

Caulfield Racecourse Work Package 3& 4

Heritage Impact Statement



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Prepared by

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Melbourne Racing Club

Quality Assurance Register

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Cover image: 1956 aerial photograph of the Caulfield Racecourse
Source: Central Plan Office, Landata, Victorian Land Registry Service

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1.0 INTRODUCTION

This Heritage Impact Statement (HIS) has been prepared on behalf of the Melbourne Racing Club (MRC) to accompany a planning permit application to the City of Glen Eira for the redevelopment of the northern precinct of the Caulfield Racecourse. The works are referenced as Work Package 3 & 4 (WP3 & 4). The project involves the substantial redevelopment and re-presentation of the northern precinct of the racecourse. Key actions of WP3 & 4 include:

- New patron arrival zones and a public forecourt at the centre of the site to improve the access and safety.
- New landscaped network of event and circulation spaces established across the site.
- New horse and racing participant areas centralised in the main race day precinct and providing best practice welfare outcomes.
- Operational and administrative areas including a new administration building, car park and loading areas to be relocated to the western edge of the site to optimise efficiency and minimise patron crossover.
- Upgrades to external interface of the site (including changes and upgrades to the fencing).

The report makes reference to the following drawing packages:

- MRC- Caulfield Northern Precinct Town Planning WP3 Drawing Set, prepared by Hassell, Rev 10, dated 7 June 2021
- MRC -Administration Building Town Planning Drawings Package prepared by Hassell, Rev C, dated 5 March 2021
- MRC Clubhouse Building Town Planning Submission, prepared by Techne, Rev C, dated 20 April 2021
- Boomerang & Heritage Toilet Block, prepared by Hassell, dated 30 March 2021

The following report has been consulted in the preparation of this report:

- Caulfield Racecourse Draft Conservation Management Plan (CMP), Lovell Chen, 2019

1.1 Background

No statutory heritage controls currently apply to the Caulfield Racecourse under the Glen Eira Planning Scheme. Notwithstanding, the complex has been assessed as a place of heritage significance in work undertaken by Lovell Chen for MRC and a Conservation Management Plan (CMP) was prepared in 2019 (Lovell Chen, 2019). The CMP was commissioned to provide MRC with an appropriate basis for considering change at the place consistent with its heritage values, noting the racecourse was found to be of historical, social and aesthetic/architectural significance. The CMP has previously been provided to the City of Glen Eira in draft form.

The CMP includes a series of policies for the place. These are directed at supporting the ongoing viability and operation of the place as one of three major continuously operating nineteenth century racecourses in Melbourne, consistent with its heritage values and having regard for significant elements and fabric. The HIS provides comment on the proposed works having regard for the findings and recommendations of the 2019 CMP.

1.2 Approach

The works for WP3 & 4 would occur across a wide area at the northern end of racecourse and are relatively complex. They would result in a substantial transformation and re-presentation of this area, albeit a change that retains key framework elements, traditional spatial arrangements and heritage buildings and structures.

The works would interface with a number of different elements and areas of heritage value in the CMP and where this occurs, a detailed assessment of impacts on the affected element or area has been

undertaken. Equally, there is a need to consider the broader heritage impact and benefits of the works on the place as a whole.

Reflecting this, and the complexity of the proposal, the approach to presenting the assessment of heritage impacts has been to provide an overall impact assessment against the CMP policies in this heritage impact statement (main report), and to provide the detail of specific impacts and issues for individual elements within the complex separately (refer to Appendix A).

2.0 BRIEF HISTORY AND DESCRIPTION

Caulfield Racecourse is located to the south of Caulfield Railway Station, and south of Normanby Road and Station Street, Caulfield. Kambrook and Booran roads are to the west of the racecourse proper, with Neerim Road to the south, and Queens Avenue to the east. The main entrance, grandstands, administration and racing facilities, and car parks are located in the north of the site, with the racecourse proper, tracks and the central Flat area with lakes taking up the bulk of the remainder of the site. Training and stabling facilities are located to the west on Kambrook and Booran roads and at the south end of the site on Neerim Road.

Caulfield Racecourse was formally established in 1859, and has been subject to ongoing development and change since that time. It is one of three major metropolitan racecourses in Victoria, the others being Flemington Racecourse, established in the 1840s, and managed by the Victoria Racing Club; and Moonee Valley Racecourse, established in 1883, and managed by the Moonee Valley Racing Club. A fourth course is Sandown Racecourse, where racing has been intermittent since the 1880s, but the current racecourse dates from 1965, and is the only metropolitan racecourse to be opened in Victoria in the twentieth century.

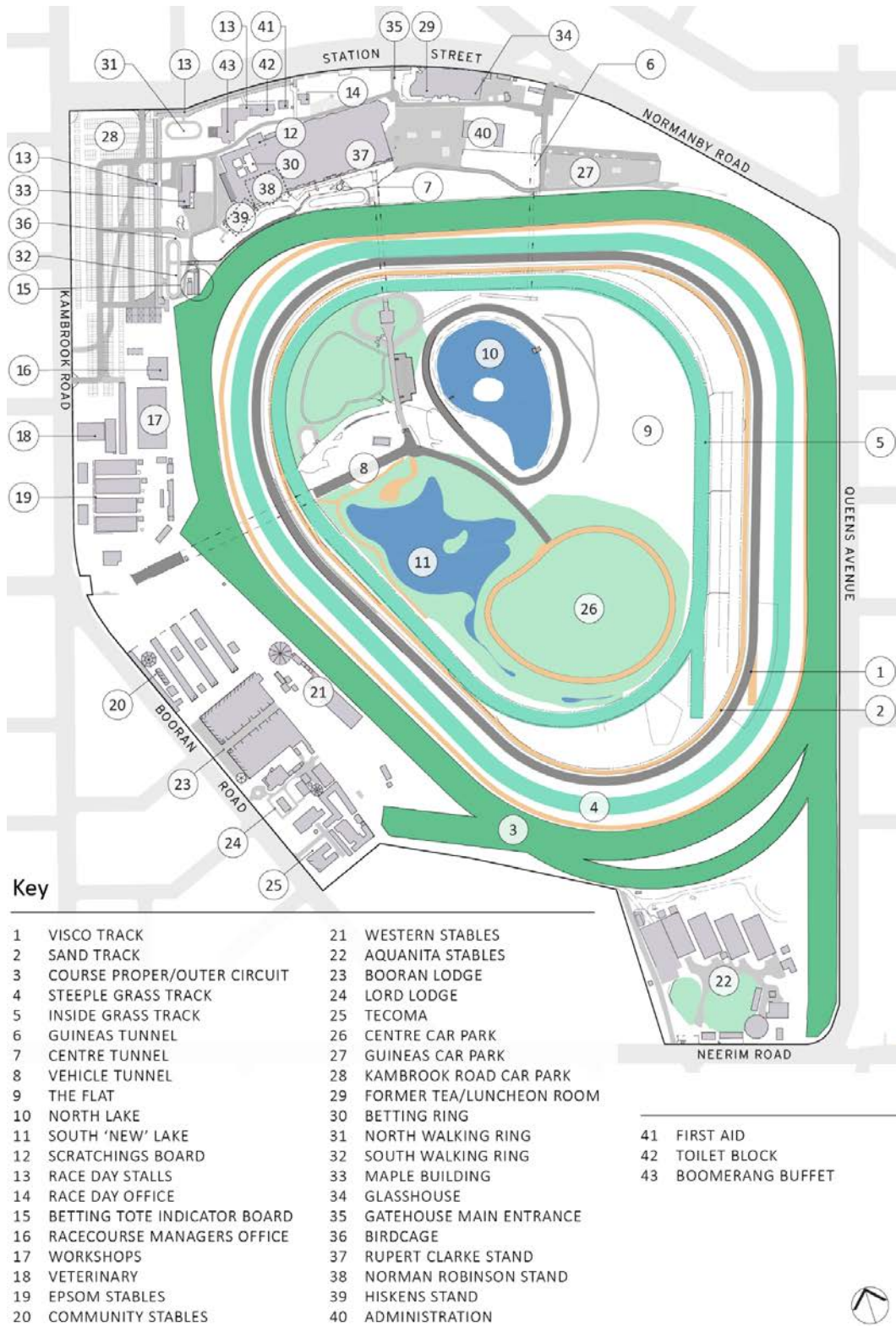


Figure 1 Site plan of existing racecourse
 Source: Caulfield Racecourse CMP, Lovell Chen

3.0 PROPOSED WORKS

The works included in WP3 & 4 include the redevelopment of the northern precinct of the racecourse to provide improved user and operational facilities, including for the public, staff and trainers/jockeys/horses. The redevelopment will transform the northern precinct of the racecourse and co-locate and prioritise issues of horse welfare, safety and integrity and the user experience. The following description provides a summary overview of the works. A more detailed description is included at the assessment at Appendix A.

New horse area

The key outcome of redevelopment is the construction of a new subterranean horse area to the north-western section the racecourse. The new horse area will include race day stalls, walking rings, wash areas, staff areas and storerooms, and these are deliberately visible from the public areas in this part of the racecourse. The horse area will connect, via a tunnel, to a relocated mounting yard to the east of Rupert Clarke Stand. Between the horse area and the mounting yard will be a new underground race day administration and staff area including female and male jockey changerooms and amenities, trainers' and jockeys' lounges and stewards' inquiry room.

A new above ground horse integrity area will be constructed to the west of the horse area and the historic Boomerang Building (c. 1919) which is to be retained and adapted for racecourse use. Float parking and drop off will be provided further to the west, replacing existing horse exercise yards in this area (the mature lemon scented gum in this area will be retained).

The construction of the horse area will result in the demolition of a number of structures in this area, most notably the interwar Toilet Block (c. 1920s) adjacent to the Boomerang Building. The new horse area will also require the removal of a number of mature trees. Existing horse areas including the day stalls and walking/exercise rings will be rendered redundant and are to be removed.

A late 1950s former totalisator and refreshments building (the Maple Building) located in the western part of this precinct will also be demolished to allow for the reconfigured access and parking arrangements to the new horse area.

A full list of the demolished elements and trees identified for removal is provided at in Appendix A.

New entry sequence and fencing

In addition to the construction of the new horse area, the WP3 & 4 project reconfigures and re-presents the entry sequence to the racecourse from the north.

To the east of the historic Main Entrance Gate, a new entry arrangement will be created including a new forecourt space fronting Station Street to the north of the Afternoon Tea Room (1939). This building is to be retained and adapted for a new use, following the demolition of the existing large-scale unsympathetic 'Glasshouse' additions on its northern and eastern sides. The existing brick fence to the east of the existing Glasshouse will partially retained to reference the traditional boundary, with the original 1920s plinth and columns elements to remain and later infill panels demolished. A new, contemporary fence will be constructed on a new angled alignment set into the site behind the existing fence line.

To the west of the Main Entrance Gate, the existing fence line will be maintained with the existing brick elements retained. Some bays of the existing corrugated metal infill panels will be replaced with a new palisade fence treatment to promote permeability. A new entry in the vicinity of the Boomerang Building will also be created.

Other works and WP4

Other key changes included the demolition of the existing circa 1990s administration building and creation of a new events area to the east of the new mounting yard. A new administration building will be constructed to the south-west corner of the northern precinct (WP4).

The proposed new Administration Building will be four storeys and adopts a contemporary design of floor to ceiling glazing divided in a grid by extruded fin elements.

Heritage actions

The WP3 & 4 programme has been developed in consideration of the heritage values of the racecourse and the specific elements in this part of the racecourse. Several key heritage actions are proposed including the conservation and adaptation of the Afternoon Tea Room (Clubhouse) following the demolition of the Glasshouse, the development of an interpretation strategy and the recording of elements to be demolished.

4.0 SIGNIFICANCE & HERITAGE MANAGEMENT

4.1 Significance

The CMP includes a statement of significance for the place as a whole:

What is significant?

Caulfield Racecourse is located to the south of Normanby Road and Station Street, Caulfield. It occupies Crown land managed by Trustees. The earliest horse races were held at the site in the late 1850s, but were intermittent until the racecourse was more formally established by the Victoria Amateur Turf Club (VATC) in 1876. The racecourse has been subject to ongoing development since that time, with the majority of historic buildings concentrated at the northern end of the site where they are the focus of race day operations, members' facilities, and the spectator experience. Buildings and elements of primary heritage significance include the 1919 Boomerang Buffet building; 1928 Gatehouse at the main entrance; sections of the boundary wall to the racecourse (including the east and west delivery gates); 1920s toilet block; 1920s Scratchings Board; 1935 Hiskens Stand; 1939 Afternoon Tea Room (Luncheon Room); 1960s Norman Robinson Stand; and the collection of race day horse stalls (1920s-1950s). The racing and training tracks follow the basic alignment and arrangement as established in the nineteenth century. The landscape includes extensive lawns and plantings, and the 'Flat' with the early north lake in the centre of the racecourse. Trees of significance, also mostly located in the northern part of the racecourse, include an Elm avenue, pair of London Planes and other mature specimen trees.

How is it significant?

Caulfield Racecourse is of historical, social and aesthetic/architectural significance to the state of Victoria.

Why is it significant?

Caulfield Racecourse is of historical significance. It was formally established in 1876 by the VATC, now the MRC, one of the leading nineteenth century racing associations in Victoria. Since 1879, it has been the home of the Caulfield Cup, an internationally renowned 2,400m handicap race, and one of the most important races of Victoria's Spring Racing Carnival. The racecourse is one of three major continuously operating nineteenth century racecourses in Melbourne (together

with Flemington from the 1840s and Moonee Valley from the 1880s), and the second most important after Flemington in terms of the number of feature (premium) race day meetings and Group 1 races. In addition to the Caulfield Cup, Caulfield stages other leading races including the Caulfield Guineas and Blue Diamond Stakes. Caulfield is significant for retaining a collection of racecourse related buildings and elements from the first half of the twentieth century; these form a distinctive and intact grouping of a specific type not seen at Flemington or Moonee Valley. Caulfield is both a racecourse and a Recreation and Public Park Reserve, which has required ongoing public access to the racecourse. The race track is a key historical element of the racecourse, with plans from the late 1850s showing the track in its largely current form and alignment.

Caulfield is of historical significance for its occupation during WWII by the Australian Army, when it was used as a recruiting depot and barracks, primarily as a 'clearing house' for recruits prior to active service. During these years, the Caulfield Cup was run at Flemington.

Historically, the racecourse has influenced the development of the surrounding area. Caulfield maintains an historical relationship with two abutting nineteenth century properties on its western flank, these are Lord Lodge and Tecoma at 30 and 32 Booran Road (respectively built as a horse training complex and residence for a bookmaker). In the post-World War II period, new stabling and training facilities were developed on additional land acquired on the western side of the racecourse. These developments effectively brought the stabling and training operations into the racecourse, and out of the surrounding suburb.

Caulfield Racecourse is of social significance as a long running historic racecourse, and the home of the Caulfield Cup and other leading races in the Melbourne horseracing calendar. It is valued by punters and members of the Victorian racing community, and by members of the Melbourne Racing Club.

Architecturally, Caulfield Racecourse is significant for retaining a collection of comparatively intact racecourse related buildings and elements of some distinction, dating from the first half of the twentieth century, and which demonstrate some of the principal characteristics of a racecourse. These include the 1919 Boomerang Buffet building; 1928 Gatehouse at the main entrance; sections of the boundary wall to the racecourse; 1920s toilet block; 1920s Scratchings Board; 1939 Afternoon Tea Room (Luncheon Room); and the collection of race day horse stalls (1920s-1950s). More generally, the buildings and structures, in their planning and arrangement, demonstrate aspects of the historic layout and function of the racecourse, including the concentration of racecourse-related structures at the northern end where they are the focus of race day operations, members' facilities, and the spectator experience.

Aesthetically, the landscape of the racecourse is also significant. The extensive lawns and tree plantings enhance the setting of the course and historic buildings; augment the race day experience; and provide valued amenity for race goers. The Flat is a large expanse of green which dominates the centre of the racecourse. Individual and row trees also include historic plantings, which provide evidence of the early and evolved landscape character of Caulfield Racecourse.

Elements of primary and individual significance are indicated respectively at Figure 2 and Figure 3 below and these are discussed in more detail in this HIS as relevant to the proposed works.

The primary and secondary significant trees at the site are shown at Figure 5 and Figure 6 with numbers as follows:

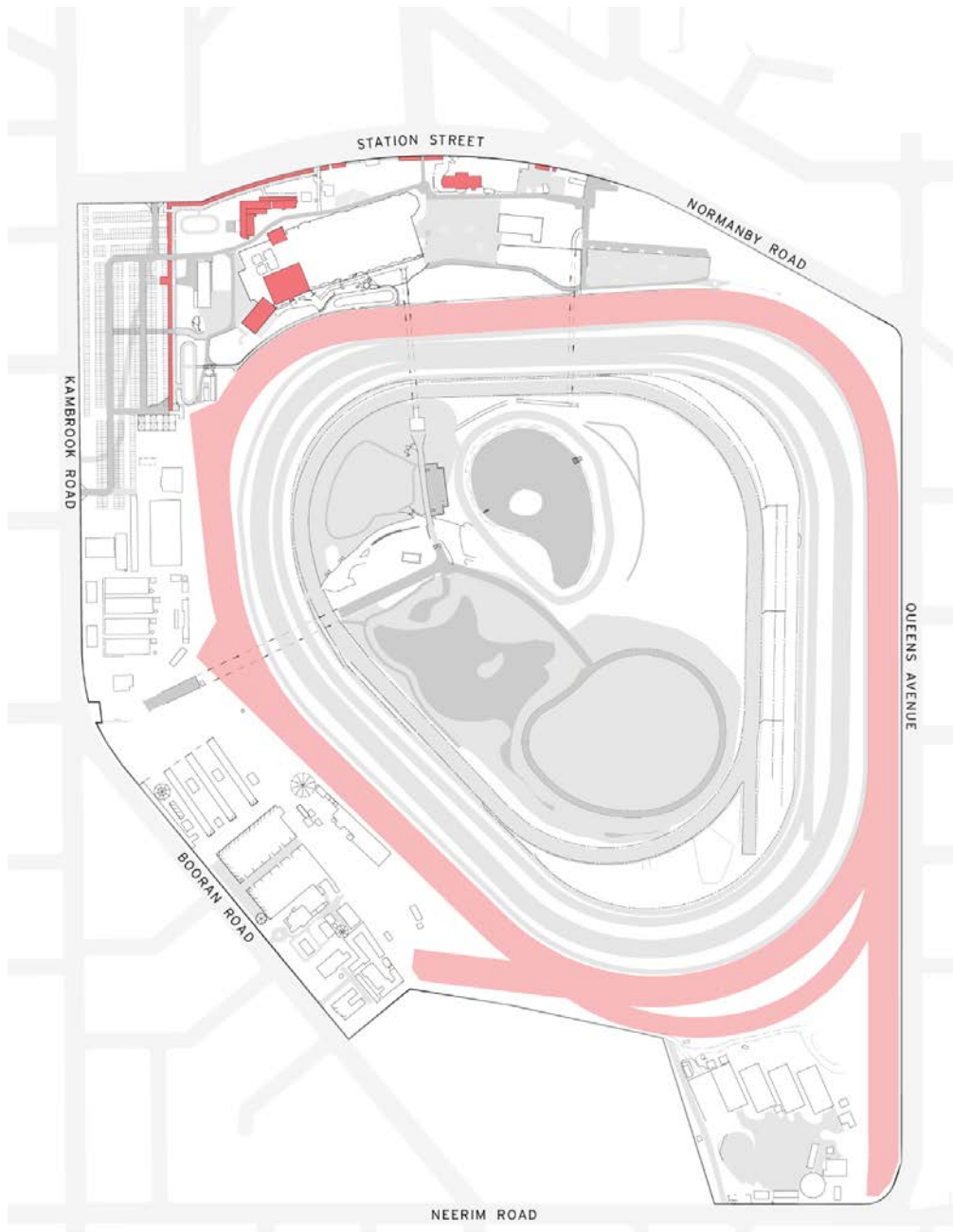
Primary

- Northern elm row - *Ulmus procera*, English Elm (tree nos 1, 3, 4, 5, 6, 7, 8, 9, 10, 19, 20, 21)
- Northern tree plantation - *Cedrus deodara*, Deodar (tree 2, 18, 23 and 24), *Platanus xacerifolia*, London Plane (trees 11, 12, 15 and 16) *Quercus robur*, English Oak (trees 13 and 14).

Secondary

- Individual tree: *Pinus halepensis*, Aleppo Pine (Lone Pine), tree 17.

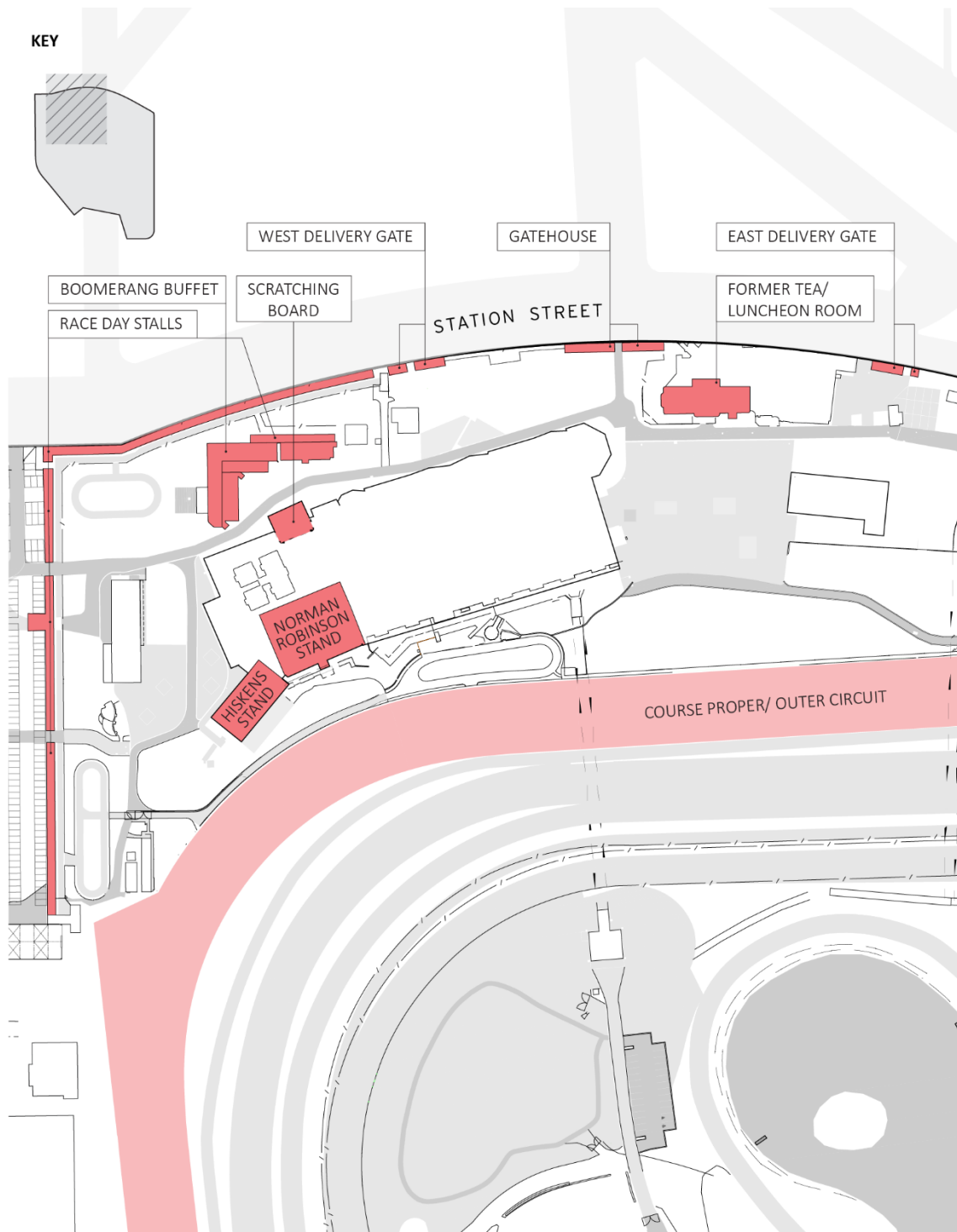
While not identified in the CMP, it is also noted that the existing landscape also includes a number of mature Canary Island Date Palms (*Phoenix canariensis*). The origins of these trees have not been confirmed, although at least one (Tree 36, west of the Boomerang Building) is visible on an interwar oblique photograph. The palms are a typical interwar planting and it is possible the other specimens may date from that period. They contribute to the character of the landscape.



Key Primary heritage significance - Overall site plan

-   Primary structures
-  Primary landscape

Figure 2 Plan illustrating the elements of primary significance; noting that the boundary walls are indicated on separate plans



Key Primary heritage significance - Northern site detail



-  Primary structures
-  Primary landscape

Figure 3 Extract from the above plan, focusing on the northern area of the racecourse
 Source: Caulfield Racecourse CMP

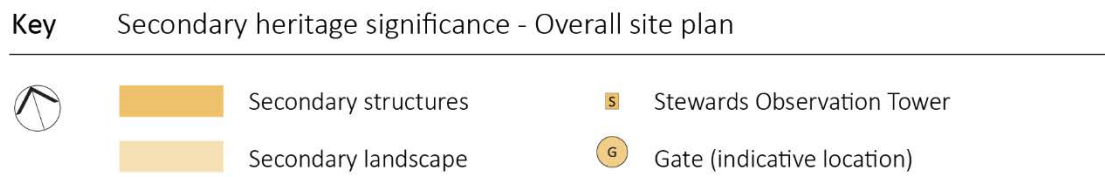
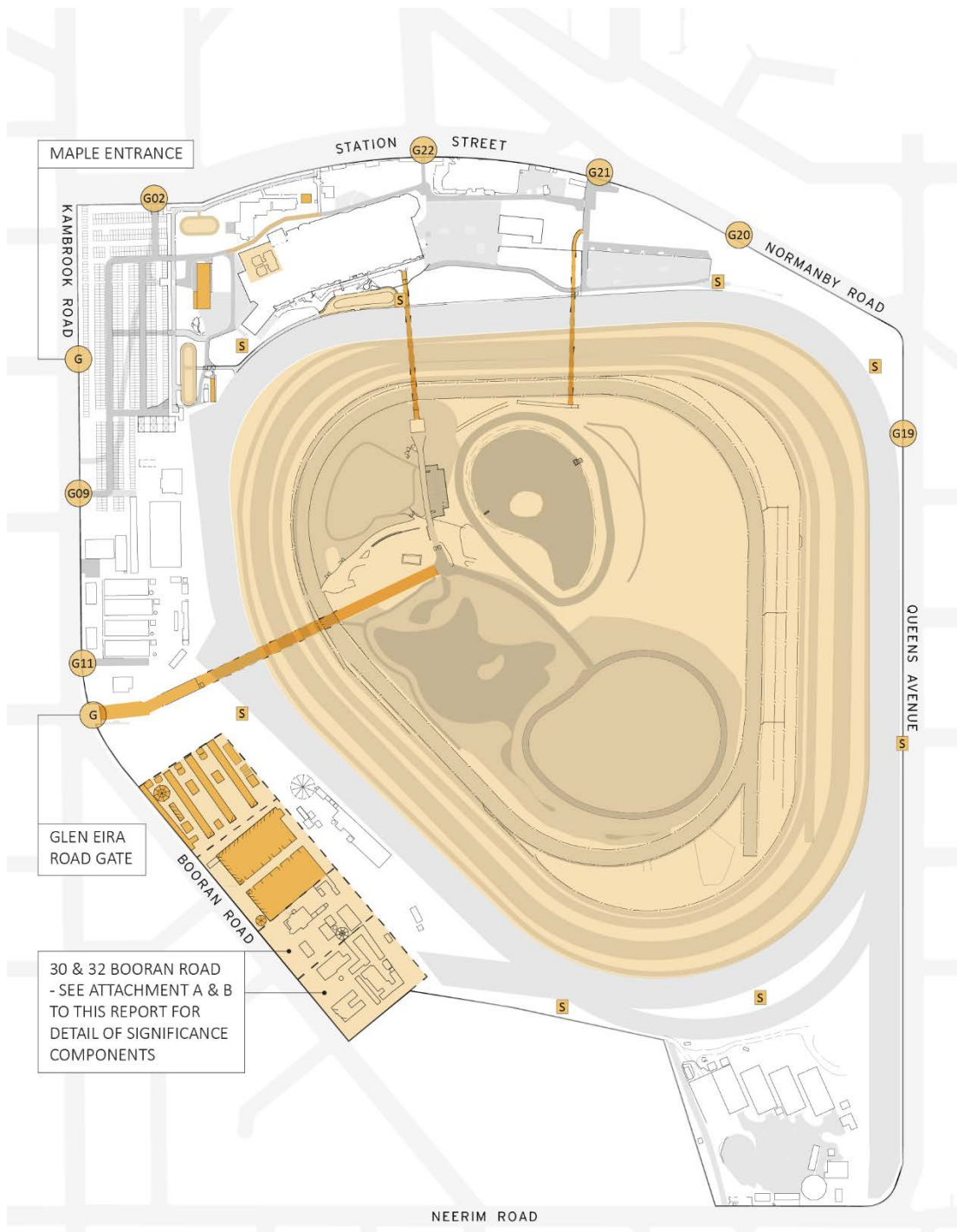


Figure 4 Plan illustrating the elements of secondary significance; noting that the boundary walls are indicated on separate plans

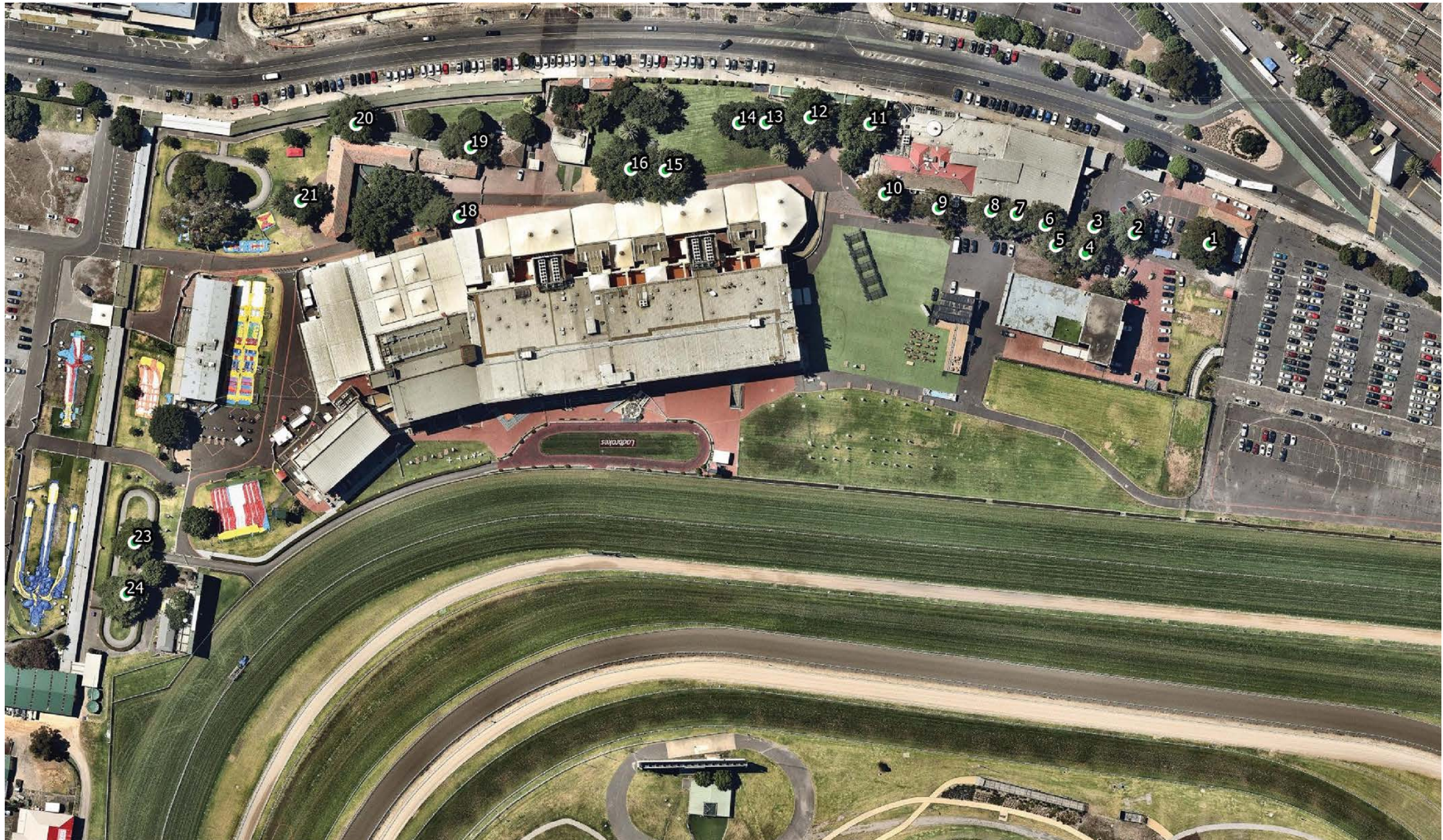


Figure 5 Plan showing trees of primary significance in the northern area of the racecourse, refer to section 4.1 for tree numbers

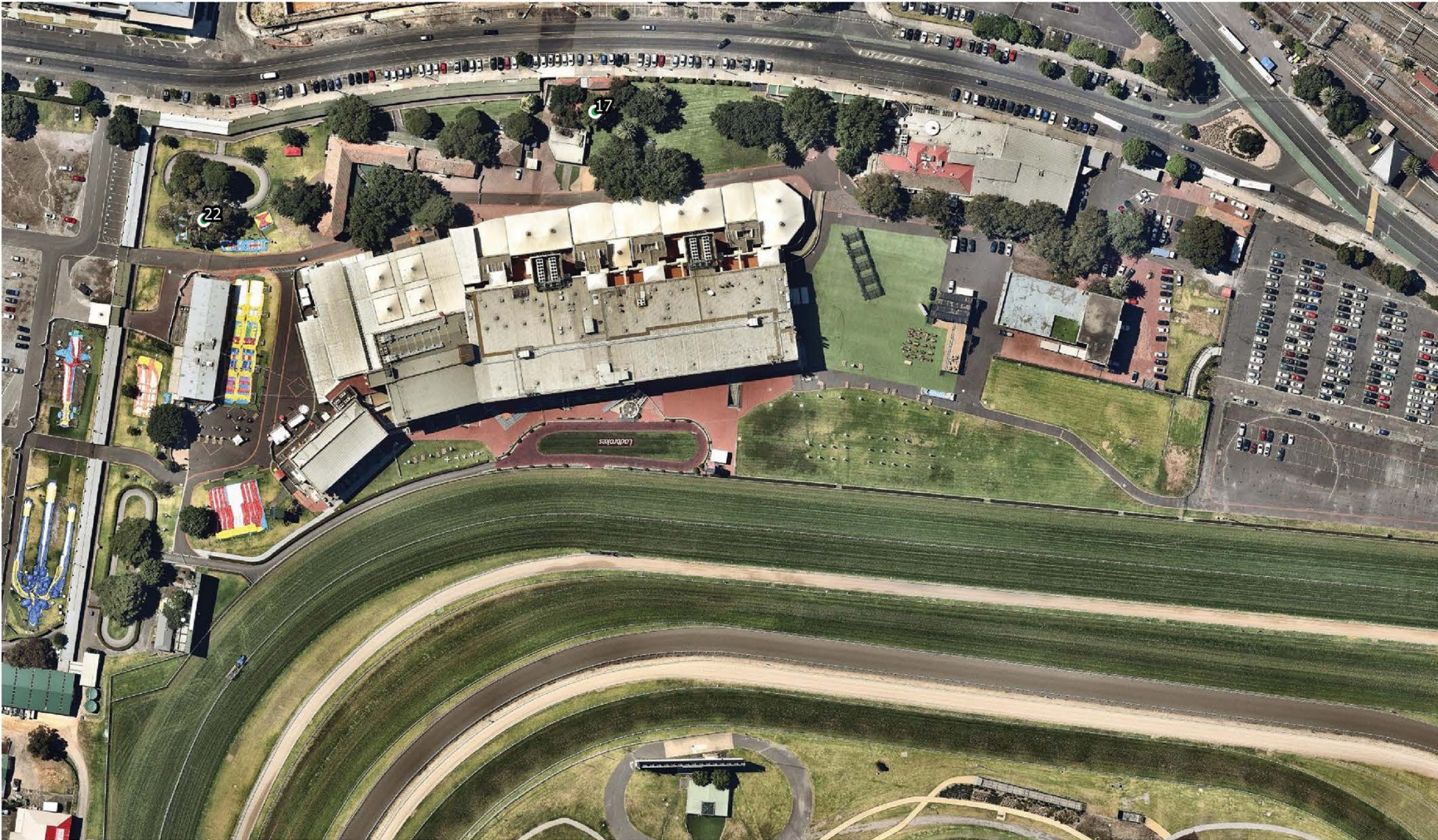


Figure 6 Plan showing trees of secondary significance in the northern area of the racecourse

4.2 Conservation Policy and Management Plan

The CMP provides a policy framework for the management of Caulfield Racecourse consistent with its heritage values. This includes general and more detailed (element-specific) policies.

Relevant general policies for the place as a whole are reproduced below, while the more specific policies are set out in detail in Appendix A.

Note that the general policies focus both on issues of use and viability as well as those of significant fabric and landscape. This reflects the values of the place, which derive in large part from the primacy of the historical and social associations and the long use of the place as a major metropolitan racecourse. The policies recognise that the management of the place consistent with its heritage values should support its ongoing operations and continuing use, while also seeking to retain and conserve elements and fabric of heritage significance (both built and landscape) as part of the place. The policies also allow for the renewal of facilities and infrastructure that support racing operations, including through relocation or replacement (both of which are in themselves a tradition at the place).

General conservation policies: racecourse management and redevelopment

6.4.1 Use of the racecourse

Policy: *The use of the place as a racecourse is fundamental to its heritage significance. Works and development which support this use, including maintaining the viability of the place as a racecourse, should be supported.*

6.4.2 New buildings and works

Policy: *New works and development required to support the ongoing operation of Caulfield Racecourse should be sympathetic and responsive to the heritage values and significant buildings and elements, and where relevant the historical pattern of development on the site.*

[...] It is also the case that Caulfield Racecourse has an existing diversity of buildings from different periods, and diverse building forms in terms of height, scale and materials. It is not a heritage place with a consistent heritage character, or consistent building scale and form. Accepting this, there are patterns to development, including placing large structures – such as grandstands and members facilities – generally on alignment with the race track in the northern area of the racecourse, complemented by betting facilities in proximity. Horse day stalls, Walking Rings, the Birdcage and the like are also concentrated in the north-west of the racecourse. [...]

There is also an established relationship (visual, historical, etc) between the stands and associated facilities in the north of the racecourse, with the horse and race day elements in the north-west. Such relationships and patterns should generally be retained and maintained with new development, as the concentration and co-location of these important race day elements/functions in a prominent part of the racecourse is supported in heritage terms. There is however scope to move or relocate individual elements within this broader framework; some of these elements/functions have also been relocated before. [...]

6.4.3 Settings to heritage buildings

Policy: *The settings to heritage buildings at Caulfield Racecourse are an important component of the aesthetic significance of these elements and the racecourse overall, and should be managed and conserved.*

6.4.7 Heritage buildings

Policy: *The significant buildings at the racecourse should be managed and treated in any future development, in a manner which has regard for their heritage values and attributes, as identified in this report.*

Relocation of elements

The racecourse has a history of relocating elements. For instance, the Walking Rings and Mounting Yard have been moved and/or rearranged, and could reasonably be relocated again subject to being retained in the northern part of the racecourse and in the area which has a race day focus. The race day horse stalls have also previously been relocated.

6.4.8 Managing change in the landscape

Policy: *A tree replacement strategy should be prepared to guide future tree plantings within the site to reinforce valued landscape character whilst considering both natural change to the landscape as well as change as a consequence of new development.*

Whilst new works and development required to support the ongoing operation of Caulfield Racecourse should be sympathetic and responsive to its heritage values and significant elements, intensive race-day uses are concentrated in the north of the site where the majority of significant trees are located in close proximity to buildings. There is an inevitability that some trees identified of heritage significance will require removal to facilitate expansion or development of race-day activities such as new grandstands, stalls, walking rings etc.

6.4.9 Balancing/compensatory actions

Policy: *Where a heritage building or structure is demolished, or a heritage loss results from development, balancing actions or compensatory works should be undertaken, as a means of ensuring some balance is achieved to help mitigate the potential heritage impacts*

Conservation

There is an opportunity, as part of a substantial development program, to undertake conservation and restoration works at the racecourse, in relation to the retained buildings and elements of heritage value. Buildings and structures to be considered in this regard include the Boomerang Buffet Building, toilet block and Scratchings Board. The recommended reconstruction/reinstatement of the original external form of the Afternoon Tea Room/Luncheon Room is also in this category. In addition, conservation works to the retained sections of boundary wall, and to a retained collection of race day horse stalls, would also help mitigate the loss of the remainder of the walls and stalls.

Recording

While not mitigating the loss, the recording of any elements of significance where full or part demolition is proposed is also good practice.

This can include one or more of the following:

- archival quality photographs
- detailed drawings

- 3D modelling
- Preparation of a building history and/or architectural overview.

Copies of recording packages should be held by the MRC but can also be lodged with appropriate repositories for future reference or research.

Interpretation

On-site interpretation can be a balancing action, and is a means of conveying aspects of the history of the place, in this case the racecourse as a whole, to visitors and the public.

Caulfield Racecourse provides an outstanding opportunity for innovative interpretation for visitors and users of the site. Interpretation uses various means and methods to present and deliver information. It introduces visitors to the place, gets key messages across, and assists in developing an understanding and appreciation of the history and significance of the place. The information can be presented in a readable format, and through objects and artefacts, buildings and structures, research, art works, and landscape elements. Additional supporting information can also be made available, including brochures, pamphlets, books and websites. Signage and interpretive material should be professionally designed and prepared, to ensure the highest standards are maintained.

5.0 IMPACT ASSESSMENT

5.1 Rationale for the works

Many of the existing facilities at Caulfield Racecourse are aged and no longer support the operation of the course as a premier race day facility, and new horse area and associated facilities are considered essential to the continuation of Caulfield as a major metropolitan racecourse hosting a number of high-profile Group One races throughout the year. It is noted that the other major heritage-listed racecourses in Melbourne, Flemington and Moonee Valley, have undergone/are undergoing significant upgrades and redevelopment over the last decade. Without a similar upgrade Caulfield risks being 'left behind'.

The removal of end of life or under-utilised facilities and their replacement with modern facilities is a standard practice at sporting venues. In the case of Caulfield Racecourse, this is consistent with the ongoing management and use of the place to maintain its heritage values; the CMP supports new works to support the continued operation of the racecourse in the north-west are of the site (refer policy 6.42), as is currently proposed.

A key outcome of the redevelopment proposal for Caulfield is the new horse area /day stall complex; this has been designed to meet contemporary expectations with regard to horse welfare, best-practice racing integrity and patron experience and participation in race-day activities. The new horse area including day stalls to a contemporary standard, is considered essential to the successful ongoing operation of the Caulfield Racecourse at a world-class horse racing facility. The new horse area will separate direct interaction between race day patrons and horses, increasing safety standards for both. Notwithstanding the physical separation, the horse area will also remain visually accessible, ensuring that the traditional race day experience whereby patrons can interact with horses and visit the 'birdcage' is maintained.

The changes to the entry arrangement and fence treatment to Station Street is also driven by a desire to create a safer, more streamlined and more public-facing entry experience. The existing main entrance is located at some distance from the main Caulfield Station exit and is built direct to the boundary. There is currently no space for patrons to safely queue and wait for entry except on the narrow public footpath. On major race days, where thousands of visitors attend the racecourse by public transport,

this arrangement is both unsafe and inefficient. The proposal to create a forecourt and new inset entry turnstile arrangement will help to manage the influx of race day patrons and avoid spillage of crowds onto the street or monopolising public space.

Other actions proposed as part of the WP3 & 4 package also demonstrate the desire of the MRC to focus on enhancing the race day experience, including the relocation of administration away from the key events area to a less trafficked area to the west and creation of a new 'events' zone. The new administration building will ensure that the MRC can continue to administer and successfully operate the racecourse and maintain a presence on the site but will remove the existing building that dominates the northern part of the racecourse to the east of the grandstands. Converting this area to an events lawn that can cater for larger crowds is a positive outcome with regard to access and use and has no associated heritage impact.

The CMP policies prioritise the continuation of the use of the racecourse as essential to maintaining the heritage values of Caulfield Racecourse. The works envisioned in WP3 & 4 will provide an upgrade that supports the long-term use and operation of the racecourse and the long-term maintenance of the identified heritage values of the place.

5.2 Assessment of heritage impact

In order to facilitate the upgrade of Caulfield Racecourse a number of works are required that will interact with or impact on elements that contribute to the cultural heritage values of the place. This reflects the northern precinct as the traditional focus for race day activities and associated buildings over the history of the racecourse.

The project has prioritised the retention and conservation of key heritage buildings, including the Boomerang Building and the Afternoon Tea Room. The Boomerang Building is to be adapted for race day use (as part of a separate approved planning application for WP2), while WP3 & 4 propose conservation and interpretative reinstatement of the Afternoon Tea Room. The grandstands, which are particularly reflective of the race-day experience and use of the place, will also be retained with minimal change in this works package. Retention of mature trees is proposed where possible, and planting of new trees throughout the precinct will occur; the CMP recommends the development and implementation of a tree replacement strategy.

There would be some impacts on significant elements and fabric. These include the boundary fence (largely retained but modified in part), Toilet Block (demolished) and mature trees in the northern plantation (to be removed), all elements assessed as of primary significance in the CMP. Two elements of secondary significance, the First Aid Room and Maple Building, are proposed to be demolished. The more significant impacts are in the area to the east of the Boomerang Building (trees and Toilet Block); these impacts cannot be avoided and arise as a consequence of the necessary siting of the horse area in this area of the site.

In considering the impact of the demolition and alteration works, it is important to note that most of the key heritage buildings in the northern precinct contributing to the valued character and aesthetic/architectural values of the racecourse will be maintained and conserved. Specifically, an appreciation of the valued interwar theme that is distinctive to Caulfield Racecourse will be maintained through the retention of the prominent distinctive Boomerang Building and the Scratchings Board, even despite the demolition of the adjacent red brick Toilet Block, also of primary significance and part of this group. In the case of the First Aid Room, while this it has interwar origins, it is a heavily modified building as reflected in its secondary status and its demolition will have no measurable impact.

Albeit located further to the east, the retention of the Afternoon Tea Room is also key to maintenance of the interwar character. This building is to be retained and previous unsympathetic Glasshouse additions are to be removed, allowing a combination of conservation, part-interpretive reinstatement and adaptation works and allowing the building to be appreciated in the round.

The mature trees from the northern tree plantation (refer to Figure 5) contribute to the heritage qualities of the precinct and the required tree removals (around half of the identified primary significance trees) will have an adverse impact, but this would be mitigated over time through the implementation and maturing of a new landscaped setting to the area. It is anticipated that the strategy for landscape would be subject to further more detailed consideration and development to establish an appropriate landscape character.

The removal of the north lawn area for the new horse area is balanced by the establishment of a new events lawn directly in front of the racetrack.

The Maple Building is an altered building from the post-WWII phase of development. While it evidences that particular phase in the history of the place, its loss would not impact significantly on the broader heritage values.

On the Station Street boundary of the site, the approach is one that seeks to balance the retention of the traditional hard edge (boundary wall) presentation and related significant fabric, with contemporary requirements for greater permeability and the management of pedestrian traffic. Where the boundary alignment is proposed to be varied and set in from Station Street, sections of the original fence are retained to interpret the traditional alignment. Elsewhere on the site, extensive early racecourse fencing will be retained.

In addition to their direct interface with heritage fabric, including buildings, structures and landscape, the works included in WP3 & 4 clearly will introduce a layer of new contemporary design to the complex, through new buildings and other works, including a new entry sequence to Station Street and on the western boundary where the new Administration Building is to be sited. The design approach is clearly contemporary but is responsive and respectful where it has a meaningful interface with heritage buildings or structures. This approach is aligned not only with the CMP policies which as noted above, support the introduction of new facilities to support the ongoing operation of the place, but also represents a continuation of the history of development and renewal at Caulfield.

Overall, the works will result in a substantial change to the overall presentation and experience of the northern section of the racecourse but the proposal has been developed in a manner that prioritises, as far as is practicable, the retention and conservation of key heritage elements.

In addition, consistent with the CMP policies, actions are proposed to record changes to the place consistent with good heritage practice and to recognise history and heritage at the place. These measures are identified in the detailed assessment below where relevant but can be summarised as follows:

- Undertake an archival photographic recording of the northern precinct, with a particular focus on elements of primary and secondary significance
- Develop an interpretation strategy for the overall site, with a particular focus on the northern precinct and retained heritage elements.

The interpretation strategy should pursue opportunities to incorporate existing statutes, memorial plaques and the like which are located in the public areas of the racecourse (including statues to Bill Collins and fallen jockeys).

6.0 CONCLUSION

The works included in the Work Packages 3 & 4 program for the Caulfield Racecourse represent a significant redevelopment and reorganisation of the racecourse facilities. These works have been planned with the overall objective to upgrade the race day experience for all users including the public, trainers and horses and the staff of the MRC. The works all support the ongoing and active use of the place as a public racecourse, which is fundamental to its identified significance.

While in the course of the works a number of buildings and elements identified as contributing to the heritage values of the place are required to be demolished, removed or altered, as a whole, the works are considered to represent an appropriate balance the retention of the existing character of the racecourse with the change required to ensure effective ongoing use and operation.

The WP3 & 4 projects provide a responsive approach to the conservation of the heritage values of the Caulfield Racecourse.

APPENDIX A

1.0 DETAILED ASSESSMENT OF HERITAGE IMPACTS

This appendix provides a detailed description and assessment of the proposed works included in WP3 & 4. This includes the consideration of the individual statements of significance and policies provided in the CMP for the various elements.

1.1 Fences and entrance gates

1.1.1 Description and proposed works

Key changes at the Main Entrance Gate, and fence to the east, include the removal of a section of the existing fence and the realignment of the fence line as it interfaces with Normanby Road. The works include the retention of the main gates and former Afternoon Tea Room (also referred to as the Clubhouse), and the removal of the Glasshouse addition to this building. The works to the Afternoon Tea Room are considered in detail below (refer Section 1.3).

To the west of the Main Entrance Gate, the existing fence alignment will be retained, however there will be some sections of new, more permeable, fence sections introduced.

The works to the existing fence will support the creation of a new entry arrangement to the racecourse, including the establishment of a new forecourt and entry gates. The main entry will be to either side of the Afternoon Tea Room, although a secondary entry will be provided to the west in the vicinity of the Boomerang Building. The new entry gates will be square red brick arches with turnstiles below.

Main Entrance Gates

The Main Entrance Gates, also known as Gate 23, date from 1928 and are a large and elaborately detailed brick Gatehouse/entrance building (Figure 7). The architect was Albion H Walkley.¹

The building comprises open bays to the centre, with modern steel gates and turnstiles; and enclosed bays (brick ticketing boxes) abutting the gated bays to the east and west sides, with modern roller doors in the large openings to Station Street. The easternmost bay has no rear ticketbox.

Proposed works

The Main Entrance Gates are to be retained and conserved as existing, with the exception of the easternmost bay where the existing non-original roller shutter is proposed to be removed. This bay will remain open to the new public forecourt and entry gate behind.

The existing painted render elements will be repainted, with the colour to be based on paint investigation.



Figure 7 Gatehouse to Station Street

Fence to east of Main Entrance

The following works are proposed to the fence to the east of the Main Entrance Gate (the wall codes used are those from the CMP and are on the key plan at Figure 8):

Wall	Description/Proposed works	Significance
BF4	This comprises a small section of wall to the east of the eastern delivery gate. It was constructed in the 1920s to designs by Albion Walkley. These comprise expressed brick columns with rendered capitals that date from the original construction. For some of this section of fencing, steel picket fencing has been introduced between the columns.	Primary
	It is proposed to retain the brick plinth and columns (including rendered capital). Sections of later brick and steel palisade infill will be demolished	
BF1	Low brick walls and/or brick pillars associated with new entrance gates, constructed from modern brick, with painted galvanised steel palisade fences and gates.	Little or no
	The non-original gate is proposed to be demolished. The brick plinth and columns will be retained.	
BF8	These are components of the 1920s boundary wall designed by Albion Walkley, which were originally low brick walls with engaged plinths, topped with pillars with infill corrugated iron fencing both now demolished. The current painted galvanised steel palisade fencing dates from the 1990s.	Primary

Wall	Description/Proposed works	Significance
	<p>It is proposed to demolish the palisade infill to the brick plinth and to retain the plinth.</p> <p>A new angled fence will be constructed behind the plinth.</p>	
BF6	<p>This comprises a small section of wall in the vicinity of the eastern delivery gate. The earliest sections of this wall were constructed in 1920s to designs by Albion Walkley. Works in the 1950s saw the original low brick wall demolished and replaced with a tall plain wall surface, with a painted concrete capping, running to the tops of the pillars. These sections retain evidence of the original low brick wall (sometimes a single brick course) between expressed brick columns with rendered capitals that date from the original construction.</p> <p>The brick plinth and columns will be retained. The infill brick sections will be demolished.</p>	Primary
Eastern delivery gate	<p>The eastern delivery gates reflect the detailing of the Main Entrance Gates on less grand scale. The gate incorporates two doorways flanked by red brick pilasters and topped by a rendered cornice element.</p> <p>The eastern delivery gate will be retained and the existing timber doors conserved. The non-original metal finished door will be replaced to match the timber arrangement.</p> <p>The store building to the rear is proposed to be demolished.</p>	Primary
Glasshouse	<p>Directly to the east of the Main Entrance, the Glasshouse forms a modern boundary treatment that directly fronts the street (i.e. no fence treatment).</p> <p>It is proposed to demolish the Glasshouse and the associated boundary treatment.</p> <p>It is proposed to construct a new brick plinth to the newly exposed forecourt and install a contemporary pillar arrangement</p>	Little or no

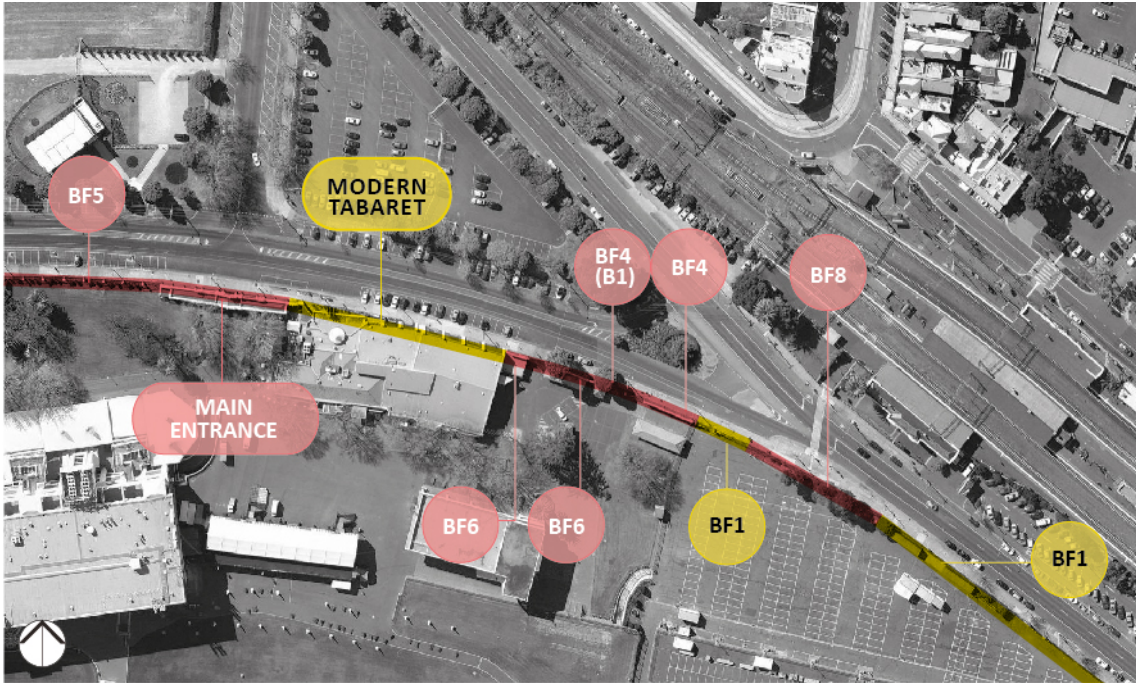


Figure 8 Key plan indicating the status of the fence to the east of the Main Entrance
Source: Caulfield Racecourse CMP, base image Nearmap c. 2016



Figure 9 View of the existing arrangement of the boundary treatment to the east of the Main Entrance/Gatehouse (indicated by red arrow)
Source: Nearmap

Fence to west of the Main Entrance

To the west of the Main Entrance a mixture of retention and removal of walls is proposed (Figure 11). The subject sections of wall include (from east to west, refer key plan at Figure 10):

Wall	Description	Significance
BF5: c. 1920s with 1990s alteration	<p>These are components of the 1920s boundary wall designed by Albion Walkley, these sections comprise brick plinths and brick pillars with painted rendered cement capitals. The low walls between these pillars were originally topped by a corrugated iron fence which was painted green as evidenced by extant green paint on the column surfaces. 1990s works saw the height of the plinth reduced and the replacement of the corrugated iron sections with painted galvanised steel palisade fencing.</p>	Primary
	<p>It is proposed to retain the brick elements of this section of the fence and to install a new palisade treatment.</p>	
BF3: c. 1920s	<p>This is a component of the 1920s boundary wall by Albion Walkley. It comprises corrugated steel fencing, with steel cladding situated above a low face brick plinth and between tall face brick columns with rendered capitals. The corrugated steel is a modern replacement of the original. Horse stalls are attached to the internal face. This section of the wall differs from the other 1920s boundary fencing in that the expressed brick columns rise from the lower sections of the wall without a boldly expressed plinth found elsewhere.</p>	Primary
	<p>It is proposed to retain the brick elements of this fence and to convert six corrugated bays to a palisade treatment.</p> <p>Three new bays will mark a new entry point with the brick plinth to one bay to be demolished and the flanking bays converted to palisades.</p> <p>The balance of this fence will be retained as is with the corrugated infill (noting one panel will have a new door openings installed as part of WP2 scope of works).</p>	
BF2: c. 1950s	<p>Wall replicates earlier 1920s boundary wall detail of Albion Walkley, including integrated plinths supporting expressed brick pillars with painted cement tops. The wall design reflects the incorporation of the horse stalls attached to the inner face.</p>	Secondary
	<p>It is proposed demolish and reconstruct this wall on a new alignment to the corner of Station Road where it turns into the site.</p> <p>This rebuilding/realignment is proposed to two existing northern bays and six bays to the west wall.</p> <p>The balance of the wall will be retained with some changes associated with the removal and addition of gates.</p> <p>Sections of this wall will also be removed as part of the construction of the new Administration Building (refer 1.7)</p>	

Wall	Description	Significance
Western delivery gate	<p>The western delivery gates adopts a design very similar to the eastern delivery gate however includes six entrances to stores behind plus one direct to the racecourse. The original gate arrangement included five gates and a later extension of two gates was added to the east.</p> <p>The original timber doors to these gates have been removed and replaced by roller shutters.</p> <p>Two original brick store buildings are located to the rear of the gate.</p> <hr/> <p>It is proposed to convert three bays of the original gate to palisade infill and to return timber infill to the two eastern gates, with signage to match the eastern gate.</p> <p>The later eastern section of the gate will be demolished and palisades installed.</p> <p>The store buildings to the rear are proposed to be demolished.</p>	Primary

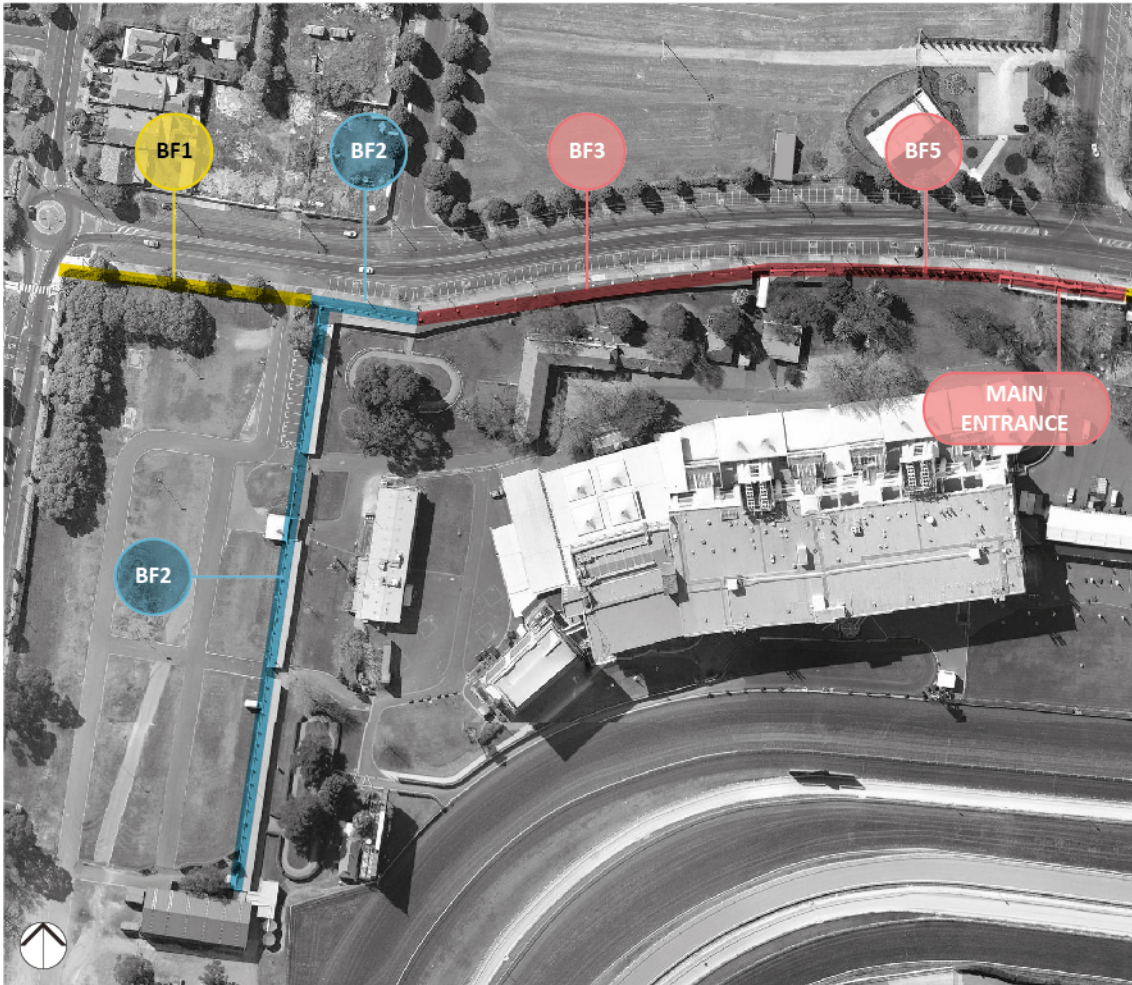


Figure 10 Key plan showing the significance of sections of wall to the west of the Main Entrance Gate and the interior fence (red equates primary significance and blue indicates secondary significance)
 Source: Caulfield Racecourse CMP, base image Nearmap c. 2016

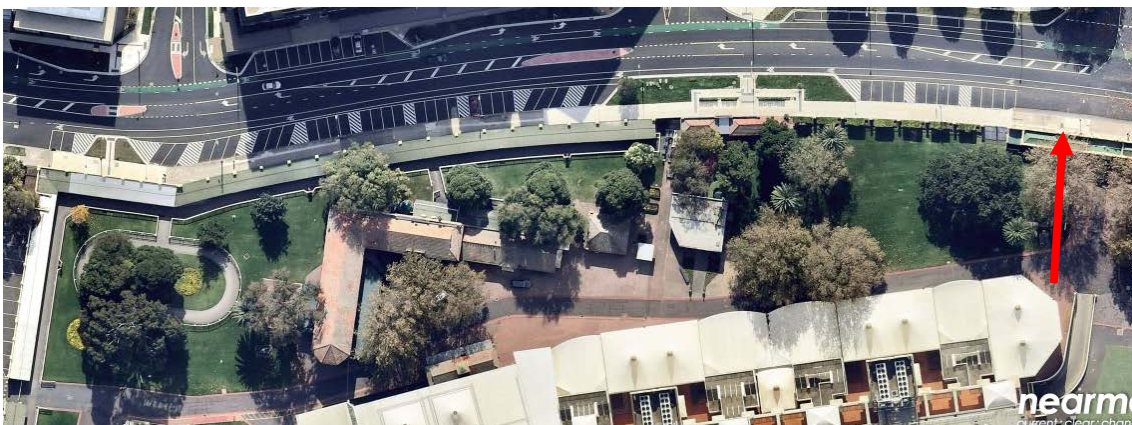


Figure 11 Aerial photograph showing the wall to the east of the Main Entrance Gate. Main Entrance Gate indicated by arrow
 Source: Nearmap

Trees

Two English Elms (trees 5, 6 in WP3 & 4, trees 4,5 in CMP) identified as being of primary significance in the CMP are proposed to be removed to facilitate the new entry. These elms form part of the 'internal streetscape,' with the alignment possibly reflecting the pre-1920s realignment of the street to the north of the racecourse.

1.1.2 Significance

The individual statement of significance for the Main Entrance Gates and east and west delivery gates are as follows:

Gatehouse/main entrance

Values: Historical, Social, Architectural

The 1928 face brick Gatehouse is significant as a large, substantially intact and elaborately decorated historic building associated with the main public and ticketed entry to the racecourse, in an area assumed to be the main patron entry since the 1870s. The structure is also a prominent element to Station Street, and one of the more publicly visible historic buildings at the racecourse. It has important presentations to both its north (Station Street) and south (internal racecourse) facades.

East and west delivery gates

Values: Historical, Architectural

These face brick and timber gates are sited to the east and west of the Gatehouse on Station Street, and date from the 1920s. The delivery gates are significant for complementing the main Gatehouse in terms of form, materials and detailing, and are substantially intact. While of more modest scale than the latter, they contribute to the heritage character of the northern boundary of the racecourse, and are highly visible elements to Station Street. Historically, they are believed to have functioned as structures which could receive direct delivery through their Station Street openings, and efficiently bypassed the need for delivery vehicle entry to the course.

The fences subject to works as part of this proposal include sections of walls of primary and secondary significance described as follows:

Walls of primary significance (Boundary wall/fence to Station Street/Normanby Road/Queens Avenue)

Values: Historical, Aesthetic, Architectural

The red brick boundary wall to Station Street and Normanby Road is of historical and aesthetic/architectural significance. It is a substantial and prominent historical component of the northern end of the racecourse, which demonstrates the traditional racecourse boundary, providing the required management of access and enclosure to the course. The wall also makes a contribution to the heritage character of Station Street and Normanby Road, with the earliest sections of the 1920s being the most significant, albeit these sections have in part been altered and in some cases only limited 1920s fabric remains. Through their height and provision of ventilators, many sections of the wall – particularly at the west end – demonstrate their role in the race day stalls. The Queens Avenue fence is also of historical significance. It too provides a boundary marker and enclosure to the

eastern part of the racecourse. The serrated top additionally provides evidence of the need to control entry to the racecourse from early in its history.

The sections of primary significance are illustrated in the plans Figure 189 [refer Figure 10 above] and Figure 190 [refer Figure 8 above] in Chapter 4, and include the sections marked BF3, BF4, BF5, BF6, BF8 and BF10.

Walls of secondary significance

Values: Historical

The sections of boundary wall of secondary significance are mostly from the 1950s, and indicated as BF2 in the plans at Figure 189 [refer Figure 10 above] in Chapter 4.

Walls of little to no significance include Racecourse boundary wall/fencing (sections BF1, BF7 and BF9).

1.1.3 *Policies*

Relevant CMP conservation policies with regard to the gates and boundary walls are as follows:

Gatehouse at main entrance, together with the east and west delivery gates

The 1928 Gatehouse at the main entrance is one of the more publicly visible heritage buildings at the racecourse, given its presentation to Station Street and its association with the main public entrance into the racecourse. Desirably, this building should continue to be a focus of the main racecourse entry at the northern end of the racecourse, which is a function it has performed historically. As a narrow rectilinear building, with important and mostly intact presentations to both its north (Station Street) and south (internal racecourse) 'frontages', the Gatehouse does not easily lend itself to adaptation or addition. It is also recommended that, to enhance its appearance, the addition to the west end could be removed and the original wall/fabric made good in this area. Internally, the spaces have been altered and can be so again, particularly to sustain the use of the building.

The delivery gates on Station Street to the east and west of the Gatehouse are simple storage buildings which could continue with this use, or be altered to accommodate a new use, although their modest scale and form offers limited opportunity for extension. The original or early timber doors should be retained; timber doors could also be reinstated to the openings where these have been replaced by modern steel roller doors.

Racecourse boundary wall/fencing (primary significant sections)

The elements of the boundary wall/fencing to the racecourse, which are of primary significance, are generally recommended to be retained. However, if it can be demonstrated that not all the walls of primary significance are required for retention, for future racecourse use, then some partial removal can be contemplated. The important consideration here is also to retain evidence of the historically enclosed nature of the racecourse, and to continue to use early fabric mark to the boundary in some way.

A similar approach is recommended for walls of secondary significance, albeit in a fabric sense these are of less significance.

Partial removal should adopt the following principles:

- Retain as many long runs/long sections of wall as possible, with minimal demolition
- New openings or penetrations are preferred to demolition of large sections of wall
- Where demolition is proposed, work within the rhythm of the wall, and its bays and piers, and retain wall nibs to provide evidence of existing wall height
- With openings, retain the framing piers and low sections/low courses of existing wall to maintain evidence of the wall and its design and form, in preference to full demolition of wall sections
- Where wall is removed, consider the use of a landscape treatment or similar, to mark the historic racecourse boundary.
- Where new brick walls are proposed, then these should be constructed in a sympathetic and complementary form and materials to the existing brick walls, but preferably avoiding identical detailing so as to mark them as contemporary.

Regarding the corrugated steel fencing to Queens Avenue, ongoing maintenance is preferred over demolition and replacement of the existing fence. Where the latter is required, then the replacement walls should be of a similar material such as corrugated galvanised steel. Where partial demolition is sought, then retention of a representation section is recommended. As with the brick boundary walls, new openings or penetrations are preferred to demolition of large sections of wall.

Racecourse boundary wall (secondary significance sections)

The boundary wall/fence components of secondary significance are mostly located in the western part of the racecourse boundary, constructed following the extension of the course to the west in the late 1950s.

The policy relating to the walls of primary significance also generally apply here.

In applying the policy, however, in the case of walls of secondary significance, there is less concern for the fabric of the walls (recognising that the fabric of the secondary walls is later and of no particular significance in itself) and the value of these is historical, not aesthetic.

The same policies of retaining the enclosed nature of the racecourse and marking the boundaries generally apply.

1.1.4 *Comment*

A key part of the WP3 & 4 project is the reconfiguration of the entry arrangement and boundary edge alignment to Station Street to create an expansive entry forecourt to help manage pedestrian flow and security.

The proposal to Station Street/Normanby Road, east of the Main Entrance Gates, retains the significant fabric and arrangement of the existing fence treatment, while allowing for a more open and permeable boundary treatment. This is particularly important given the proximity of this part of the course to the Caulfield Railway Station. On race days the majority of the crowd arrives from the railway station, and the provision of sufficient space to safely and effectively manage this influx is the main driver for the revised entry arrangements. The new forecourt will provide sufficient space to manage pedestrian movements on racecourse land, removing the requirement for queuing and milling on the public

footpath. While there will be change to the existing fence arrangement to achieve this outcome, the key fabric of significance will be retained and a sense of the overall enclosed nature of the racecourse will continue through the new angled fence treatment.

To the west of the Main Entrance Gates, the existing fence alignment will be retained, with section of the corrugated infill removed to create a more permeable and engaging street interface. The brick fabric of the fence will be retained and the overall understanding of the boundary treatment will be retained, within the context of the redeveloped northern precinct and new horse area in this location.

The proposed works to the boundary fence comply with the CMP policy in that they 'retain evidence of the historically enclosed nature of the racecourse', and to continue to use early fabric mark to the boundary in some way. This has been balanced with the requirement to provide a safer, more efficient and contemporary entry experience through the establishment of new gates and an associated forecourt in front of the Clubhouse (former Afternoon Tea Room).

Main Entrance Gates

Relatively minor works are proposed to the Main Entrance Gates.

The CMP Policy comments that this building should continue to be used for entry to the racecourse. This is not the proposal, and for functional reasons the gates would be closed in preference of the new gates to the rear. Accepting this, the historical use would remain readily legible through the retained fabric.

It is proposed to remove the non-original roller door to the eastern bay of the gates and retain this as an open bay to the new forecourt behind. The roller door itself is of no significance, and there will be no impact on significant heritage fabric as a result of the change. The open bay will somewhat alter the presentation and traditionally closed nature of this part of the gates, however the change is entirely reversible.

East of the Main Entrance

The following table provides a comment regarding the impact on individual sections of fence.

Wall	Comment
BF4	The retention of the original brick section of the wall is a positive heritage outcome and is consistent with the CMP policy. The demolition of the later brick infill and steel palisade sections will have no heritage impact.
BF1	This section of wall is of little to no significance and the removal of the upper brick sections of the wall will have no heritage impact. The retention of the low brick plinth and columns will help to retain an understanding of the realigned 1920s boundary although this fabric is late. The gate in this location is of little to no significance and its removal will have no heritage impact.
BF8	The retention of the original brick section of the wall is a positive heritage outcome and responds to the CMP policy. The demolition of the later brick infill and steel palisade sections raise no heritage concerns.
BF6	As with BF4, the retention of the original brick section of the wall is a positive heritage outcome and responds to the CMP policy. The removal of the 1950s brick infill will have no heritage impact.

Wall	Comment
Eastern delivery gate	<p>The retention of the eastern delivery gate is a positive outcome. The existing timber doors will be retained, and the non-original metal finished door will be replaced to match the timber arrangement.</p> <p>The small store to the rear of the gate is a straightforward brick structure of no particular significance beyond its association with the gate. The demolition of this element will not have a detrimental impact on the heritage values of the racecourse.</p>
Glasshouse	<p>The Glasshouse is an element of little to no significance and the removal of this building will have no heritage impact and is a positive action as related to the Afternoon Tea Room.</p> <p>The installation of an interpretative fence treatment to the State Road boundary is a positive outcome that will reinforce the original (realigned) racecourse boundary in a modern treatment.</p>

West of the Main Entrance

Wall	Comment
BF5	<p>The replacement of the existing pickets with a new palisade arrangement raises no heritage concerns. The retention of the brick sections of the fence is a positive heritage outcome.</p>
BF3	<p>The approach to BF3 balances the historically enclosed nature of the northern part of the racecourse with the desire of the MRC to have a more permeable street interface.</p> <p>While the corrugated infill treatment is the original treatment, the existing panels are later replacements. The replacement with a palisade infill will alter the character of the fence treatment in this location, however the brick sections will be retained and the overall rhythm of the fence treatment will remain. The works do not preclude the future reinstatement of the corrugated metal treatment should it be desired.</p> <p>The retention of a substantial section of fence with the corrugated panel ensures that the original fence treatment is retained at the site and continues to express the originally visually enclosed boundary treatment.</p>
BF2	<p>This wall is identified as being of secondary significance and the demolition and rebuilding of the wall on a revised alignment will have no impact on the assessed significance of the racecourse. The CMP recognises that these walls are not of significance for the fabric but for their historical value in marking a racecourse boundary. This value will be retained through the reconstruction of the corner section of the wall and the retention of the majority of the wall.</p>
Western delivery gate	<p>The works to the western delivery gate include the replacement of the non-original roller doors with a mix of timber and palisade infill and the demolition the brick stores to the rear.</p>

Wall	Comment
	<p>The existing roller doors are of no significance and their replacement with timber and palisade treatments to match adjacent fence treatments and create consistency along the fence line is a positive outcome.</p> <p>The stores to the rear are not identified as contributing to the significance of the gates and the demolition of these utilitarian structures will not adversely impact the significance of the gate or the place as a whole.</p>

Trees

A detailed discussion regarding tree removal and the associated impact on the identified values of the place is provided at Section 1.4.5. With regard to the two elms proposed to be removed for the new entry, these form part of the 'internal streetscape' and may reflect an original planting of street trees to the former road in this location. The two trees proposed to be removed are located at the end of this row and their removal will not impact the overall appreciation of the tree lined 'street.'

1.2 Day stalls

1.2.1 Description

The extensive collection of race day stalls is located in the north and north-west areas of the racecourse. The stalls are constructed as part of the boundary wall to Station Street and to the return wall which separates the racecourse from the Kambrook Road car park. The stalls house horses on race days and are part of the experience and activity of the Birdcage.

In terms of age the stalls fall into two broad groups. The existing horse stalls along the crescent section of boundary wall to Station Street (see Figure 12) are believed to date from the 1920s redevelopment of this section of the site, the stalls to the shorter straight section of boundary wall to Station Street, at its west end, and the return wall which separates the racecourse from the Kambrook Road car park, were constructed in the 1950s.

The stalls are numerous and repetitive in terms of construction methods and materials. They are built of timber, roofed over, and are of a consistent size. They are delineated by timber post supports to the front and separated by timber barriers comprised of vertical timber slats with a prominent top rail at waist height and two further rails below.



Figure 12 Race day stalls, adjacent to the boundary wall to Station Street

1.2.2 *Proposed works*

It is proposed to remove the majority of the race day stalls at the racecourse. A section in the north-west corner (stalls 68-75) will be retained.

1.2.3 *Significance*

The collection of race day stalls at the racecourse are identified as being of primary significance in the CMP. The individual state of significance for the stalls is as follows:

Race day stalls

Values: Historical

While varying in age, the race day horse stalls are of long-standing, numerous and repetitive in terms of their form, construction methods and materials. As a collection, the stalls are significant in terms of racecourse operations and race day activities and they make an important contribution to the experience and activity of the Birdcage area of the course. In attributing a level of primary significance to the collection, however, it is recognised that the stalls vary in age, may incorporate relocated/reused fabric and also that some have an impact on the presentation of other structures of significance (eg the Boomerang Buffet building).

1.2.4 *Policy*

The policy with regard to day stalls is as follows:

The race day horse stalls are of long-standing, numerous in number and repetitive in terms of form, construction methods and materials. They are variously attached to, or abut and utilise, existing walls including boundary and building walls.

The race day stalls have been collectively identified as of primary significance, however from a heritage perspective full retention is not required or recommended.

In circumstances where stalls are no longer fully required, or are not fit for modern racecourse use, part demolition and/or replacement of the stalls.

A representative section of stalls, preferably one which retains some length and is demonstrative of their repetitive form and construction, should be retained to provide evidence of this aspect of racecourse operations. As a guide, a row of older stalls (together with associated boundary wall) could be retained on the Station Street boundary, west of the western delivery gate, not including the stalls at the far west end constructed in the 1950s. The extent of retention could be in the order of six-eight stalls, working with the regular rhythm of the stalls and the boundary wall bays.

Examples of stalls could also be relocated elsewhere at the racecourse, if suitable for re-use.

1.2.5 Comment

The race day stalls are identified as being of primary significance for their historical value to the racecourse, albeit the relevant policies recognise that full or even majority retention is not required on heritage grounds, and their replacement with contemporary facilities could be contemplated if required for operational reasons. The requirement to replace these and other race day facilities is considered in the main body of the report (refer Section 5.1) and in detail below at Section 1.4.

The stabling for horses on race day, will be continued in the new horse areas proposed as part of the redevelopment in a contemporary manner that reflects modern expectations and requirements.

As recommended in the CMP policy, a representative example of the stalls is being retained to reflect and interpret the historical approach to the stabling of horses at the racecourse. The interpretation of these elements will be considered in the interpretation strategy to be developed for the racecourse.

The impact is the loss of fabric of significance which reflects earlier stabling arrangements but the practice itself (day accommodation for horses) as an integral part of race day operations, is ongoing and will be reflected in the new facility. On the basis and with the mitigation of a section of earlier stalls, the demolition is considered acceptable.

1.3 Afternoon Tea Room (Clubhouse)

1.3.1 Description

The Afternoon Tea Room (also known as the Luncheon Room, now Clubhouse) is located adjacent to the northern boundary of the racecourse between entrance gates 22 and 23. As constructed in 1939, the building was a modestly-scaled, freestanding pavilion. It has subsequently been enclosed on its east end, and partly on its north side, by the modern Tabaret building, known as 'the Glasshouse', to Station Street. The south side of the building is partly obscured by a recent steel picket fence (Figure 13). The architect was Albion H Walkley.² This building is now referred to as the 'Clubhouse' by the MRC.

The majority of the eastern façade appears to have been demolished as part of the Glasshouse construction; elements of the north façade may remain as concealed in later structure (Figure 14).



Figure 13 View of the south façade of the Afternoon Tea Room



Figure 14 View of the south façade of the Afternoon Tea Room, showing Glasshouse addition to the east

1.3.2 *Proposed works*

It is proposed to demolish the Glasshouse additions to the former Afternoon Tea Room and to retain and conserve the original surviving fabric as far as is practicable. On the east and north elevations where fabric has been wholesale demolished, altered and/or obscured by the Glasshouse an approach of interpretative reinstatement (rather than full reconstruction) is proposed. This includes a contemporary interpretation of the demolished eastern bay, in a pre-cast panel system, with the same plan form to a slightly larger footprint to accommodate a medical room. The reinstated northern façade (originally the rear elevation of the building) will also incorporate a pre-cast element to the central bay, with the remainder of the walls reconstructed (where reconstruction is required) in brick. To the western bay of the northern façade the existing demolished section of the original concrete canopy will be reconstructed. A contemporary canopy will be installed to the eastern bay, where the original canopy has been lost altogether.

The existing significant external fabric, largely the southern and western facades, will be retained and conserved.

A new door opening is proposed to be introduced to the western bay of the south façade, through the conversion of an existing window and a new door is also proposed to the western bay.

As noted above, an entry forecourt is proposed to be constructed to the north of the Afternoon Tea Room following the removal of the Glasshouse.

1.3.3 *Significance*

The Afternoon Tea Room is identified as being of primary significance in the CMP:

Values: Historical, Architectural

While this interwar (1939) building has been enveloped by modern development to its north and east sides (the Glasshouse), the original Moderne form and expression of the building, including original details and fabric, remain substantially evident. It is also one of the more architecturally distinguished buildings at the racecourse, despite the enveloping later development.

1.3.4 *Policy*

The policy with regard to the Afternoon Tea Room is as follows:

While this interwar building has been in part enveloped by modern development to its north and east sides (the Glasshouse), the original Moderne form and expression of the building, including original details and fabric, remain substantially evident.

Desirably, the Glasshouse addition to the north and east sides of the building should be removed and the north and east facades reconstructed, reconstruction works to be documented based on the original plans.

It would be highly desirable that an adaptive reuse is found for the building which integrates it into racecourse operations and/or race day activities.

1.3.5 *Comment*

The removal of the Glasshouse, which has obscured and overwhelmed the Afternoon Tea Room, is a positive heritage outcome. The re-establishment of the Afternoon Tea Room as a standalone and independent building will help to reinforce the interwar character of the northern racecourse precinct.

The eastern bay, which has been partly demolished as part of the Glasshouse works, is proposed to be reinstated as a contemporary element on a slightly larger footprint to than the original building (and as

remains to the west). This reflects the requirement to establish direct and discreet entry to the proposed medical room in this area. This allows for an active race day use to be introduced in this building, as supported by the CMP policy. The new eastern bay will be a sympathetic but interpretative element that reflects the original design intent and footprint but does not attempt to recreate it.

The same design language will be incorporated to the (new) main entry to the north façade. The north façade was originally the rear of the building and will now form part of the ‘front door’ of the racecourse as part of the new open forecourt and entry arrangement. In this context, noting that the original north façade has been substantially demolished/extensively modified and overbuilt, the adoption of an interpretative design approach to the reconstructed entry is an appropriate heritage response. The new design will mark the new entry, without compromising the overall design composition or competing with the remaining original heritage fabric.

To the west of the new northern entry, the concrete canopy which is still partially extant to the north, will be reconstructed to its original extent. This is a positive heritage action, that recognises that enough of the original fabric remains to justify this action. The same action is not proposed to the east, where the canopy has been completely lost. This approach is considered acceptable, noting this northern elevation was originally the rear face of the building.

The conservation of the primary south façade including retention and conservation of the existing windows and its use for food and beverage will improve its presentation. The additional door in this elevation is required for the use and will have a relatively modest impact.

In summary, while the CMP recommends full reconstruction of the Afternoon Tea Room based on the original documentation, the current approach (a mix of reconstruction and contemporary interpretation) is equally considered an acceptable alternative from a heritage perspective. The primary south elevation of the building and the surviving west elevation are both to be carefully conserved, while the interpretive response is focused on the original rear elevation, noting this will now have a significant interface and response to the new public realm forecourt. This treatment and the approach to the eastern medical room are respectful of the original while introducing a restrained contemporary design and materiality layer that is understood as such.

Overall, the works to the Afternoon Tea Room provide a positive heritage response that will ensure an active race day use, while allowing the building to once again read as an independent structure.

1.4 New horse area

1.4.1 Description

Horse accommodation and activity areas are an essential part of the operation of the Caulfield Racecourse. A key aspect of the Caulfield Masterplan is the substantial redevelopment of these areas and the construction of new supporting spaces in the north-western section of the racecourse.

The new area intersects with a number of existing elements in this section of the racecourse which are described individually below.

Boomerang Building

The Boomerang Buffet building was completed in 1919, with the ground floor used for refreshments and the first floor understood to have been for a buffet. Architecturally, it is distinguished by its ‘boomerang’ (L-shaped) plan with conjoined north and west wings and its highly-regulated facades. It adopts an understated Federation Free style expression in red brick with rendered detail. The building is articulated into bays, on all sides, by heavy brick pilasters. Round-headed arched openings at the first-floor level of each bay form a consistent motif around the perimeter of the building. On the southern and eastern building elevations, each arch forms a highlight window above a trio of casement windows.

On the northern and western elevations, they serve as upper lights set high above the works spaces within.

Toilet block

The toilet block is a simple single-storey building on a rectilinear plan dating from the c. 1920s (Figure 15). It abuts the eastern end of the Boomerang Building and accommodates male and female toilets to its western and eastern end respectively.

First aid room

This small building from the interwar period is located to the east of the 1920s toilet block. It is currently used as the first aid room and baby change room. It is a single-storey building with a single hipped roof on a square plan.

Existing horse areas

There are various 'horse areas' located throughout the racecourse precinct, including walking rings, training tracks and the mounting yard (Figure 16). These are generally oval in shape and are fenced from the public.

Landscape and Trees

A number of mature trees and lawn areas are located in the northern area of the racecourse. To the west of the main entrance gate in particular is a substantial expanse of lawn dotted with mature tree specimens, particularly Deodar Cedars, English Oaks, London Planes and Canary Island Palms (Figure 17). A Lone (Aleppo) Pine is also located in this area.

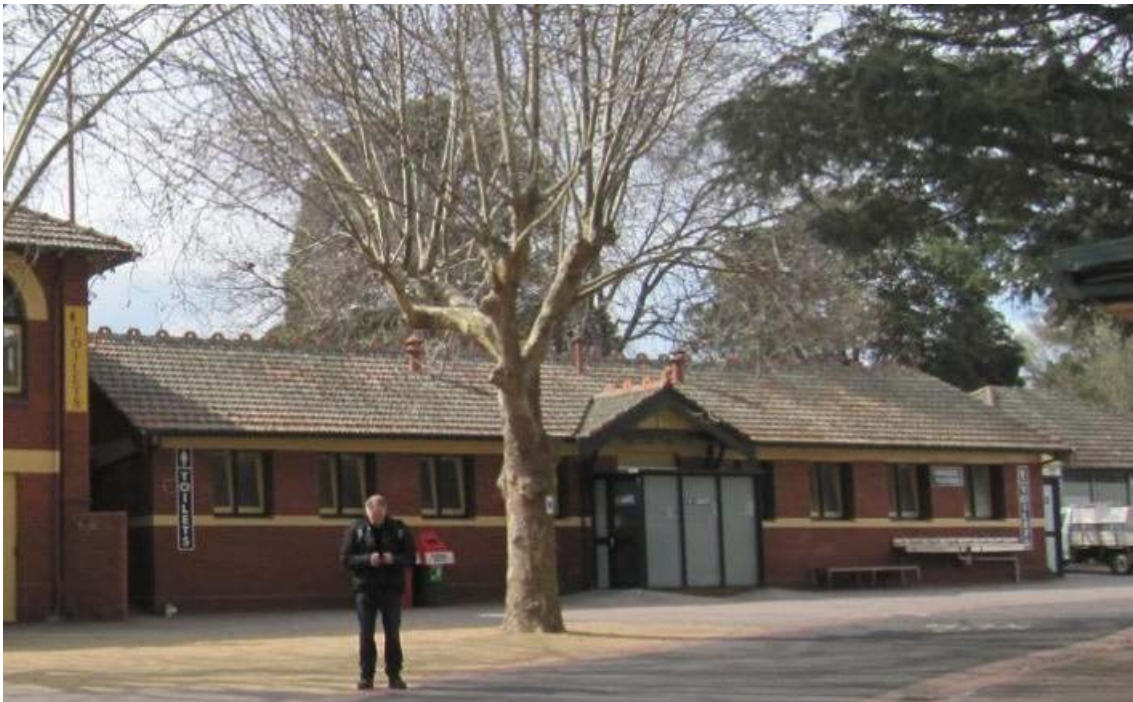


Figure 15 South elevation, 1920s toilet block

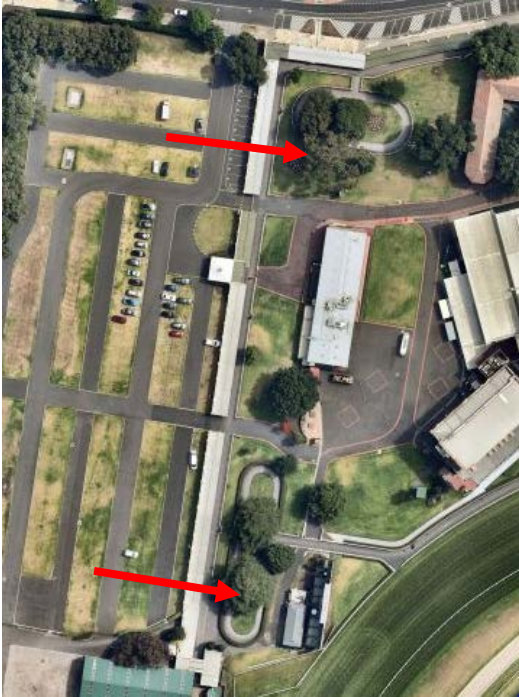


Figure 16 Recent aerial photograph showing existing walking rings at the north end of the racecourse indicated by arrow
Source: Nearmap



Figure 17 View of lawn area to west of Main Entrance Gate including mature tree specimens

1.4.2 Proposed works

It is proposed to demolish the existing day stalls, walking rings and horse exercise areas dotted around the racecourse and replace them with a new consolidated 'horse area'. The mounting yard will also be relocated – this is considered below (refer Section 1.5)

The new horse area is proposed to be constructed in the northern section of the racecourse to the west of the Main Entrance. The horse area will comprise a sunken section which connects to the new mounting yard and substantial underground back of house areas. Public access to the sunken area will be restricted, however visibility into the area – replicating the 'Birdcage' experience - will be possible from the concourse. To the west of the Boomerang Building a partially above ground Horse Integrity Area is proposed. Unlike the majority of the new horse area this structure will extend above the existing ground level. The new roof level will rise to approximately the first floor level of the Boomerang Building.

The new horse/day stalls area will accommodate the following features and minimum requirements:

- Provision of 130 day stalls
- Provision of 8 wash and urine stalls
- A secured site design with high levels of stall visibility from all areas for integrity purposes
- Weather protection, particularly sun, for horses
- Minimum widths and for horse pathways and access points
- Pre-parade and exercise ring of a minimum 150m length
- Vehicle accessibility to day stalls and horse paths
- Direct adjacencies to float loading area, integrity complex and the mounting yard.
- Patron accessibility to all stalls from separate secure areas with cross over between patrons and industry areas
- High quality horse stall views from the public realm and grandstand

The construction of the horse area requires the demolition of a number of buildings and removal of several mature trees:

- Toilet Block abutting Boomerang Building
- First aid room
- Race book sales kiosk
- TAB Office
- Race Day office
- Stores
- Day stalls
- Tent structures to rear of Rupert Clarke Stand (Scratching Board retained)

Trees to be removed in this area include (refer to Figure 5 for CMP tree numbers):

Tree number (WP3 & 4)	Tree no (CMP)	Common name	Species	Significance (CMP)
12	N/A	London Plane	<i>Plananus acerifolia</i>	N/A
14	12	London Plane	<i>Plananus acerifolia</i>	Primary
16	13	English Oak	<i>Quercus robur</i>	Primary
15	15	Canary Island Date Palm	<i>Phoenix canariensis</i>	N/A
17	14	English Oak	<i>Quercus robur</i>	Primary

Tree number (WP3 & 4)	Tree no (CMP)	Common name	Species	Significance (CMP)
18	15	London Plane	<i>Plananus acerifolia</i>	Primary
19	16	London Plane	<i>Plananus acerifolia</i>	Primary
20	20	Canary Island Date Palm	<i>Phoenix canariensis</i>	N/A
21	21	Desert Ash	<i>Fraxinus angustifolia</i>	N/A
22	22	Canary Island Date Palm	<i>Phoenix canariensis</i>	N/A
23	23	Magenta Cherry	<i>syzygium paniculatum</i>	N/A
24	17	Aleppo Pine	<i>Pinus halepensis</i>	Secondary
25	25	Desert Ash	<i>Fraxinus angustifolia</i>	N/A
26	26	Elm (grove of 5)	<i>Ulmus sp</i>	N/A
27	27	Variegated Brush Box	<i>Lophostemon confertus</i>	N/A
28	28	Brush Box	<i>Lophostemon confertus</i>	N/A
29	29	Brush Box	<i>Lophostemon confertus</i>	N/A
30	19	English Elm	<i>Ulmus procera</i>	Primary
31	31	Brush Box	<i>Lophostemon confertus</i>	N/A
32	20	English Elm	<i>Ulmus procera</i>	Primary
35	21	English Elm	<i>Ulmus procera</i>	N/A
36	36	Canary Island Date Palm	<i>Phoenix canariensis</i>	N/A
39	39	Lilly Pilly	<i>Syzygium smithii</i>	N/A
40	40	Lilly Pilly	<i>Syzygium smithii</i>	N/A
41	41	Tulip Tree	<i>Liriodendron tulipifera</i>	N/A
42	42	Brush Box	<i>Lophostemon confertus</i>	N/A
60	N/A			
61	N/A			

1.4.3 Significance

Boomerang Building

The CMP identified the Boomerang Building as being of primary significance as follows:

Values: Historical, Architectural

This is a large and substantially externally intact 1919 building, prominently located in the north-western area of the racecourse, where it contributes to the heritage value and character of this area of the course. Comparatively, no similar buildings - substantial purpose built refreshment buildings of the early twentieth century - exist or have survived at the other major Melbourne metropolitan racecourses. Architecturally, the building is distinguished by its 'boomerang' (L-shaped) plan with conjoined north and west wings and its highly-regulated facades. It also adopts an understated Federation Free style expression in red brick with rendered detail; and the round-headed arched openings at the first floor level of each bay form a consistent motif around the perimeter of the building.

Toilet block

The Toilet Block is identified as being of primary significance in the CMP. The following individual statement of significance is provided for the block:

Values: Historical, Architectural

The toilet block is a modest historic building at the racecourse, dating from the c.1920s. Providing public amenities for patrons, it is not considered to be a high level of individual significance, but its interwar origins are clear through its form, architectural design, materiality and detailing. It contributes to the significant interwar group in the northern area of the course.

First aid room

The first aid room is identified as being of secondary significance as follows:

Values: Historical

The First Aid room is a modest and heavily altered building, however it is clearly of interwar origin and is one of the collection of buildings retained at the racecourse from this period.

Trees

A number of trees in the northern part of the site are identified as part of the primary significant 'Northern Tree Plantation' as follows:

Northern Tree Plantation

Values: Historical, Aesthetic

A number of other mature trees located within the northern racecourse area were planted as part of a major phase of building works within the site, comprising Deodar Cedars (*Cedrus deodara*), English Oak (*Quercus robur*) and London Planes (*Platanus xacerifolia*).

These trees provide a contemporary landscape setting to a number of structures identified of significance within the racecourse, reinforce the overall character of the place, as provide a high level of amenity and visual interest within the northern precinct.

Horse areas

The existing horse area are identified as being of secondary significance as follows:

Values: Historical, Social

The Walking Rings, Mounting Yard and Birdcage are significant spatial, or in this case, landscape elements of the racecourse and race day function, but the fabric and form of these elements is not of heritage value. They are a valued and popular component of the race day experience, including for their association with the race day horse stalls, and this in turn relates to their general form and location.

Other elements

The following elements are identified as being of little or significance in the CMP:

- Race book sales kiosk
- TAB Office
- Race Day office
- Tent-like structures to rear of Rupert Clarke Stand.

1.4.4 Policy

Boomerang Building

The CMP policy for the Boomerang Building is as follows:

This is a large and substantially intact historic building at the racecourse. While it no longer accommodates its original function, it remains a prominent building which contributes to the heritage value and character of the northern area of the racecourse.

Given the unusual form of the building, it offers limited opportunity for additions or extensions and any works to the exterior should be carefully considered.

No works should be contemplated that would diminish or obscure a reading of the 'boomerang' (L-shaped) plan, or of the articulated bays. The canted south end of the west wing is a particularly strong architectural feature of the building, as is the central stair up to the elevated first floor entry, at the junction of the two wings.

Internally, at first floor level, the large uninterrupted spaces which may have been dining areas and which retain some original skirting boards and strapped ceilings, should preferably be retained as open plan areas; or if adapted, new partitions should be kept to a minimum. At ground floor level, it is recommended that the original serving arrangements/counters be retained on the west elevation of the west wing; if the opportunity arose, this arrangement could also be reinstated on the east elevation. The original signage on the south façade of north wing should also be retained.

Toilet Block

The policy for the Toilet Block is as follows:

It retains its original function but with a modern fitout.

The building has modern additions on both the south and eastern ends, both of which detract from its presentation. Of the two, the modern glazed entry bay to the south elevation has had a more significant impact and in preference would be removed to reinstate the original porch entry. The glazed bay to the east end also

obscures original fabric but has a lesser impact. The building also has horse stalls constructed up to it on the northern side (date unknown but pre-1949). Removal of the stalls would reveal the original north wall and detailing.

There are no constraints on the adaptation of the interiors for the existing or an alternative use.

As a small building, viewed on three sides, the toilet block does not lend itself to the construction of a major addition or extension.

First aid room

The policy with regard to the first aid room is as follows:

While a relatively minor and heavily altered building, the First Aid building retains some of its original interwar form and character and on that basis, retention is preferred.

Horse areas

The policy with regard to the horse areas and other similar features is as follows:

The Walking Rings, Mounting Yard and Birdcage are, as with the Betting Ring, significant spatial, or in this case, landscape elements of the racecourse and race day function, but the fabric and form of these elements is not of heritage value. They have been changed and relocated in the past. While it is important to maintain these facilities at the racecourse, their current arrangement and form can be modified

Landscape and trees

The relevant policy with regard to landscape change (including trees) is as follows:

Policy: A tree replacement strategy should be prepared to guide future tree plantings within the site to reinforce valued landscape character whilst considering both natural change to the landscape as well as change as a consequence of new development.

Whilst new works and development required to support the ongoing operation of Caulfield Racecourse should be sympathetic and responsive to its heritage values and significant elements, intensive race-day uses are concentrated in the north of the site where the majority of significant trees are located in close proximity to buildings. There is an inevitability that some trees identified of heritage significance will require removal to facilitate expansion or development of race-day activities such as new grandstands, stalls, walking rings etc.

An over-arching tree replacement strategy should be prepared so that removal and replacement of trees is undertaken in a considered and programmed manner, rather than a reactive and piecemeal approach that would otherwise diminish the valued landscape character of the racecourse. The strategy would include mitigation measures for tree removal such as:

Phased replacement of over-mature specimens so that impacts to the valued, visual qualities of the place are limited as far as possible by the natural aging and decline of trees;

Consideration for tree transplanting where removal of significant trees is required for new development;

Planting new tree specimens prior to tree removals for new development to mitigate against loss of amenity and diminution of the broader aesthetic appeal of the Caulfield racecourse landscape, especially within public areas;

Guidelines for selection of tree replacement stock and best practice planting measures for urban landscapes so that new plantings thrive and develop in optimal planting conditions;

More broadly, the strategy should recognise that the public area landscape comprises not only treed, shaded areas but also valued open areas of paving and lawns allowing for a variety of passive uses and experiences, and this balance should be maintained.

The strategy can be implemented as part of new landscape works associated with building development so that these works are complementary to the valued landscape character of the place. This approach recognises that because of the use of the place change within the landscape is inevitable, including removal of significant trees and other elements. In imposing such change, changes to the valued character of the Caulfield Racecourse landscape are mitigated in the short term, with a longer-term objective of enhancement.

It is noted that when trees reach the end of their useful life or their removal occurs as part of the expansion or development of race-day facilities, there is not considered to be a need from a heritage perspective to replace trees on a like-for-like basis (i.e.: same species in the same location). This includes the Elm row. The historical significance as a (likely) remnant of the earlier alignment of Station Street rather than as a formal element as part of a landscape design for the racecourse. On the basis it is as a remnant and the age of the trees demonstrates that association, the replanting/ reinstatement of the row form is not a requirement.

Elements of little or no significance

The relevant policy with regard to elements of little or no significance is as follows:

Policy: The elements of little or no significance at Caulfield Racecourse can be retained or removed, but works to and involving these elements should not have an adverse impact on the elements of primary and secondary significance, nor detract from or diminish the overall significance of the racecourse.

1.4.5 Comment

The proposed horse area will result in a substantial change in this part of the course including the removal of a building of primary significance and a significant number of mature trees. Acknowledging this, and as discussed in detail in the main HIS report, the proposed change is directly related to the operations of the course and will support the overall race-day experience. Bringing the horse areas from disparate back of house areas that have reached the end of their useful life to a more centralised and public-facing space is a positive outcome, and supports the continuation of race day activities as desired by CMP policy 6.4.1 which is as follows:

The use of the place as a racecourse is fundamental to its heritage significance. Works and development which support this use, including maintaining the viability of the place as a racecourse, should be supported.

The removal of elements of little or no heritage significance will have no heritage impact and is supported by the CMP policy. The removal and relocation of the existing horse areas (including walking rings, day stalls etc) is also supported by policy, which notes that it is the use of these elements to

support the race day operation that is of value, not the specific fabric or location of the existing examples.

While the majority of the fabric proposed to be removed to support the construction of the new horse area is of little or no significance, there are also several elements of primary or secondary significance which will be impacted by the works. These are considered in detail below.

Boomerang Building

With regard to the Boomerang Building, no direct works to the building are proposed with the exception of conservation and make good works where the abutting Toilet Block currently connects and is proposed for demolition. It is noted that an extensive scope of works to the Boomerang Building, including external conservation works, internal adaptation works and code compliance works, was the subject of an earlier planning application (WP2).

Policy 6.4.3 of the CMP notes the following with regard to the setting of heritage buildings:

Significant buildings which are currently viewed in the round, or have views to more than one elevation, should also in preference retain this aspect of their presentation. This includes the Boomerang Buffet Building and adjoining 1920s toilet block, albeit the north sides of these buildings are already impacted by the later addition of race day stalls.

The proposed new horse area and associated horse integrity area are in close proximity and would partially obscure the western elevation of the Boomerang Building in views from the west. This impact would be mainly in longer views noting that the new building is offset from the western elevation of the Boomerang Building and it will continue to be read and experienced in the round at close quarters.

On balance, the construction of a new building in proximity will not harm the significance of the building or detract from an understanding of its historical and architectural values.

It is also noted that the day stalls to the north elevation are to be removed and that this will re-establish views to this part of the building.

Toilet Block

While a relatively modest building and not one of high individual significance, the demolition of the Toilet Block will have an adverse heritage impact in that it will reduce the extent of surviving building stock from the interwar period (an attribute that distinguishes Caulfield from other racecourses) and specifically a building that relates strongly to the adjacent Boomerang Building (1919).

The demolition is proposed to facilitate the construction of the new horse area, which is the centrepiece of the redevelopment from an operational perspective.

Consideration was given to retention however this was not feasible in consideration of the interface with the horse area, and the functional and spatial requirements of the new facility. The 'Toilet Block and Boomerang Package' prepared by Hassell illustrates the relationship of the horse area with the existing northern precinct and the positioning of Toilet Block relative to the proposed new facility. The Toilet Block sits directly above a section of the new sunken area and its retention would significantly compromise the layout and operation of the new horse area, which is guided by minimum requirements to achieve best practice welfare outcomes for horses. Altering the siting or layout of the horse area to allow for retention of the Toilet Block would also potentially create flow-on impacts on the Boomerang Building or Main Entrance Gates, both also elements of primary significance and of a higher order.

The Toilet Block is an early building that supports the continuation of the interwar character of the racecourse and its removal will have an impact on the ability to appreciate this character, however other key buildings from this era (e.g. Boomerang Building, Afternoon Tea Room, Main Entrance Gates

etc.) will be retained and conserved as part of the works, ensuring that the overall character and 'principal characteristics' of the racecourse are maintained.

The new horse area is essential to the vision of the MRC to promote world class horse racing and entertainment for their members and customers, the loss of the Toilet Block is balanced by the provision of new facilities that will support the long-term viability and operation of the place.

The building should be recorded prior to its demolition.

First aid room

While the policy notes that retention of this building is preferred, it is highly altered and plays only a minor supporting role to the significance of the racecourse. Its demolition would have a limited adverse impact.

The building should be recorded prior to its demolition.

Landscape and trees

The existing mature landscape character of the racecourse contributes to the overall significance of the place and the tree specimens specifically are of primary significance for their historical and aesthetic values. The removal of these trees will have an impact on the heritage values and the character of this part of the racecourse.

The CMP policy recognises that the removal of trees to support the main operation of the place is inevitable particularly given the concentration of trees in the northern section of the racecourse where they interact with the main race day activities.

While a number of trees are proposed to be removed, a substantial number of trees across the racecourse will be retained and a number of new trees will be planted, ensuring that the overall treed nature of the racecourse is retained.

The CMP suggests a number of mitigation measures to manage the impact associated with tree removal; key amongst there is the preparation and implementation of an effective tree replacement strategy to establish an appropriate landscape character. This strategy should be prepared by an arborist/landscape specialist with knowledge of historical heritage values as related to landscape.

Subject to the appropriate mitigation through the development and implementation of such a strategy, the removal of the trees to support the new horse area, key to delivering the upgraded race day operations and modernising facilities, is considered to be an acceptable action from a heritage perspective.

1.5 Mounting yard and new public area

1.5.1 Description

To the east of the grandstands there is large events lawn area and the existing Administration Building (Figure 18). The Administration Building is a circa 1990 two-storey office block with some habitable areas and an expansive deck at second floor level. It is realised in face brick and render with aluminium, window and door framing. The building presents an understated facade to the north with large areas of glazing to the racecourse to the south.



Figure 18 Administration building, north façade

1.5.2 Proposed works

It is proposed to demolish the existing Administration Building to create an expanded public reception/lawn area and to relocate the mounting yard from in front of the Rupert Clarke Stand to the west between the stand and the new open area.

The Canary Island Palm in this location (Tree 4) will also be removed.

1.5.3 Significance

The Administration Building is of little to no significance.

The Mounting Yard is identified as being of secondary significance as follows:

Values: Historical, Social

The Walking Rings, Mounting Yard and Birdcage are significant spatial, or in this case, landscape elements of the racecourse and race day function, but the fabric and form of these elements is not of heritage value. They are a valued and popular component of the race day experience, including for their association with the race day horse stalls, and this in turn relates to their general form and location.

1.5.4 Policy

The policy for the Mounting Yard and other similar features is as follows:

The Walking Rings, Mounting Yard and Birdcage are, as with the Betting Ring, significant spatial, or in this case, landscape elements of the racecourse and race day function, but the fabric and form of these elements is not of heritage value.

They have been changed and relocated in the past. While it is important to maintain these facilities at the racecourse, their current arrangement and form can be modified.

1.5.5 *Comment*

The Administration Building is a building of little to no significance and its demolition raises no heritage concerns.

The proposed relocation of the Mounting Yard complies with the CMP policy with regard to this element. The key consideration for such elements at the racecourse is that they are maintained and their use associated with race day activity remains. The relocated mounting yard will connect to the new horse area, creating a more seamless and safe transition for horses to the mounting yard from the back of house areas. There will be no heritage impact associated with this change.

The new events lawn in this area will also support the overall use and function of the racecourse by allowing additional space for race day attendance. Refer to the assessment at section 1.5.5 and 5.2 for discussion of tree removal works.

1.6 **Maple Building**

1.6.1 *Description*

This building was constructed following the incorporation of Bond Street into the racecourse in 1957/8, to provide additional totalisator facilities at ground floor level, and refreshment outlets (Maple Bar) at first floor level. The architect was Albion H Walkley.³

The building is of two storeys and is constructed of brick at ground floor level with a first floor comprised entirely of glazing and other lightweight panels with large areas of steel-framed windows. It has a low-pitched hipped roof clad in galvanised steel. Access to the upper level is by stairs to the north and south and brick and concrete ramp on the west side. As constructed the ground floor on the east side was set back under the overhanging first floor, providing shade and cover to patrons accessing betting windows. The verandah formed by this arrangement has been largely infilled and betting windows relocated into this area. The building is currently used as an office for the MRC.

1.6.2 *Proposed works*

It is proposed to demolish the Maple Building. This section of the racecourse will be converted to float and car parking, with upgraded road access from the west.

1.6.3 *Significance*

The Maple Building is identified as being of secondary significance as follows:

Values: Historical

The Maple Building is a late 1950s building at the racecourse, of historical interest as reflecting the development of the western area of the racecourse in the post-WWII period, following the incorporation of Bond Street into the site in 1957/8. However, it is not an architecturally distinguished building; it is not intact (it has been modified at ground floor level, although it is more intact at first floor); and it is not discernibly or distinctively a racecourse building.

1.6.4 *Policy*

The Maple Building is a later (1950s) building at the racecourse, of secondary significance. It is of limited historical interest as reflecting the expansion of the course in the 1950s and the provision of additional facilities.

On this basis, the building could be retained and adapted or demolished if required.

1.6.5 *Comment*

While the Maple Building is of some historical interest in demonstrating the post-WWII development of the racecourse, it is not of individual significance and the CMP policy recognises that this building could be removed without impact on the cultural heritage significance of the place.

The demolition of the Maple Building will support the overall redevelopment of the racecourse by allowing for space to be developed for staff and float carparking to the west. The rearrangement of the western section of the racecourse reflects the reconfiguration of the northern racecourse precinct including the collation of horse areas together to the north and administration services to the west, where they will not directly interface with users in the main grandstand/events area.

The Maple Building is a secondary building of limited interest that is now redundant for the provision race day activities and/or office space to the MRC. The demolition of this building to support the overall reconfiguration and redevelopment of the racecourse and the delivery of an upgraded race day experience will not adversely impact the identified cultural heritage significance of the place.

The building should be recorded prior to its demolition.



Figure 19 Maple Building, view from the east

1.7 Administration building

1.7.1 *Proposed works*

It is proposed to construct a new Administration Building to the north-west of the racecourse to the south-west of the existing Hiskens Stand. The construction of the new building requires the demolition and removal of a number of racecourse elements including:

- Pre-parade ring
- Day stalls
- Indicator board
- Workshop

Five bays of the brick wall to the west will be removed and new openings for services will be introduced to three of the bays. The wall will be extended to the south to match the existing design.

The proposed new building is of 4 storeys and adopts a contemporary design of floor to ceiling glazing divided in a grid by extruded fin elements.

The following trees are proposed to be removed in this area:

Tree number (WP3 & 4)	Tree no (CMP)	Common name	Species	Significance (CMP)
44	N/A	Liquid Amber	<i>Liquidambar straciflua</i>	N/A
47	N/A	Illawarra Flame Tree	<i>Brachychiton acerifolius</i>	N/A
48	N/A	Illawarra Flame Tree	<i>Brachychiton acerifolius</i>	N/A
49	N/A	Liquid Amber	<i>Liquidambar straciflua</i>	N/A
50	N/A	English Oak	<i>Quercus robur</i>	N/A
51	N/A	London Plane	<i>Plananus acerifolia</i>	N/A
52	N/A	Lilly Pilly	<i>Syzygium smithii</i>	N/A
53	23	Deodar (pine)	<i>Cedrus deodara</i>	Primary
54	24	Deodar (pine)	<i>Cedrus deodara</i>	Primary
55	N/A	Callery Pear	<i>Pyrus calleryana</i>	N/A
59	N/A	London Plane	<i>Plananus acerifolia</i>	N/A

1.7.2 Significance

For details of the significance of the wall, day stalls and pine trees refer to sections 1.1.2, 1.2.3 and 1.4.3.

Pre-parade ring

The subject pre-parade ring is one of a number of walking rings in the publicly accessible areas of the racecourse, associated with the Birdcage and race day horse stalls. The current configuration of walking rings in the north-west of the racecourse dates from the incorporation of Bond Street into the course in 1957/8, and the subsequent rearrangement of elements in this area of the site. Previously, the subject walking ring had a much smaller footprint.

The pre-parade ring, along with other walking rings, the mounting yard and birdcage, is identified as being of secondary significance in the CMP as follows:

Values: Historical, Social

The Walking Rings, Mounting Yard and Birdcage are significant spatial, or in this case, landscape elements of the racecourse and race day function, but the fabric and form of these elements is not of heritage value. They are a valued and popular component of the race day experience, including for their association with the race day horse stalls, and this in turn relates to their general form and location.

Indicator board

The Betting Indicator Board was constructed in c. 1958 and incorporates a single-storey brick and concrete toilets at ground floor level with the indicator board mounted above.

The Betting (tote) Indicator board is identified as being of secondary significance in the CMP as follows:

Values: Historical

The Betting (tote) Indicator Board dates from the 1950s, with later modifications including a modern sign. It is of secondary significance for providing an important but supporting role to race day operations, although its fabric and form are not of heritage value. Like the Maple Building, it is associated with the 1950s development in the western area of the racecourse.

1.7.3 Comment

The proposed location for the new Administration Building is appropriate from a heritage perspective. This is an area of the racecourse that largely dates from the 1950s and has been subject to change. Currently it is largely a back of house space utilised for prerace horse activities.

Brick wall and race day stalls

The brick wall in the vicinity of the proposed Administration Building is identified as being of secondary significance and the policy for these sections of wall notes that the fabric itself is of little to no significance. The significance of the wall predominantly lies in its historical role in marking the boundary of the racecourse. While some sections of the wall will be removed to provide access to the new Administration Building and the associated forecourt, the overall understanding of a boundary in this area will be retained.

As noted at Section 1.2.5, the race day stalls are significant as a collection and the removal of a later section of these stalls in this location raises no additional heritage concerns.

Trees

As per the removal of the trees in the northern precinct, the loss of the two significant trees in this location will have an impact on the assessed significance of the racecourse, however the removal of mature tree specimens to accommodate redevelopment and upgrade of the racecourse is contemplated by policy. As discussed at Section 1.4, the development of a tree replacement strategy to help manage the overall landscape character and maintain the associated heritage values is encouraged.

Walking ring

The removal of the walking ring in this area is consistent with the policies in the CMP, noting this use is intended to be relocated (refer Section 1.4).

New building

The new Administration Building will be a contemporary insertion into the south-western section of the northern precinct. The Administration Building is essential to the operation of the racecourse and will support the overall operation of the place. The new building is a clearly contemporary design that is appropriate for the location and use of the building. The adoption of a contemporary design language is consistent with the approach to other new buildings at the racecourse over time (including the existing Administration Building). The new building will take advantage of the views over the racecourse, without compromising the public experience or views of the track.

Indicator board

The indicator board is proposed to be demolished. This feature is redundant and is not proposed to be replaced as this technology is no longer required at the racecourse. This feature is of secondary significance for its historical value and should be recorded prior to its demolition.

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- 1 [REDACTED] 'Caulfield Racecourse Heritage Assessment', draft, 27 June 2014, p. 92.
 - 2 [REDACTED] 'Caulfield Racecourse Heritage Assessment', draft, 27 June 2014, p. 93.
 - 3 [REDACTED] 'Caulfield Racecourse Heritage Assessment', draft, 27 June 2014, p. 93.