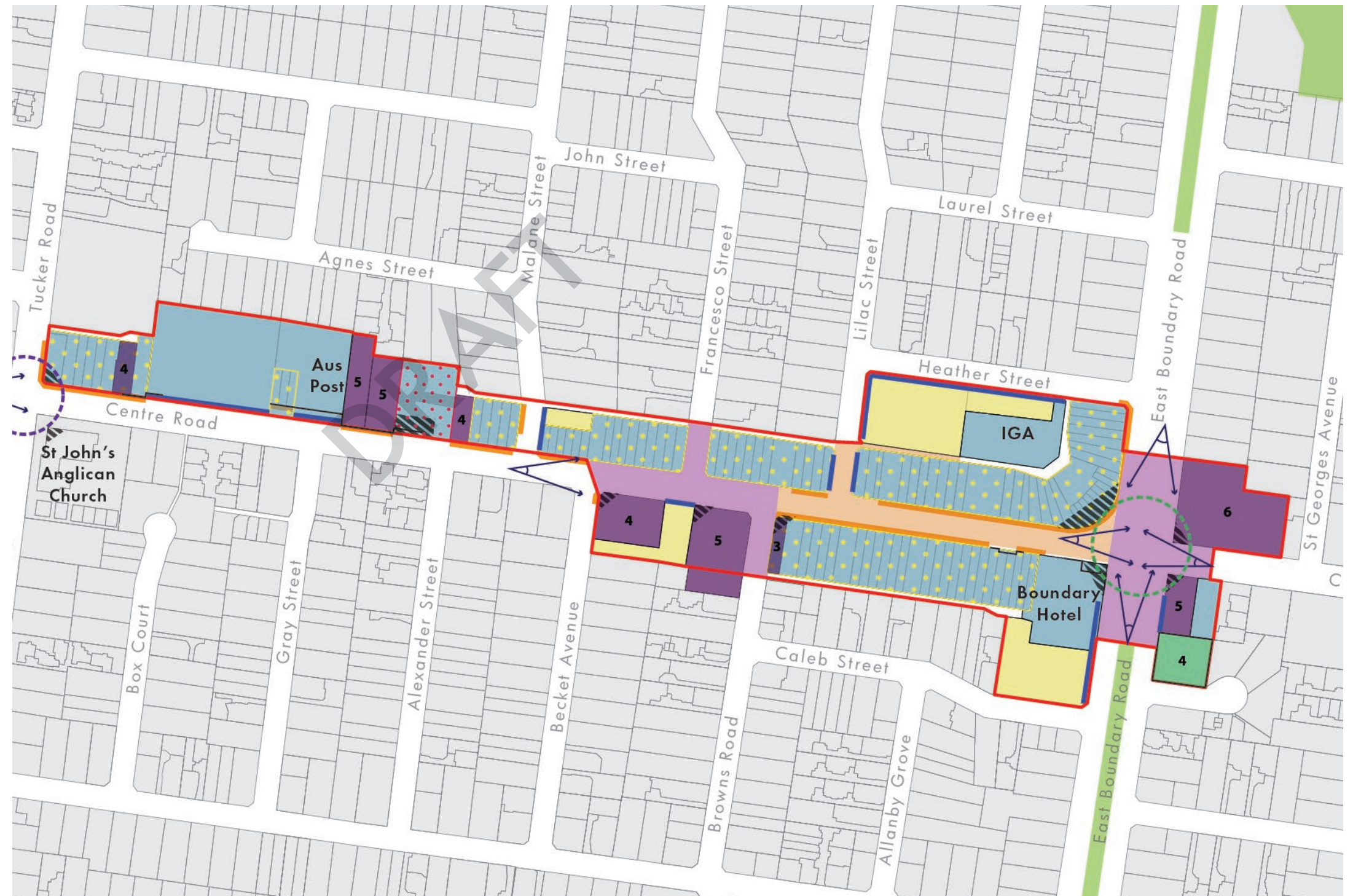


Figure 11. Built Form Character and Public Realm Analysis

2.3.2 EXISTING SHADOW ANALYSIS

Figure 12 demonstrates the shadows cast by existing buildings within the Bentleigh East NAC. Development above 3 storeys and recent approvals are shown separately in green.

The date selected for the shadow analysis is September 22 - the equinox. This represents the mid-point between the summer solstice and winter solstice. It is the date commonly used in planning assessments to assess the impact of shadows.

The analysis demonstrates that more recently developed buildings on Centre Road cast more shadow than the original low scale shopfronts however the shadow does not impact on the southern footpath.

The 3D model for the shadow analysis has been developed using PSMA Geoscape building footprint data which provides an outline of the building footprint and overall roof height. The building footprints have been extruded up to the overall roof height and integrated into a topographic model.

9am - Sept 22



10am - Sept 22



- NAC Boundary
- Approved / Constructed Development of 3 storeys or greater
- Shadow from existing and approved buildings

Figure 12. Existing Shadow Analysis



1pm - Sept 22



2pm - Sept 22





2.4 DEVELOPMENT ACTIVITY AND CAPACITY

Figure 14 maps recent development activity and an indication of constrained and opportunity sites within the centre based on their size

Development activity and capacity is an important consideration in preparing the Built Form Framework. Recent development can indicate the type of development that the centre will attract in the future. Analysing the potential capacity of the centre is also important to understand the scale and type of development that could potentially be accommodated.

Development activity has been spread across the Bentleigh East NAC. Building heights for new development range between 3 and 6 storeys. The lower scale developments have tended to be located on smaller sites in the western sections of the NAC.

2.4.1 KEY ISSUES AND OPPORTUNITIES

- The mapping opposite identifies areas at the eastern sections of Centre Road where future development may be constrained, due to lot size, width and depth. Lot consolidation may be required to support development (see Image 14-15)
- There are a number of sites that present good opportunities for development based on lot size.
- The NAC has an interface with the General Residential Zone which allows development of up to three storeys. It will be important for future building heights to transition sensitively to the surrounding residential areas.



Image 14. Smaller sites on Centre Road that may be constrained for development due to their width unless they are consolidated



Image 16. A recent development of a wider lot on Centre road



Image 15. Although properties are fairly narrow at the western end of Centre Road, some sites have been consolidated and redeveloped



Figure 14. Development Activity, Opportunities and Constraints

2.5 LOT WIDTH ANALYSIS & TYPOLOGIES

Figure 15 provides analysis of lot widths across the Bentleigh East NAC and Figure 16 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot width.

The typology analysis has been undertaken on the basis of a residential development outcome. Commercial development would be less constrained with less of a need to provide daylight into habitable rooms.

Front and rear facing apartments provide the simplest development outcome for narrow lots, as they can facilitate 1-2 apartments facing the street, and one facing the rear of the property, on each floor (subject to width). This type of development occupies the entire width of the lot, and does not provide any articulation or activation to either side of the development (which is assumed will develop in a similar manner).

Wider properties can accommodate a different model of development, podium and tower, which can have an outlook to other developments either side. This type of development can provide for a greater range of apartment types with varying aspects. It does however create issues of apartment separation for privacy and daylight that need to be considered.

The analysis indicates that there are a number of narrow lots at the eastern end of the centre, which would not support apartment development in their current configuration. Sites west of Becket Avenue are wider and will support a range of building configurations.

While this analysis identifies potential development outcomes for individual properties, it does not take into consideration the potential for multiple properties being consolidated, which often occurs in Activity Centres.



Figure 15. Lot Width Analysis

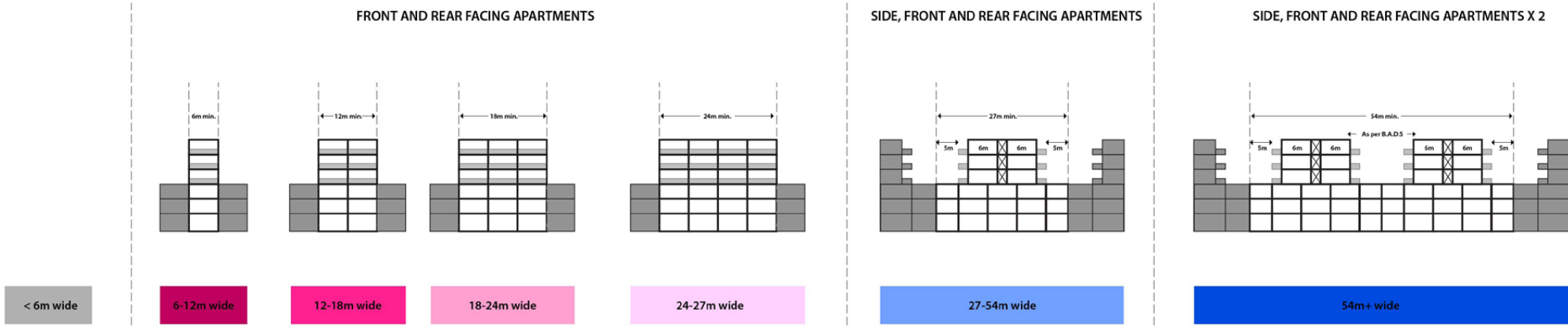


Figure 16. Lot Width Typologies

2.6 LOT DEPTH ANALYSIS & TYPOLOGIES

Figure 17 provides an analysis of lot depths across the NAC and Figure 18 provides an indication of the potential development outcomes that could be accommodated on each site based on the lot depth.

Similar to lot width, the depth of a property has an impact on the type of development that can be provided as shown on the typologies opposite.

The typologies indicate that shallow lots provide a limited opportunity for development, and allow for only single aspect apartments. This takes into account a typical setback to a residential interface, and a suitable upper level setback to the primary street frontage.

Lots which are deeper provide opportunities for dual aspect apartments, addressing the primary street, and the rear of the lot.

Lots greater than 55m (approx.) could allow for two buildings with both front and rear facing apartments. This outcome does create apartment separation issues that need to be considered.

The analysis indicates that the majority of sites are deep enough to support front and rear facing apartment developments. Deeper lots are located west of Alexander Street, along East Boundary Road and along Heather Street and could support alternative configurations.



Figure 17. Lot Depth Analysis

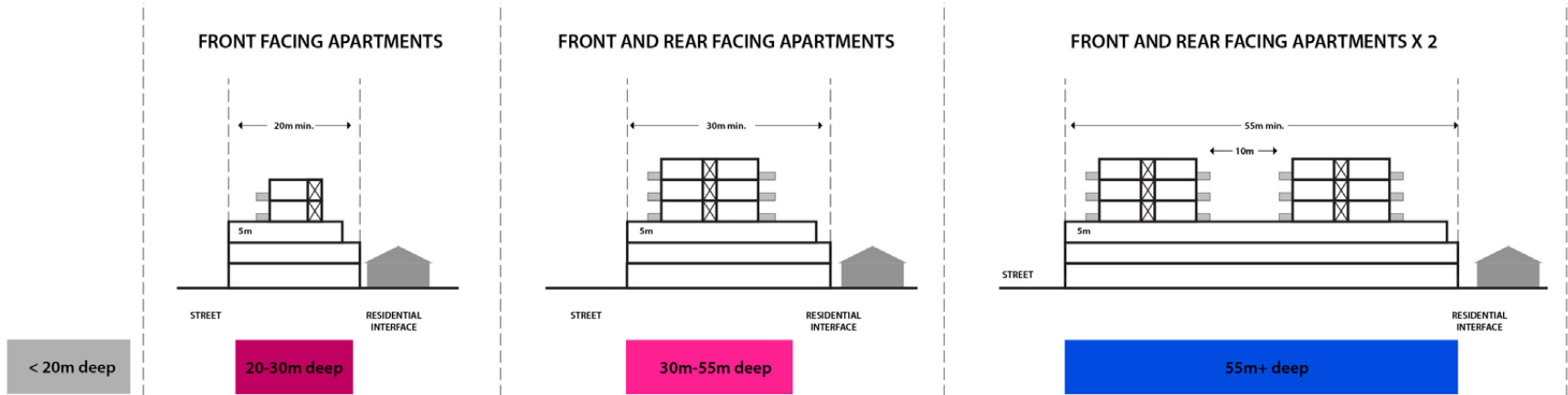


Figure 18. Lot Depth Typologies

PART 2 - THE BUILT FORM FRAMEWORK

3 DESIGN PRINCIPLES

3.1 THE PRINCIPLES

4 BUILT FORM FRAMEWORK

4.1 BUILT FORM FRAMEWORK

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

4.3 PRECINCT 1 - EASTERN JUNCTION

4.4 PRECINCT 2 - CENTRE ROAD RETAIL

4.5 PRECINCT 3 - CENTRE ROAD WEST



EAST BENTLEIGH HALL

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3 DESIGN PRINCIPLES

The following over-arching principles have been prepared to guide the drafting of design objectives and requirements for the Bentleigh East NAC. The principles aim to respond to the issues and opportunities identified in Part 1 relating to amenity and character, and an understanding of the centre’s strategic role in meeting future housing and employment needs.

3.1 THE PRINCIPLES

PRINCIPLE 1 - A VIBRANT NEIGHBOURHOOD CENTRE WITH A MID-RISE CHARACTER

MID-RISE CHARACTER

Glen Eira includes a mix of both Major Activity Centres (MACs) and Neighbourhood Activity Centres. The MACs play a significant role for the community providing a number of public transport options, and access to retail, services and employment opportunities. The NACs play an important, though less significant role in meeting future needs for housing and employment as they are smaller in size with less retail and employment opportunities. The Bentleigh East NAC also has comparatively less public transport options than some others with fixed rail.

The Glen Eira City Plan outlines the preferred focus for the NACs and notes that they will have a ‘medium’ focus for employment growth and a ‘moderate’ focus for housing growth. Bentleigh East is identified as a Neighbourhood Activity Centre and as such is intended to experience moderate growth.

The Bentleigh East NAC is somewhat constrained for development. It includes areas of smaller lots, a residential interface along most of its edges and key footpaths which need to be protected from overshadowing. Combined together, these constraints will limit development opportunities. However there are a number of larger sites in the NAC where higher scale development can be accommodated within minimal visual and amenity impact.

The recommended approach for the Bentleigh East NAC is to establish a mid-rise character providing for building heights in the 5-6 storey range. These heights will support further growth of the centre whilst sensitively integrating with the surrounding residential neighbourhoods.

PRINCIPLE 2 - AN ENJOYABLE AND SAFE PLACE TO SHOP, WORK, GATHER, DINE AND EXERCISE

REINFORCE THE HUMAN SCALE TO KEY STREETS

Existing buildings within the Bentleigh East NAC are generally low scale, between one and two storeys in height. The low scale character of the centre will change over time and there has been some recent development in the centre of five to six storeys.

It is important that new, taller buildings are designed in a way that integrates with the existing low scale of the centre and do not dominate the streetscape. This is possible by providing a lower scale building at the street edge and setting the taller elements further behind.

A three storey building height at the street is recommended for the Bentleigh East NAC, which will maintain a scale that is both compatible with existing one and two storey shopfronts, and does not overwhelm the streetscape. This has been evidenced with recent developments in the NAC that provide a three storey street wall.

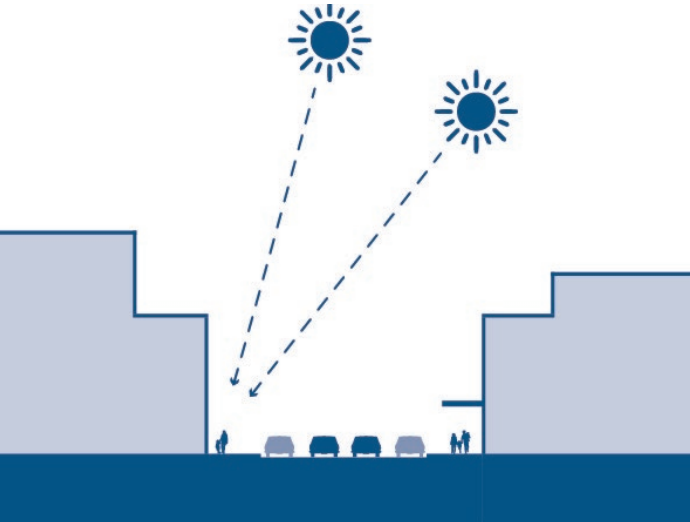
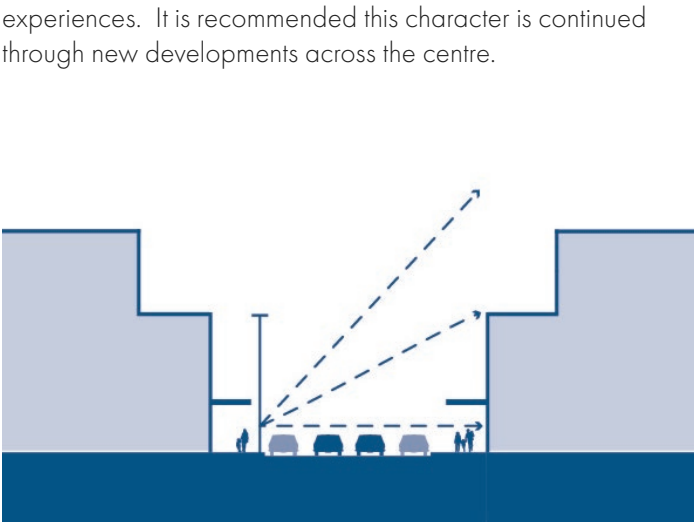
Another aspect to the human scale in the Bentleigh East NAC is the ‘fine-grain’ narrow shopfronts, which provide visual interest and provide for a greater diversity of uses and experiences. It is recommended this character is continued through new developments across the centre.

MAINTAIN SOLAR ACCESS TO KEY FOOTPATHS

The Bentleigh East NAC is limited in terms of its public gathering spaces such as squares and parks. Therefore the footpaths play an important role as spaces for people to gather and interact. Maintaining sunlight to key footpaths is important to encourage outdoor dining and street based retail, and support the vitality of the centre.

The Built Form Framework Plan for the Bentleigh East NAC identifies the key footpaths where access to sunlight is considered to be important. This includes the southern footpath of Centre Road, where retail and hospitality activity is currently focused, and the eastern and western footpath of East Boundary Road.

Sunlight access is often measured at the equinox (22 September) in Planning Schemes across Victoria. This date sits mid-point between the winter solstice (June 22) where shadows are at their longest, and the summer solstice (December 22) where shadows are at their shortest. For the Bentleigh East NAC Built Form Framework, the Equinox was selected as the date to measure solar access.



The following measures for solar access have been adopted for the Built Form Framework. These time periods will ensure sunlight is provided to the footpaths at the most active times of the day, which will help to support hospitality and retail uses.

These measures were tested and considered to provide a balance between providing good solar access whilst not unreasonably limiting development opportunities in the centre.

Maintain sunlight to key footpaths as follows:

- Southern footpaths of Centre Road - Solar access from 10am on September 22
- Western footpaths of East Boundary Road - Solar access from 10am - 12pm on September 22
- Eastern footpaths of East Boundary Road - Solar access from 12pm - 2pm on September 22

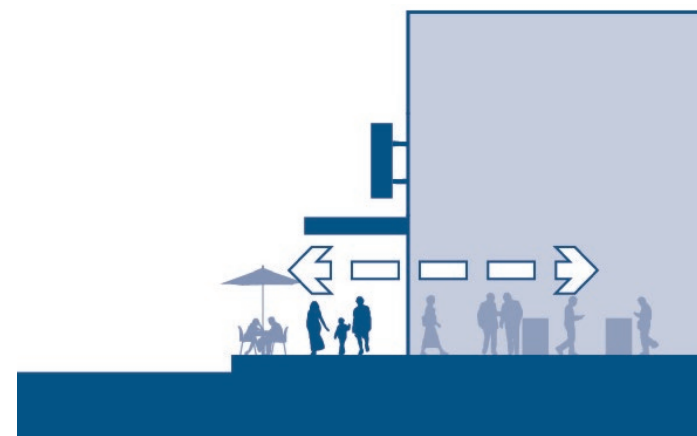
PROVIDE FOR A CONTINUOUS NETWORK OF ACTIVE FRONTAGES WITH WEATHER PROTECTION

Continuous retail and business activity across the Bentleigh East NAC is key to providing a positive pedestrian experience. Locations where there are blank walls, car park or loading areas to the street, disrupt the flow of retail activity and provide limited passive surveillance of the footpaths.

All streets across the Bentleigh East NAC are treated as active frontages, with windows at ground level, and uses at the front of the building that provide for customer engagement. Buildings on corners will provide active frontages to both streets. In addition, uses above the ground level are encouraged to address the street with windows and balconies.

All buildings should be constructed with floor to ceiling heights that would support retail, commercial and hospitality uses at ground level. This will allow for buildings to be easily adapted for such uses into the future and further strengthen street based activity.

Existing awnings provide weather protection along the majority of properties on Centre Road. New developments should continue this element with awnings placed at lowered heights to reinforce an intimate pedestrian environment.



PRINCIPLE 3 - A CENTRE THAT EMERGES SENSITIVELY FROM THE SURROUNDING NEIGHBOURHOOD AND CONSIDERS RESIDENTIAL INTERFACES BOTH WITHIN AND OUTSIDE THE CENTRE

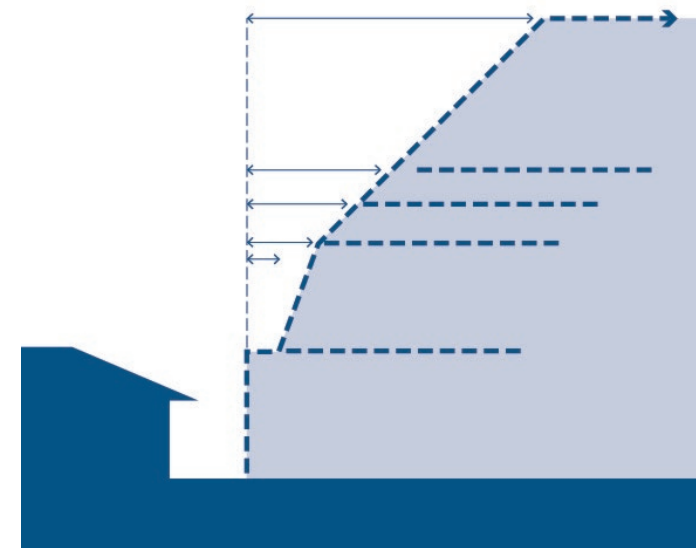
ENHANCE THE RESIDENTIAL INTERFACE

The Bentleigh East NAC abuts residential uses along all of its edges. The interface varies between direct abuttal to a residential property or a laneway separating the uses.

It is important that development in the NAC transitions appropriately to the residential edge to protect the amenity of these areas.

For locations where there is a direct abuttal to residential properties, a single storey built form edge is proposed, which is then recessed to minimise visual impact and reduce the impacts of overlooking and overshadowing. Where a laneway exists, a two storey built form edge is proposed to the laneway which then recesses considerably.

In addition to the upper level setbacks, development will need satisfy relevant requirements from Clause 54, 55 and 58 of the Glen Eira Planning Scheme to further protect residential amenity.

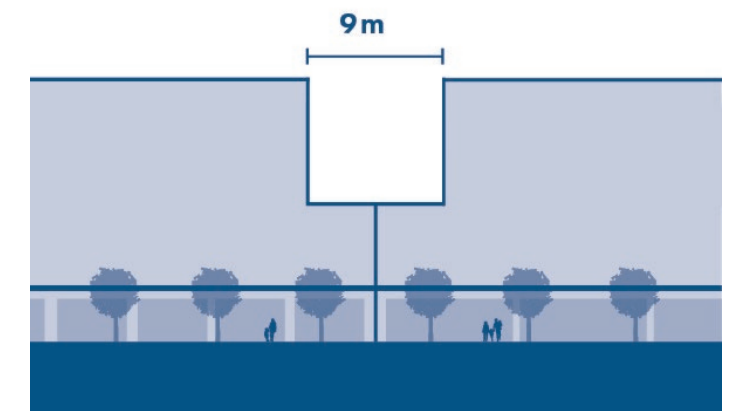


PROVIDE FOR EQUITABLE ACCESS TO AMENITY

Development across the Bentleigh East NAC is likely to be sporadic with potential for new, taller buildings to be located adjacent to existing, low scale buildings for a substantial period of time. It is important to have measures in place to ensure the future development potential of adjoining sites is not significantly compromised by the first development.

A key consideration in equitable access is ensuring adjoining buildings have sufficient separation, to limit overshadowing and ensure adequate privacy for apartments and access to daylight. The centre-wide frameworks provide for a 9 metre separation distance between apartment developments where they have balconies or windows of habitable rooms facing each other.

Zero side and rear setbacks are encouraged from ground level, up to 3 storeys. Land uses on these levels will typically be retail or office uses where access to sunlight and privacy issues are less critical.



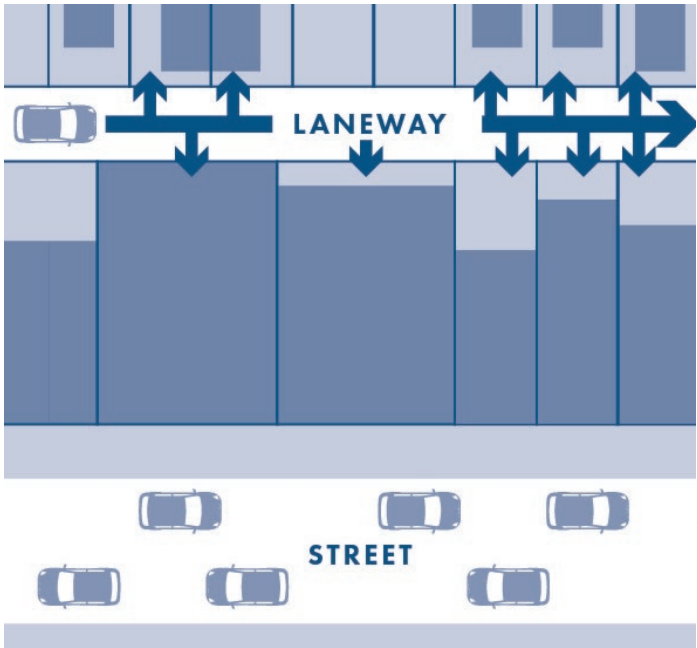
PRINCIPLE 4 - A WELL SERVICED AND ACCESSIBLE CENTRE

ENSURE ADEQUATE SERVICING OF EXISTING AND NEW DEVELOPMENTS

As the Bentleigh East NAC grows and intensifies with additional development, so do its servicing and access requirements for buildings. This includes access to car parking for residents, workers and customers, access for service vehicles, and pedestrian and cycle access.

It is important that new development takes advantage of existing service access arrangements through laneways in order to avoid vehicle disruption to footpaths. Where no laneway exists, driveway crossovers should be located on secondary frontages and minimised in width.

Another important consideration is ensuring new development can be accessed adequately by pedestrians and cyclists.



4 BUILT FORM FRAMEWORK

This section outlines Centre-Wide Design Objectives and Requirements that will apply to all development within the NAC, and more specific building height, setback and design requirements that apply to three Built Form form precincts identified in Figure 19.

4.1 BUILT FORM FRAMEWORK

Figure 19 - Built Form Framework Plan, identifies the preferred heights, setbacks and other built form requirements across the Bentleigh East NAC.

The proposed building heights have been developed through application of the design principles outlined above, responding to key issues and opportunities identified in Part 1 of this report, and testing of the built form outcomes through 3D modelling.

The recommended heights have taken into account a number of considerations including responding to sensitive interfaces, ensuring solar access to key footpaths and public spaces, and understanding the development potential of properties based on size and other constraints.

This plan identifies the key public realm areas, where good access to sunlight is required. These areas include existing footpaths where there is a high amount of pedestrian activity. Upper level setbacks are recommended at these locations to maintain good access to sunlight.

The strategic justification and rationale for the application of building heights and setbacks in each precinct is discussed in detail in Sections 4.3 to 4.5.

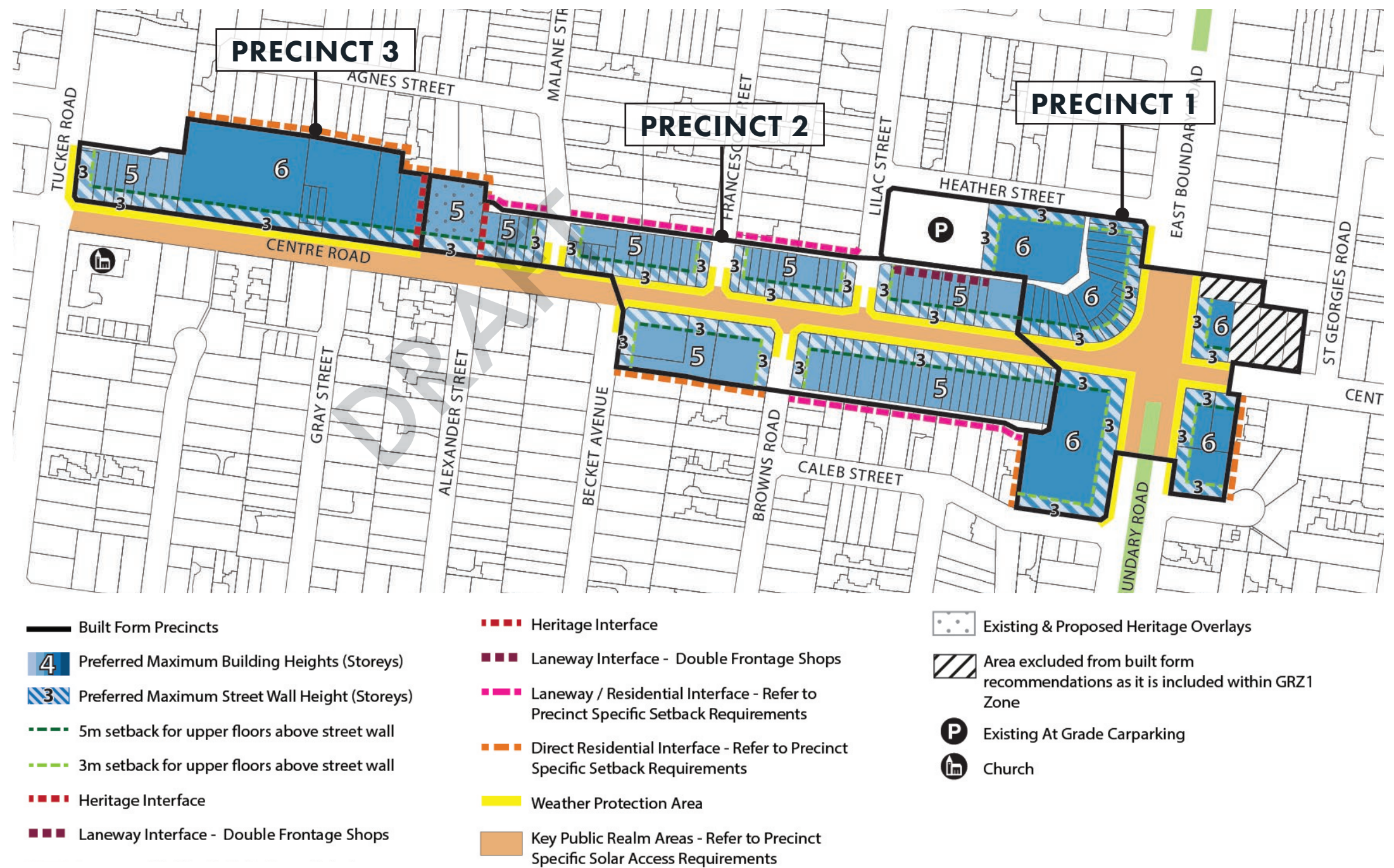


Figure 19. Built Form Framework Plan

4.2 CENTRE-WIDE DESIGN OBJECTIVES AND REQUIREMENTS

The following design objectives and design requirement apply to development within all areas of the Bentleigh East NAC.

4.1.1 DESIGN OBJECTIVES

- To support a new mid-rise scale built form character for the centre with lower built form at the street interface and to adjoining residential areas.
- To enhance the intersection of Centre Road and East Boundary Road with high quality and prominent built form.
- To provide a more cohesive built form character and improved street interface along the western end of Centre Road.
- To ensure development enhances the pedestrian experience through improved activation at ground floor and maintaining sunlight to the Centre Road and East Boundary Road footpaths.
- To ensure development protects existing residential amenity and does not overwhelm adjoining residential properties.

4.1.2 DESIGN REQUIREMENTS

Building heights

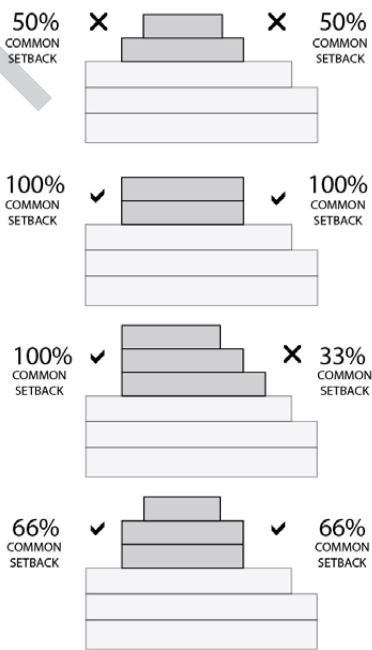
- The preferred maximum building height excludes rooftop services which should be hidden from view from any adjoining public space or designed as architectural roof top features. Roof top services includes but is not limited to plant rooms, air conditioning, lift overruns and roof mounted equipment.
- Buildings should be designed with a floor-to-floor dimension of a minimum of 4.0 metres at ground floor to enable adaptation for other uses in the future.

Building separation

- Where development shares a common boundary, upper level development should:
 - Be setback a minimum of 4.5m from the common boundary, where a habitable window or balcony is proposed.
 - Be setback a minimum of 3.0m from the common boundary where a commercial or non-habitable window is proposed.

Building form and design

- Building design should minimise the visual bulk of large buildings through significant breaks and recesses in building massing.
- Development should avoid repetitive stepped building forms by providing a common street and rear setback for a minimum of 65% of the upper levels above street wall. Refer to diagram below:



- Buildings should reflect the existing fine grain pattern (narrow shopfronts) within the shopping strip by incorporating separate ground floor tenancies and vertically and horizontally modulated forms that integrate with the streetscape context.
- Roof forms should be integrated with the overall building façade design.
- Buildings should utilise materials that do not generate glare, and can withstand the effects of weathering.
- All new buildings are to incorporate best practice Environmentally Sustainable Development (ESD) principles. Refer to the Sustainable Design Assessment in the Planning Process (SDAPP) Framework.

Heritage Design

- These requirements apply to properties within an existing or proposed Heritage Overlay or abutting an existing or proposed Heritage Overlay.
- Facade design and details for infill buildings and new buildings should:
 - Be simple and not compete with the elaborate detailing of the adjoining heritage buildings.
 - Respect the vertical proportions of the heritage streetscape and adjoining heritage buildings.
 - Avoid large expanses of glazing except for ground floor shopfronts.
 - Adaptation and reuse of heritage buildings should:
 - Maintain existing openings and avoid highly reflective glazing in historic openings.
 - Encourage the retention of solid built form behind retained facades and avoid balconies behind existing openings.
 - Maintain the inter-floor height of the existing building and avoid new floor plates and walls cutting through historic openings.
 - Development above the street wall on land within a Heritage Overlay or adjoining a Heritage Overlay should:
 - Be visually recessive and not dominate the heritage building and streetscape.
 - Utilise materials and finishes that are recessive in texture and colour.
 - Incorporate simple architectural detailing that does not detract from the heritage buildings and streetscape.
 - Reflect the fine-grain pattern of subdivision of the streetscape.

Street interface

- Provide a minimum of 80% of the building façade at ground floor level with a visually permeable façade incorporating windows and door openings with clear glazing to ‘Key Public Realm Areas’.
- Buildings on corner sites should be designed to actively address both frontages at street level.
- Upper levels of buildings should be designed to provide habitable rooms with windows or balconies that overlook the public realm.
- Buildings fronting laneways should be designed for passive surveillance with a permeable façade, including windows and door openings.
- Ensure car parking is not visible from the street by incorporating design measures such as sleeving, or alternatively providing as basement parking.

Residential Interface

- Development should provide upper level setbacks in accordance with the precinct specific setbacks (refer to sections 4.3 to 4.5).
- Development must satisfy the objectives and standards of Clauses, 54,55 or 58, which aim to protect the amenity of adjoining residential areas.

Weather protection

- Provide verandahs on all buildings located in the ‘Weather Protection Areas’.
- Verandahs should be at an appropriate height above the footpath to avoid damage whilst still providing effective weather protection, generally between 3.0 and 4.0m and consistent with adjoining sites.
- Verandahs should be designed to mitigate the potential for visual clutter effects from light fittings, service cables and under awning signage.

Access and services

- Pedestrian entries to buildings should be clearly visible and easily identifiable from the street and accessible for all abilities.
- Residential entries should be distinguished from retail and commercial entries.
- Loading, service access and car park access should be provided from laneways and secondary streets. Where this not possible, vehicle crossovers should be minimised to reduce disruption to the footpaths and active frontages and located to avoid street trees if present.
- Provide appropriate setbacks at the rear of the building to laneways ensure adequate space for car park access and servicing. Further details at Clause 52.06 of the Glen Eira Planning Scheme.
- Screen air conditioning services, antennas and other utilities from public view using balcony treatments / roof structures / architectural elements. Avoid using walls to screen services.
- Avoid and minimise building services and utilities at ground floor street frontages to prioritise active frontages at these locations. Integrate services and utilities with the building design.
- Waste storage, loading and recycling facilities should be screened from public view. They should be easily accessed by residents and well ventilated.

Landscaping

- Communal garden spaces should be provided at podium and rooftop levels where appropriate to create amenity for residents, workers and visitors. The gardens should take into consideration, aspect, materials and solar orientation.
- Large development sites are encouraged to contribute to improved pedestrian amenity of the centre by providing for new private/public pedestrian/cycling links through the centre, where sites present an appropriate and useful opportunity to do so.

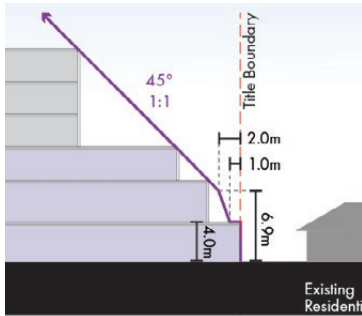
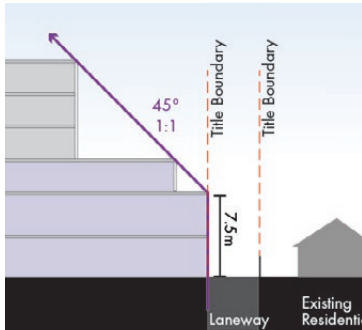
4.3 PRECINCT 1 - EASTERN JUNCTION

4.3.1 PRECINCT OVERVIEW

The Eastern Junction will provide for a high quality entry into the Bentleigh East NAC with development of up to six storeys marking the key corners.

Larger sites will support the transformation of this precinct providing substantial redevelopment opportunities.

4.3.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 6 storeys (21.0m)	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m)	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire eastern footpath of East Boundary Road from 12pm - 2pm on September 22Ensure solar access is maintained to the entire western footpath of East Boundary Road from 10am - 12pm on September 22Ensure solar access is maintained to the entire southern footpath of Centre Road from 10am on September 22 Activation <ul style="list-style-type: none">Development at 2-8 and 10-16 Heather Street should provide ground level activation to the existing laneway
			
			

4.3.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 1 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.1:

- The southern footpath along Centre Road and the Eastern and Western Footpaths of East Boundary Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to footpaths during the specified periods. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired mid-rise scale of the centre will be achieved with a maximum building height of 6 storeys. The proposed heights in this precinct will help to emphasise this important junction at the eastern entry into the NAC. The sites are generally large and have limited residential interfaces, which would allow the heights to be achieved with minimal amenity impacts. The proposed height also reflects the heights of recent development on the eastern side of East Boundary Road. The long cross section shown in Figure 22 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

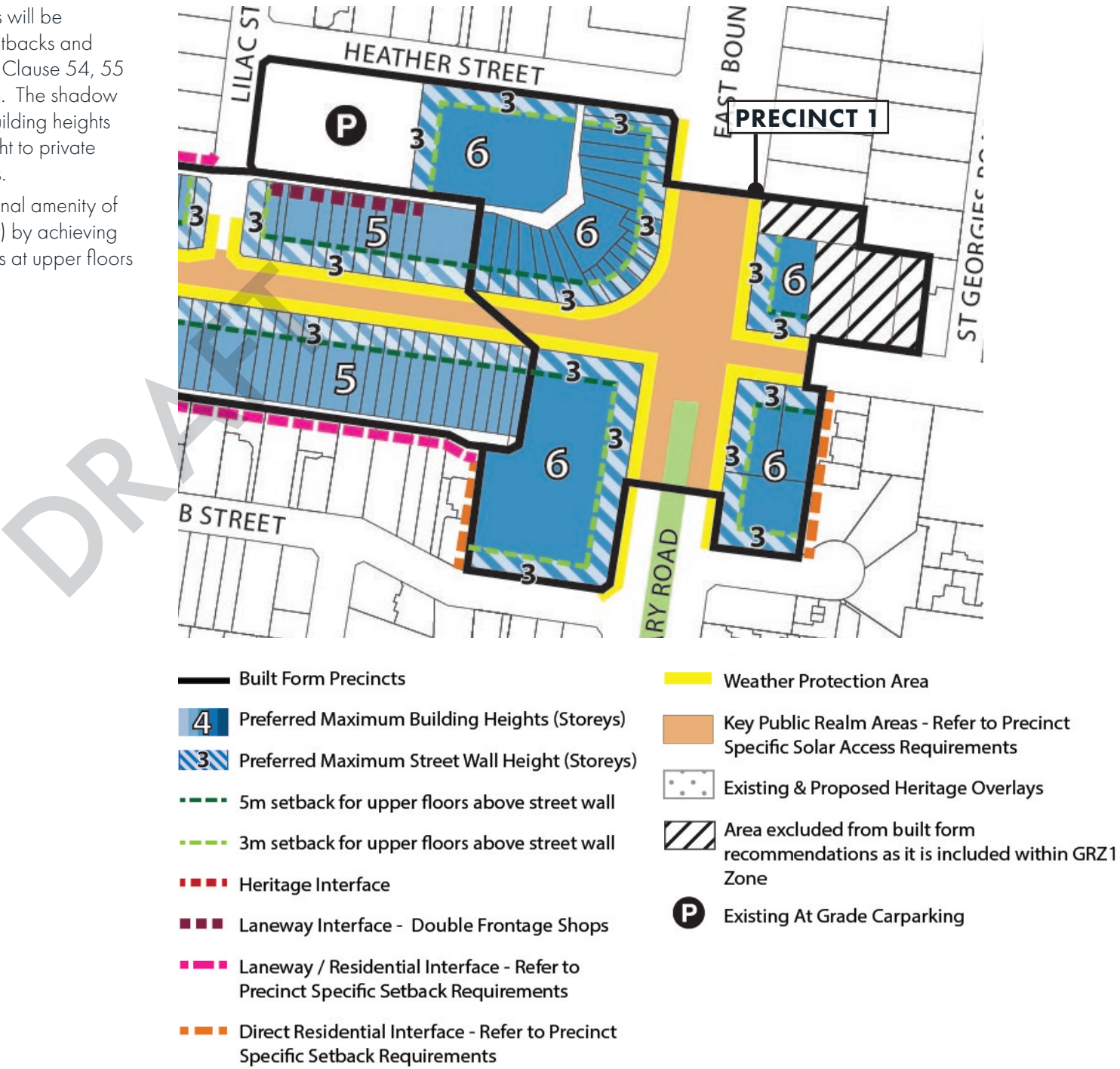


Figure 20. Built Form Framework - Precinct 1

4.3.4 CROSS SECTIONS



Figure 23. Precinct 1 - Cross Section Key Plan

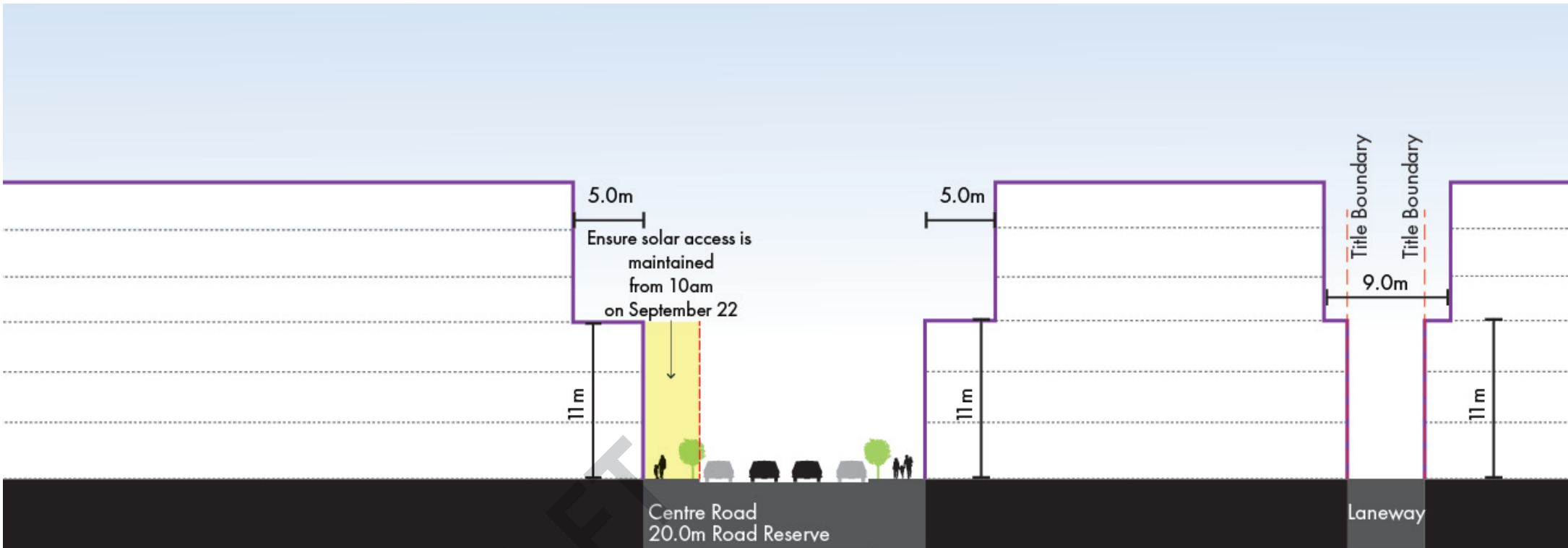


Figure 21. Precinct 1 - Cross Section A

- Maximum Building Envelope
- Potential building envelope which could be achieved under the Built Form Framework - Not illustrative of a proposed development outcome or design
- Maintain Solar Access to footpath

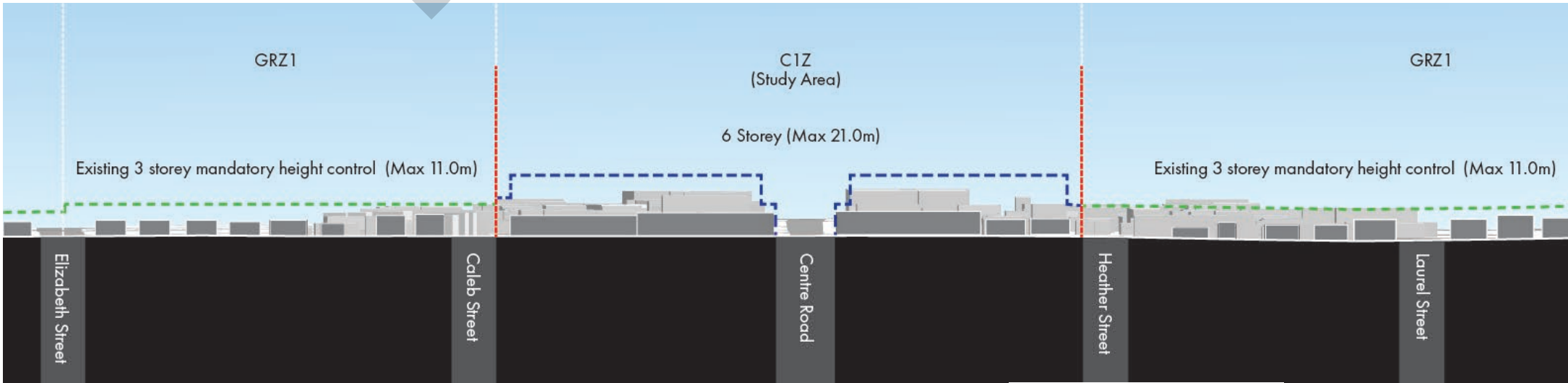


Figure 22. Precinct 1 - Cross Section B

- Study Area Boundary
- Planning Scheme Zoning Boundary
- Existing Built Form
- Existing Maximum Built Form Height
- Proposed Maximum Built Form Height

4.3.5 SKETCH VISUALISATION



Figure 24. Viewpoint 1 - Existing Conditions

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.



Figure 25. Viewpoint 1 Location Plan



Figure 26. Viewpoint 1 - With examples of supported street wall and building height shown

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows prominent built form to the corner of Centre Road and East Boundary Road. The three storey street wall wraps the curved corner and extends along Centre Road integrating with a number of two storey shopfronts.

Development of five storeys in Precinct 2 is also visible from this view.

4.3.6 PRECINCT 1 - SHADOW ANALYSIS

Figure 27 demonstrates the shadows cast by existing buildings within Precinct 1 of the Bentleigh East NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



10am - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 27. Precinct 1 - Shadow Analysis

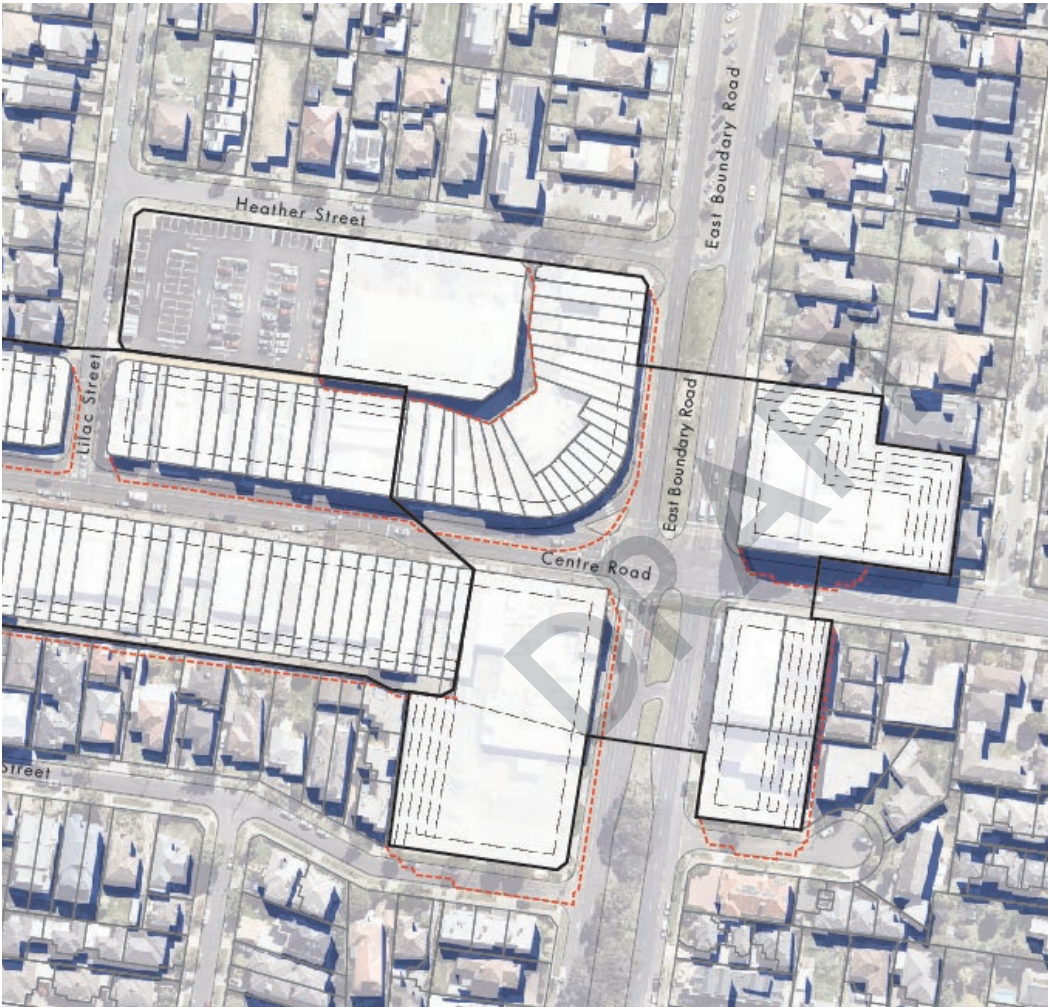
11am - Sept 22



12pm - Sept 22



1pm - Sept 22



2pm - Sept 22





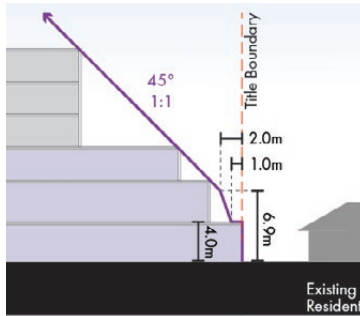
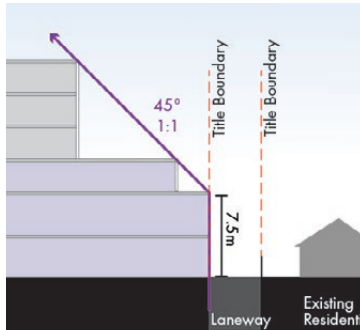
DRAFT

4.4 PRECINCT 2 - CENTRE ROAD RETAIL

4.4.1 PRECINCT OVERVIEW

The Centre Road Retail precinct will continue to transform with development of up to five storeys. This scale allows for a sensitive transition to adjoining residential areas in the south.

4.4.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 5 storeys (18.0m)	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m)	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Centre Road from 10am on September 22 Activation <ul style="list-style-type: none">Development at 2-8 and 10-16 Heather Street should provide ground level activation to the existing laneway Heritage <ul style="list-style-type: none">Development on land at 675-677 Centre Road should respect the heritage protected East Bentleigh Hall
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below:	
			

4.4.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 2 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.1:

- The southern footpath along Centre Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to footpath from 10am on September 22. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired mid rise scale of the centre will be achieved with a maximum building height of 5 storeys. This height supports additional intensification and development in the centre which is consistent with the planning policy context outlined in Part 1 of this report. It also reflects the height of recent development within the Precinct. The proposed height is slightly lower than the proposed heights for Precinct 1, which is largely due to the smaller sites. Setbacks of 5m for development above 3 storeys will ensure the street wall is dominant in the streetscape when viewed from the opposite footpath. The long cross section shown in Figure 32 and 33 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

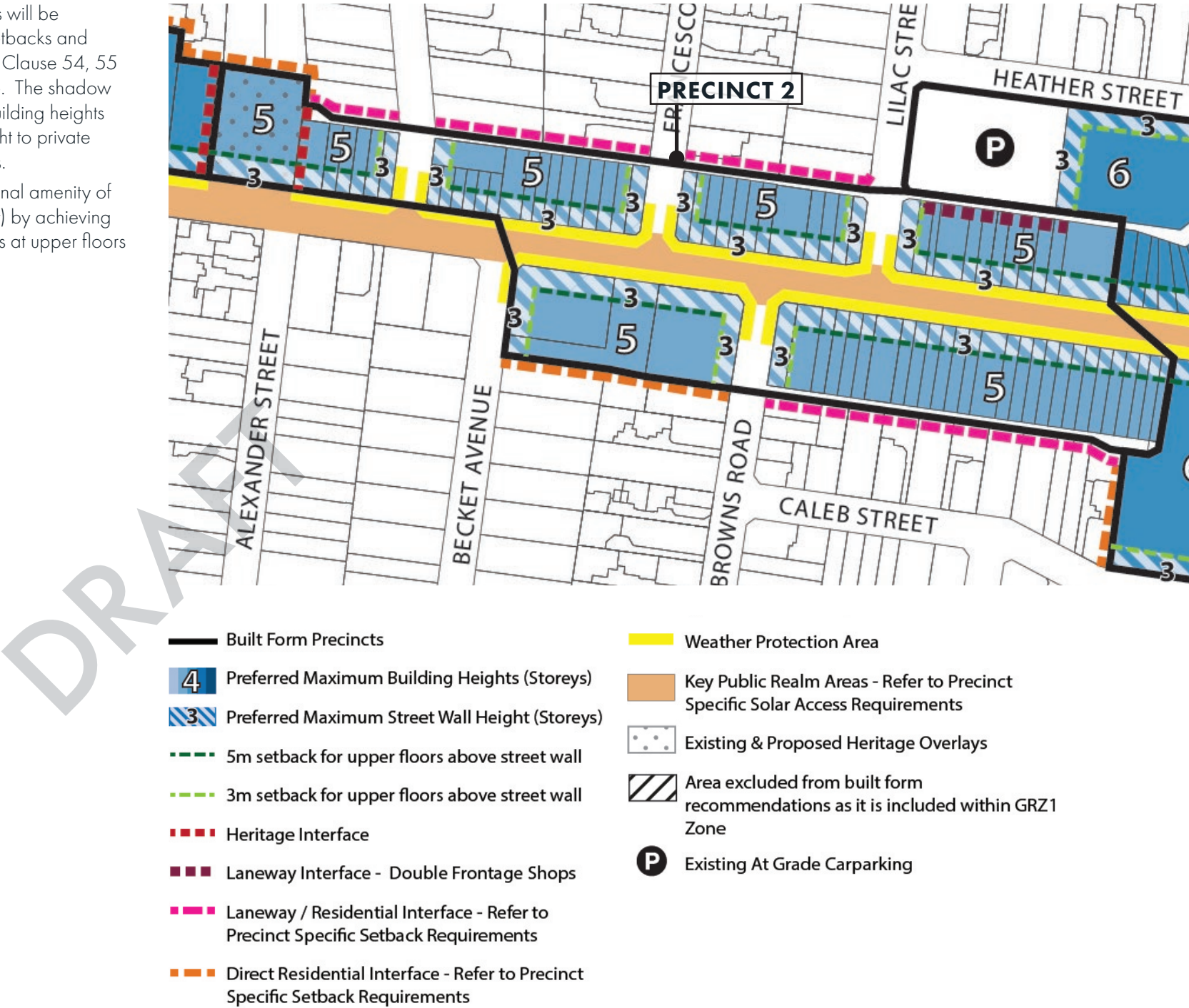


Figure 28. Built Form Framework - Precinct 2

4.4.4 CROSS SECTIONS

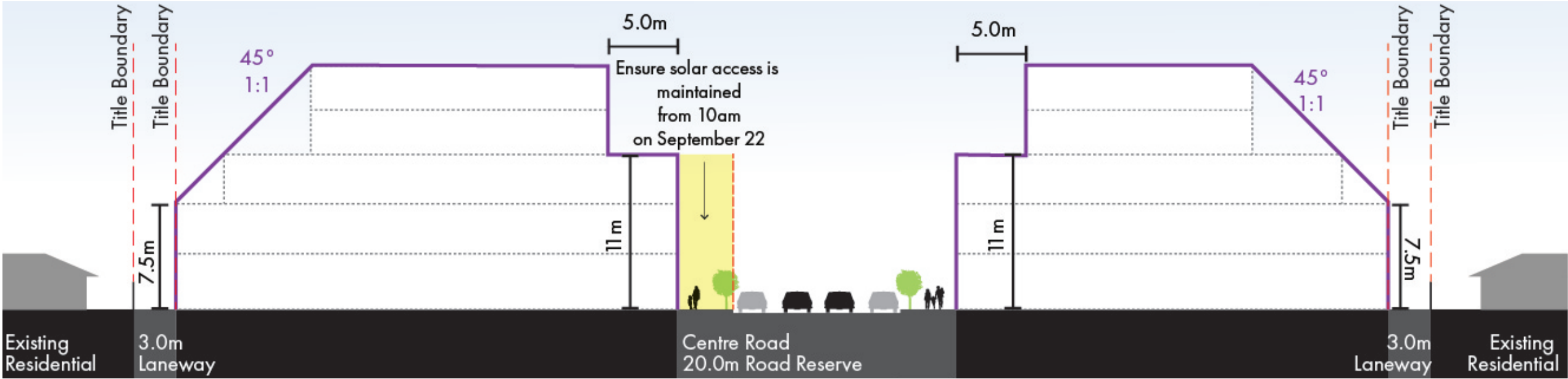


Figure 29. Precinct 2 - Cross Section A

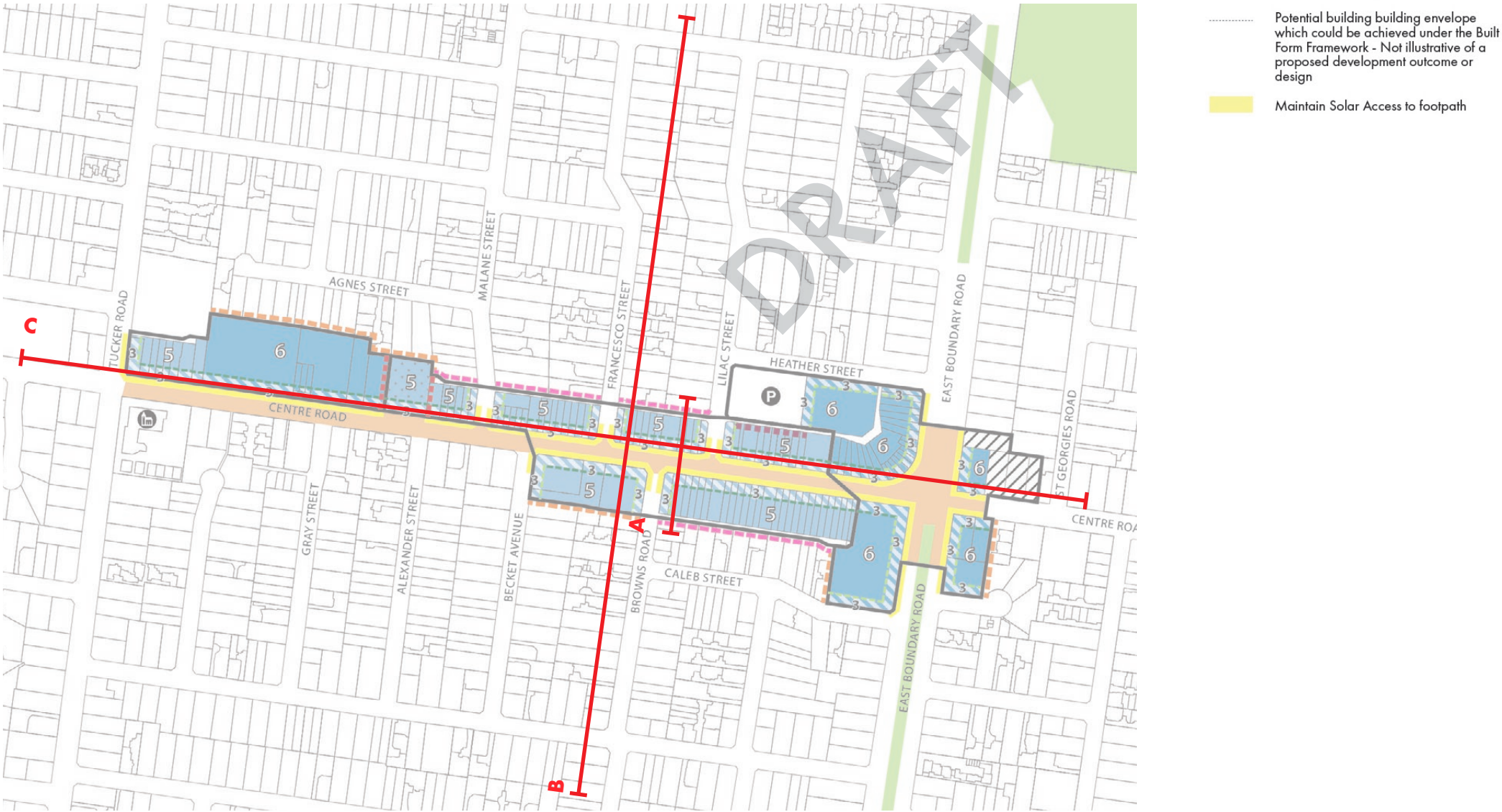


Figure 30. Precinct 1 - Cross Section Key Plan

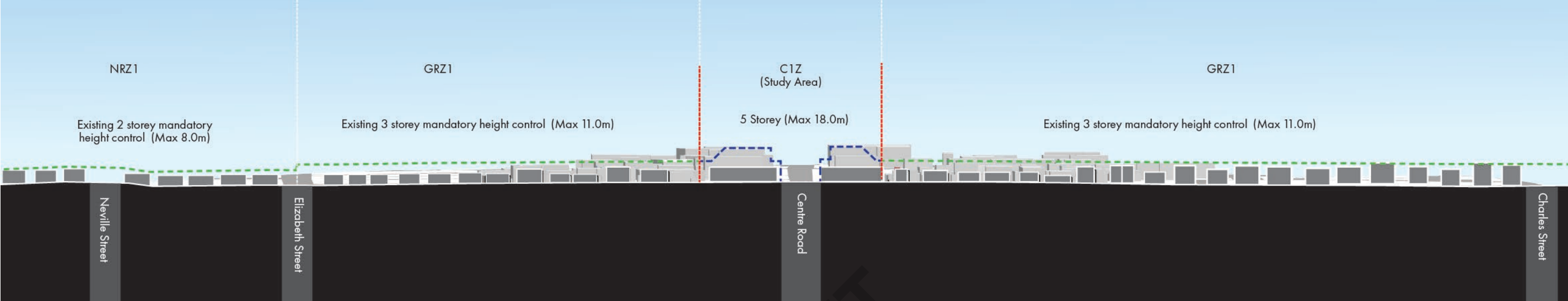


Figure 32. Precinct 2 - Cross Section B

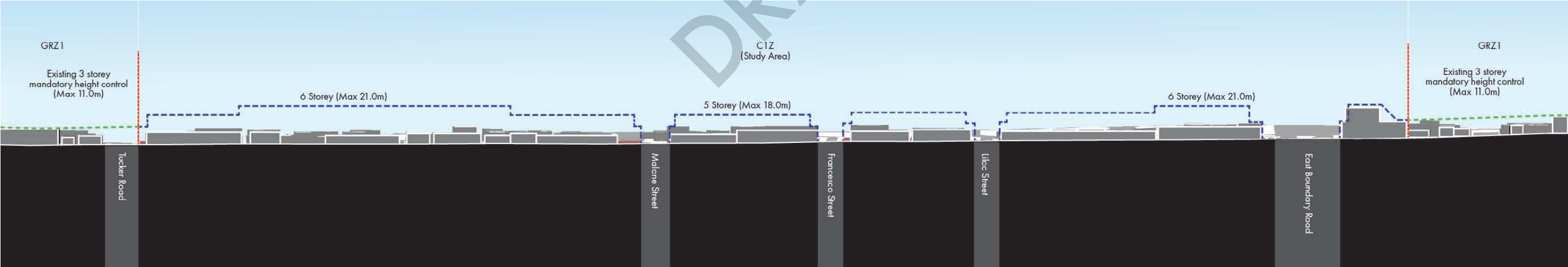


Figure 33. Precinct 2 - Cross Section C

- Study Area Boundary
- Existing Maximum Built Form Height
- Planning Scheme Zoning Boundary
- Proposed Maximum Built Form Height
- Existing Built Form

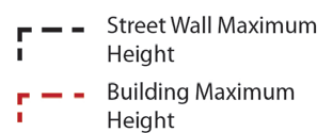
4.4.5 SKETCH VISUALISATION



Figure 34. Viewpoint 2 - Existing Conditions



Figure 35. Viewpoint 2 - With examples of supported street wall and building height shown



N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

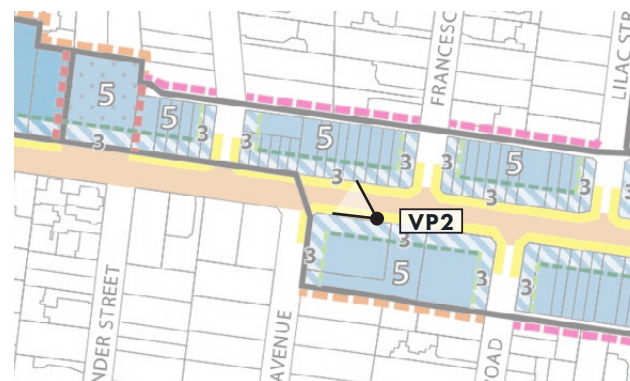


Figure 36. Viewpoint 2 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the single storey street wall will change significantly however the proposed three storey scale will not overwhelm the streetscape. The levels four and five are recessed allowing the street wall to be the dominant element.

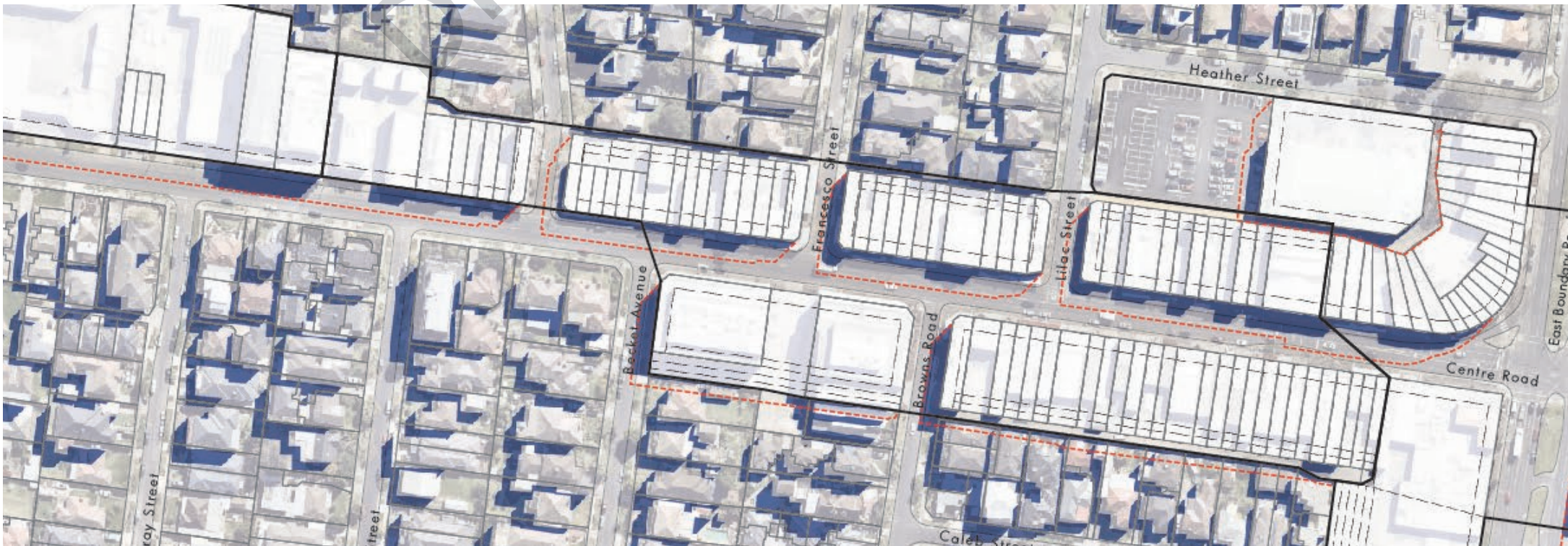
4.4.6 PRECINCT 2 - SHADOW ANALYSIS

Figure 37 demonstrates the shadows cast by existing buildings within the Precinct 2 of the Bentleigh East NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



10am - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 37. Precinct 2 - Shadow Analysis

11am - Sept 22



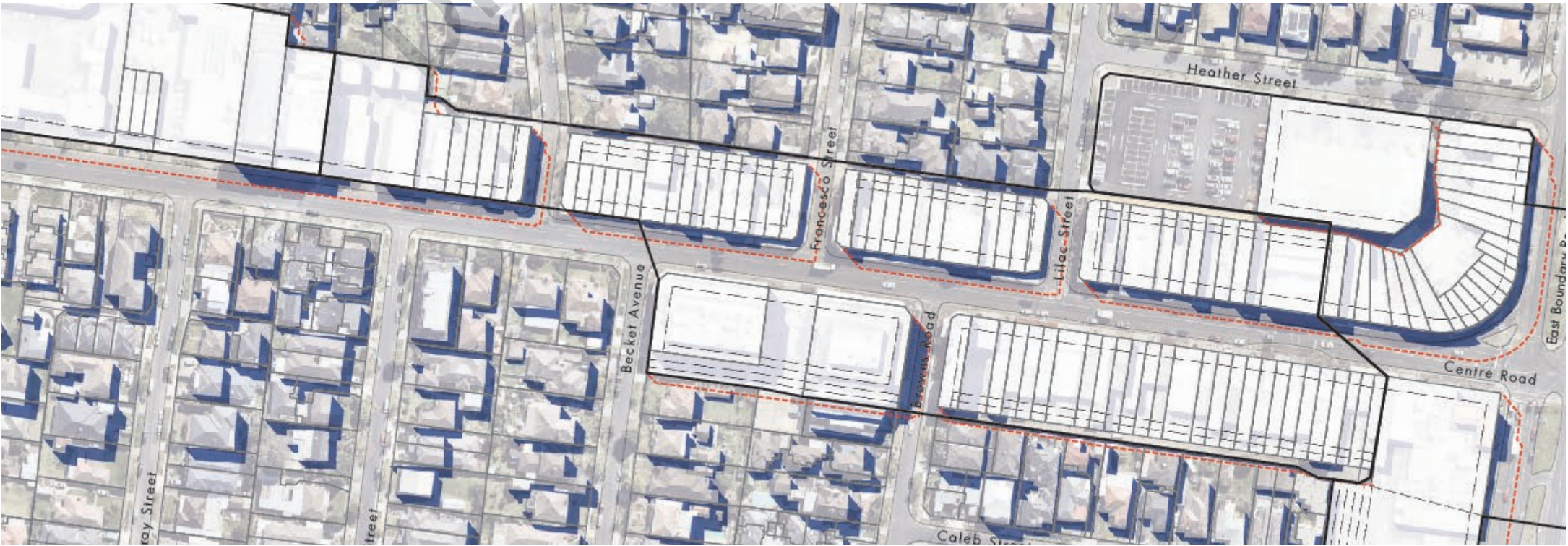
12pm - Sept 22

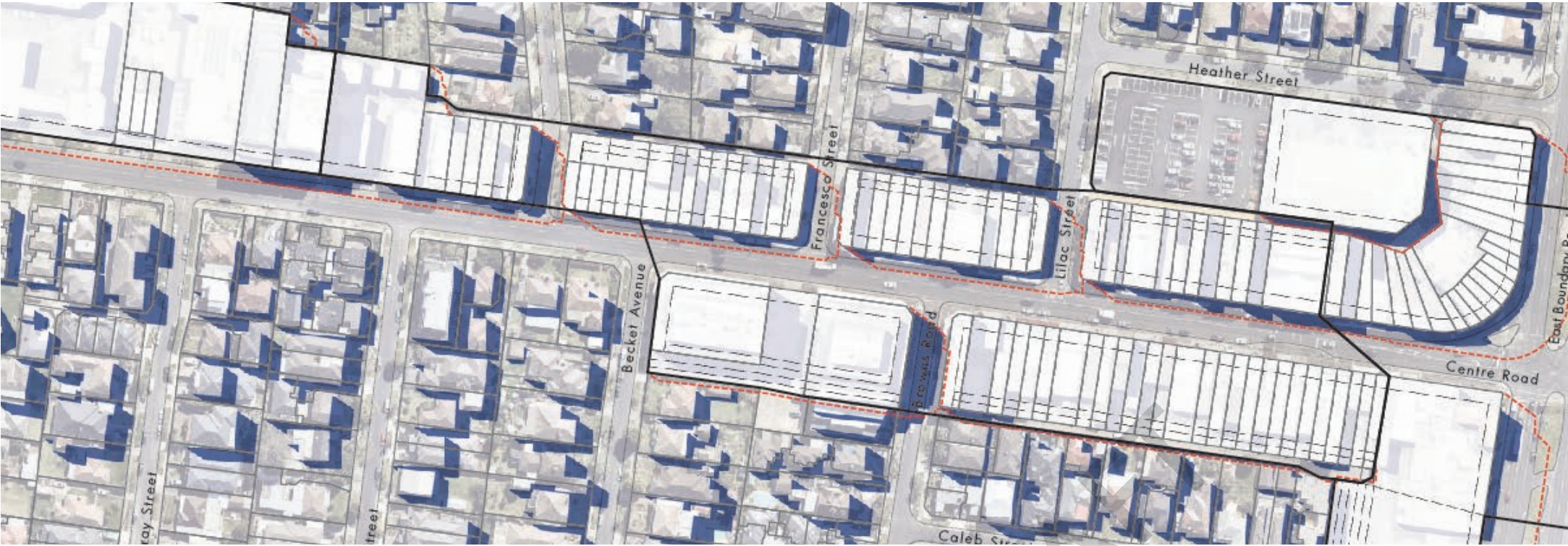


1pm - Sept 22



2pm - Sept 22





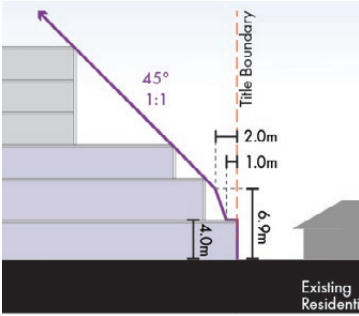
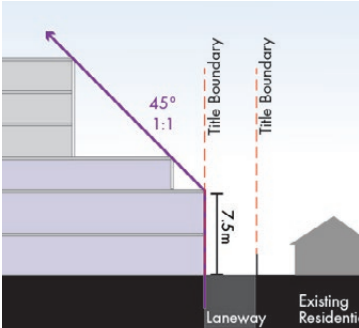
4.5 PRECINCT 3 - CENTRE ROAD WEST

4.5.1 RECOMMENDATIONS

The large sites within the peripheral retail precinct will support development of up to six storeys and provide an opportunity to create a continuous retail experience along Centre Road.

Properties at the western end of the strip will support development of up to five storeys and mark the entry to the NAC with high quality buildings.

4.5.2 BUILT FORM REQUIREMENTS

Building height	Street Wall	Setbacks	Specific Requirements
Preferred Maximum Heights <ul style="list-style-type: none">Up to 5 storeys (18.0m) for land at 619-639 Centre Road.Up to 6 storeys (21.0m) elsewhere	Preferred Street wall Height <ul style="list-style-type: none">Up to 3 storeys (11.0m)	Preferred Setbacks <ul style="list-style-type: none">Zero front and side setbacks for development up to height of the street wall.5.0m front setback for development above the street wall.3.0m setback to the secondary street frontage for development above the street wall on corner sites.Development directly abutting a residential zone to the rear or side should provide setbacks in accordance with the diagram below:	Solar Access <ul style="list-style-type: none">Ensure solar access is maintained to the entire southern footpath of Centre Road from 10am on September 22 Heritage <ul style="list-style-type: none">Development on land at 669-673 Centre Road should respect the heritage protected East Bentleigh Hall
			
		<ul style="list-style-type: none">Development that abuts a laneway directly abutting a residential zone should provide setbacks in accordance with the diagram below: 	

4.5.3 STRATEGIC JUSTIFICATION/RATIONALE

The following outlines how the proposed Built Form Framework for Precinct 3 addresses the issues and opportunities identified in Part 1 and achieves the Design Principles in Section 3.1:

- The southern footpath along Centre Road will be protected from excessive overshadowing through the proposed height limits. The shadow analysis demonstrates that sunlight will be maintained to footpath from 10am on September 22. This will support the role of the footpath as a key public space at times where it is likely to be more active.
- The desired mid rise scale of the centre will be achieved with maximum building heights of 5-6 storeys. These height supports additional intensification and development in the centre which is consistent with the planning policy context outlined in Part 1 of this report. A 6 storey height limit is proposed for larger sites within the precinct as they could support this height and transition appropriately to the adjoining residential areas. Residential areas are also located to the north which means there will be no shadow impacts from new development. A slightly lower 5 storey height limit has been applied to sites at the western end of the centre because of their shallower depths. Setbacks of 5m for development above 3 storeys will ensure the street wall is dominant in the streetscape when viewed from the opposite footpath. The long cross section shown in Figure 33 demonstrates that the proposed building scale transitions appropriately to the surrounding General Residential Zoned areas, which has a maximum allowable building height of 3 storeys.
- The human scale of the streetscape will be maintained by providing a street wall of 3 storeys (11.0m). This will be combined with requirements for vertical and horizontal articulation to ensure the bulk of the street wall is minimised. Although this height is taller than existing one and two storey shopfronts, it is a scale that will not overwhelm the streetscape as demonstrated by recently constructed examples in the centre.

- The amenity of adjoining residential areas will be protected by applying rear upper level setbacks and applying additional requirements through Clause 54, 55 and 58 of the Glen Eira Planning Scheme. The shadow testing demonstrates that the proposed building heights can be achieved whilst maintaining sunlight to private open space to adjoining residential areas.
- The built form framework will support internal amenity of dwellings (outlook and access to daylight) by achieving a minimum building separation of 9 metres at upper floors (using a 4.5m upper floor setback).

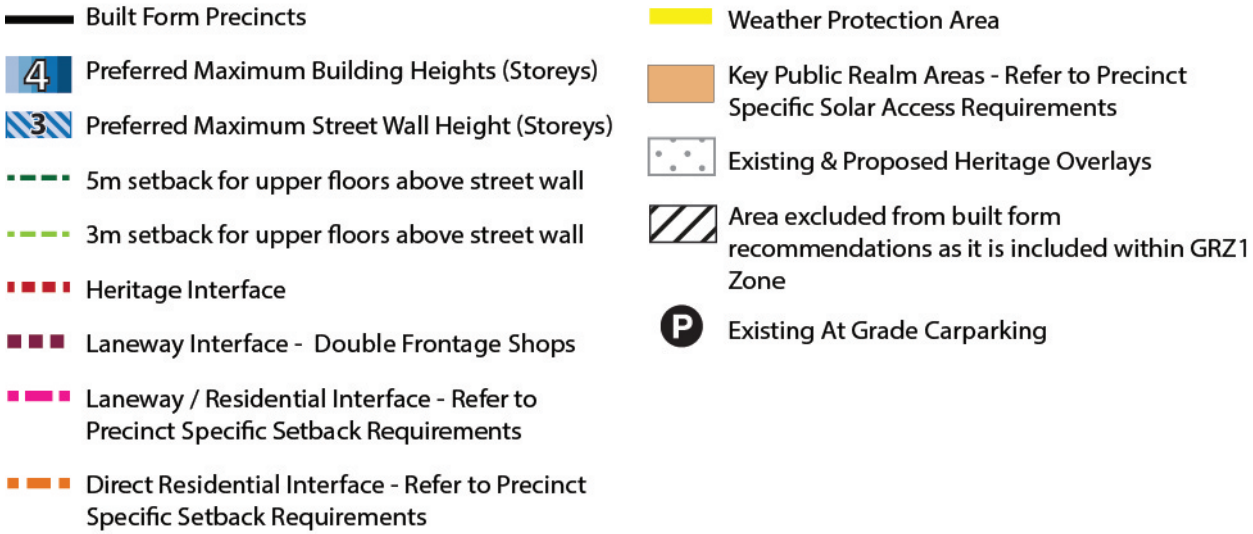
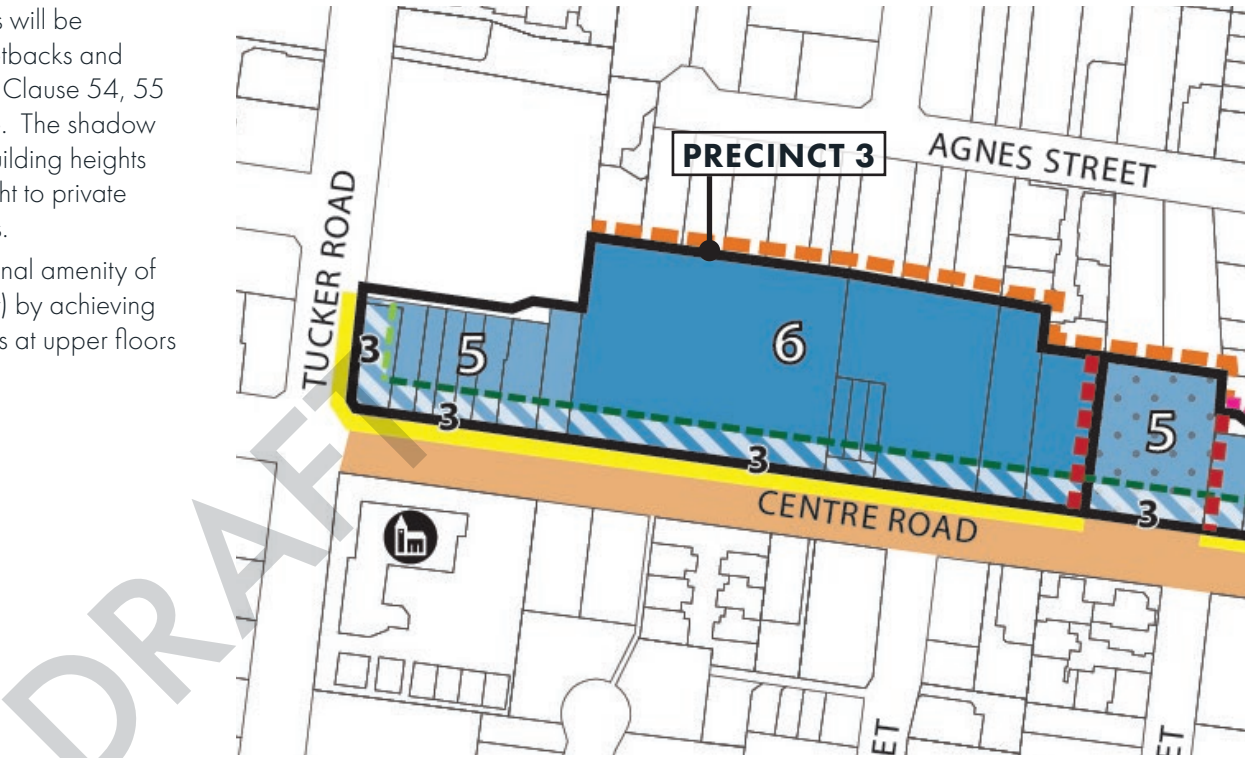


Figure 39. Built Form Framework - Precinct 3

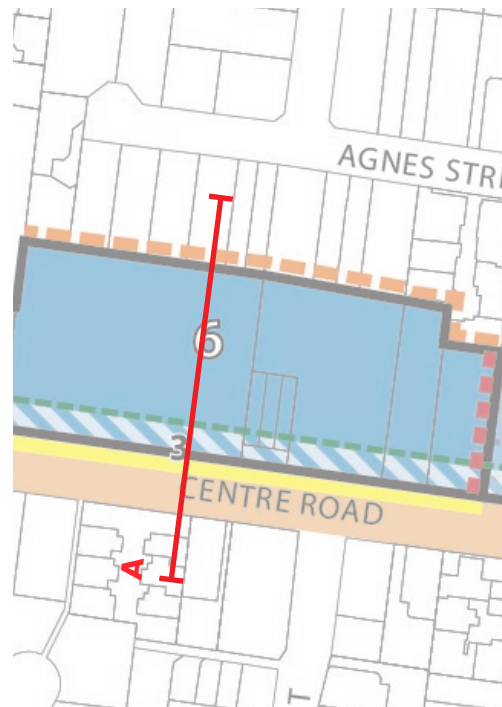


Figure 40. Precinct 3 - Cross Section Key Plan

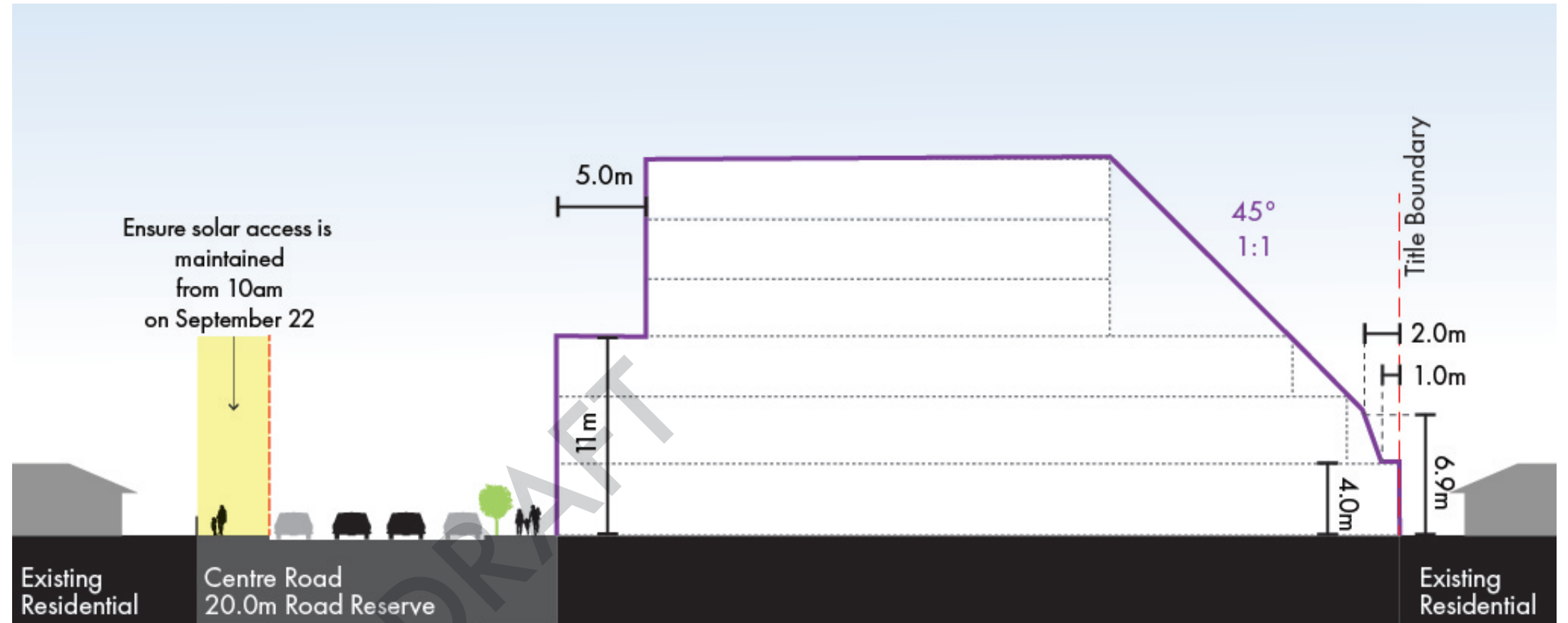


Figure 41. Precinct 3 - Cross Section A

- Maximum Building Envelope
- Potential building building envelope which could be achieved under the Built Form Framework - Not illustrative of a proposed development outcome or design
- Maintain Solar Access to footpath

4.5.5 SKETCH VISUALISATION



Figure 42. Viewpoint 3 - Existing Conditions



Figure 43. Viewpoint 3 - With examples of supported street wall and building height shown

- Street Wall Maximum Height
- Building Maximum Height

N.B. The Sketch above shows potential building envelopes which could be achieved under the Built Form Framework. It is not illustrative of a proposed development outcome or design.

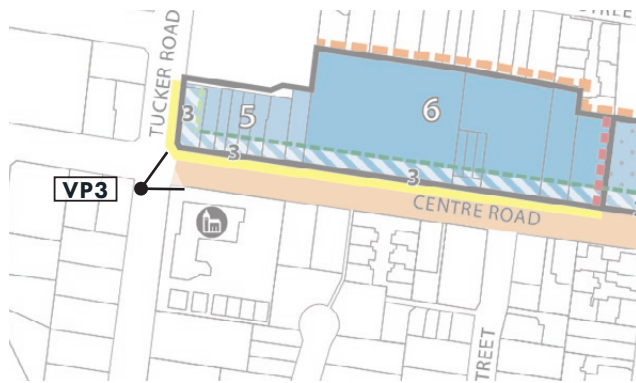


Figure 44. Viewpoint 3 Location Plan

The sketch visualisation depicts a potential built form outcome within the streetscape along with an outline of the potential maximum building envelope for both the street wall and overall building.

The visualisation shows that the proposed three storey street wall creates a more cohesive streetscape. The additional upper levels are recessed and allow the street wall to be the dominant element.

4.5.6 PRECINCT 3 - SHADOW ANALYSIS

Figure 45 demonstrates the shadows cast by existing buildings within Precinct 3 of the Bentleigh East NAC and the shadow cast by the maximum building envelope achievable through the Built Form Framework.

9am - Sept 22



11am - Sept 22



10am - Sept 22



12pm - Sept 22



- Precinct Boundary
- Shadow from existing built form
- - - Shadow from proposed maximum building envelope

Figure 45. Precinct 3 - Shadow Analysis

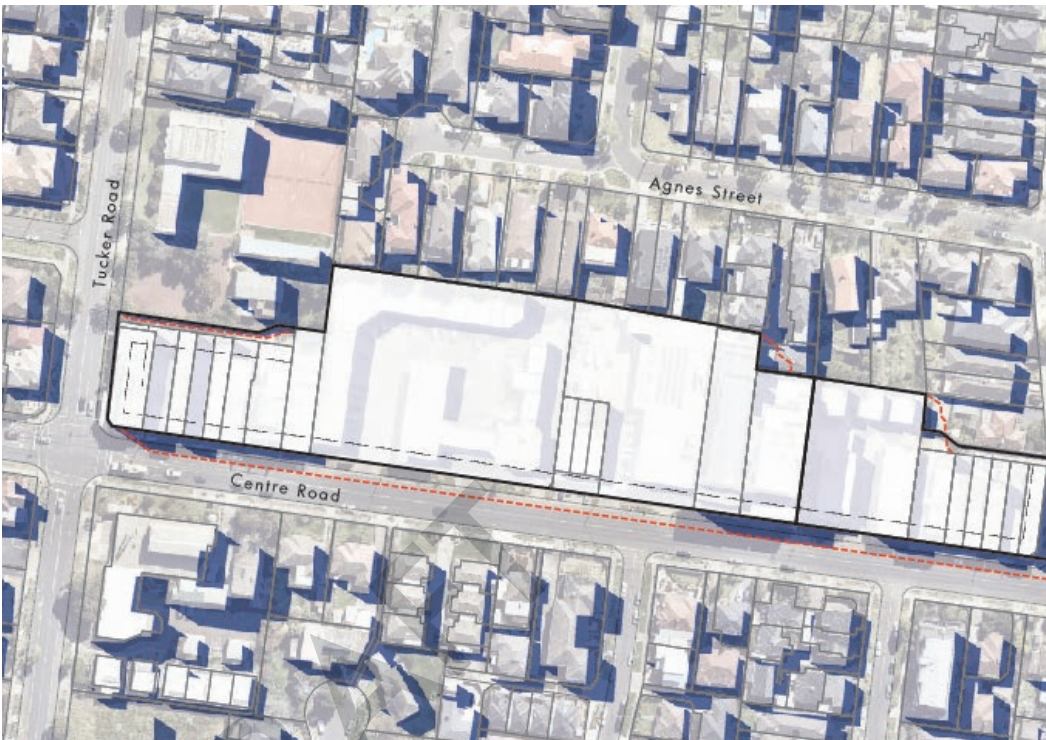
1pm - Sept 22



2pm - Sept 22



3pm - Sept 22



DRAFT